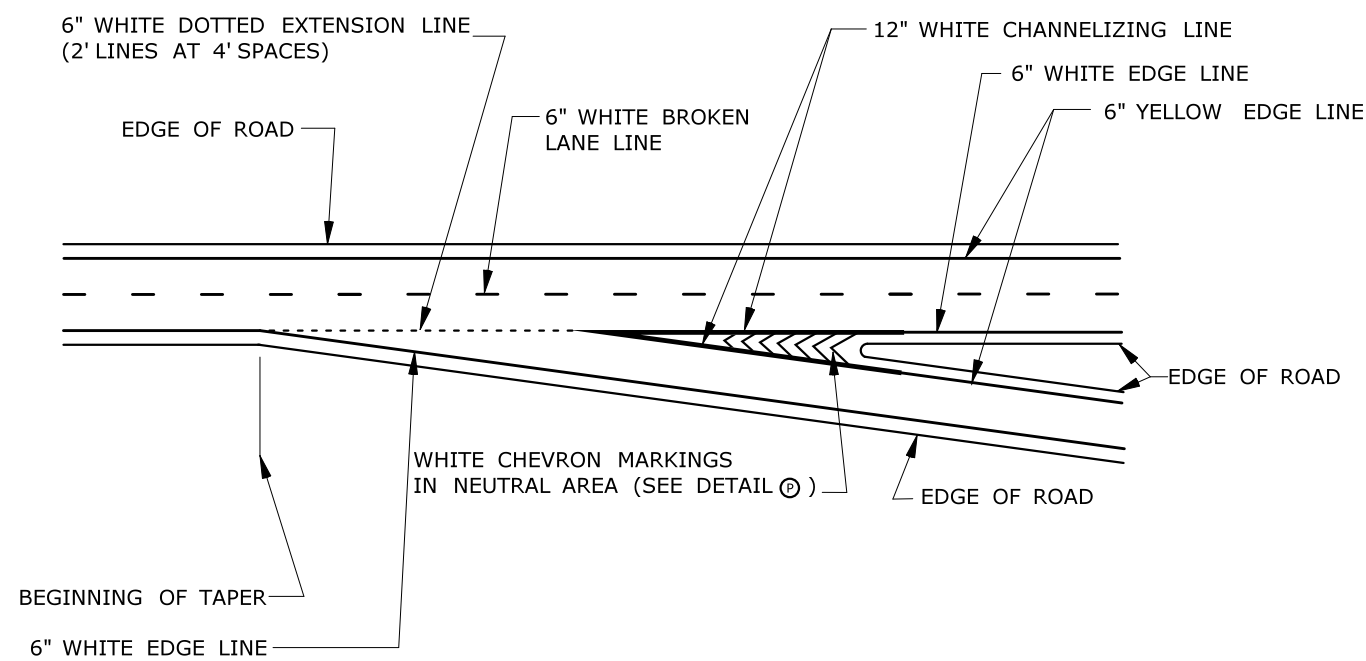
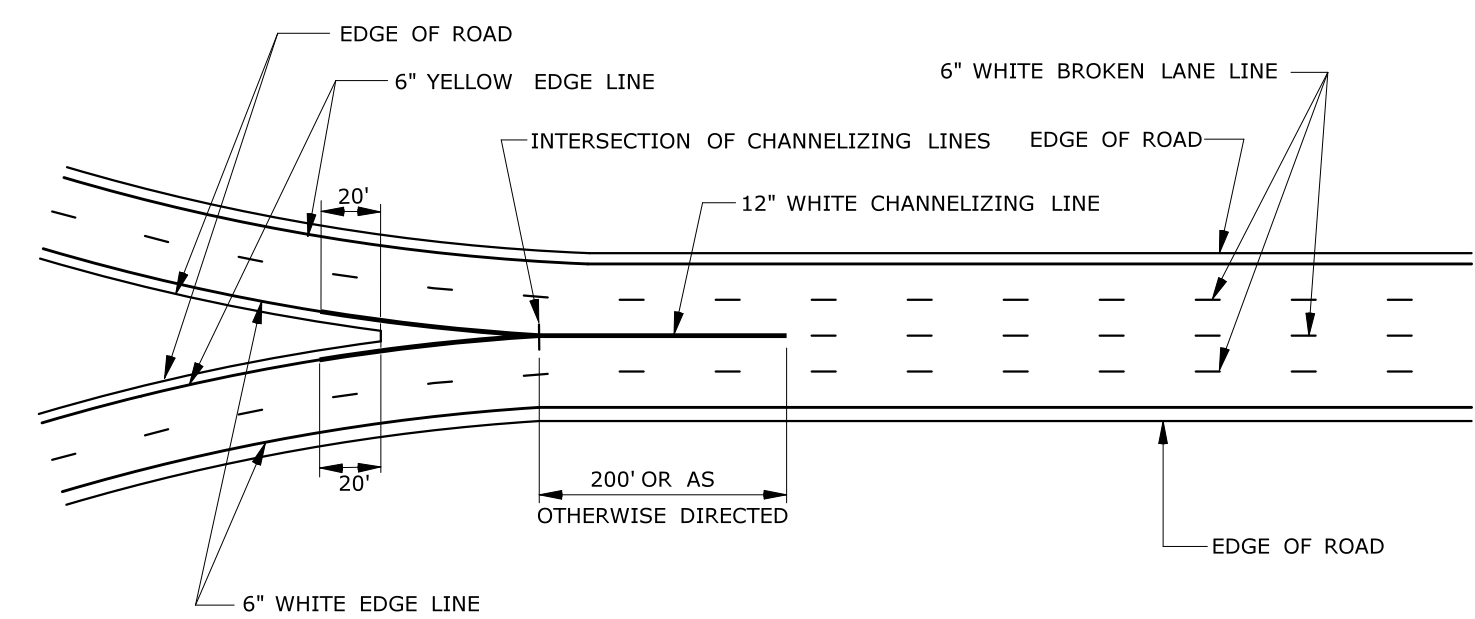


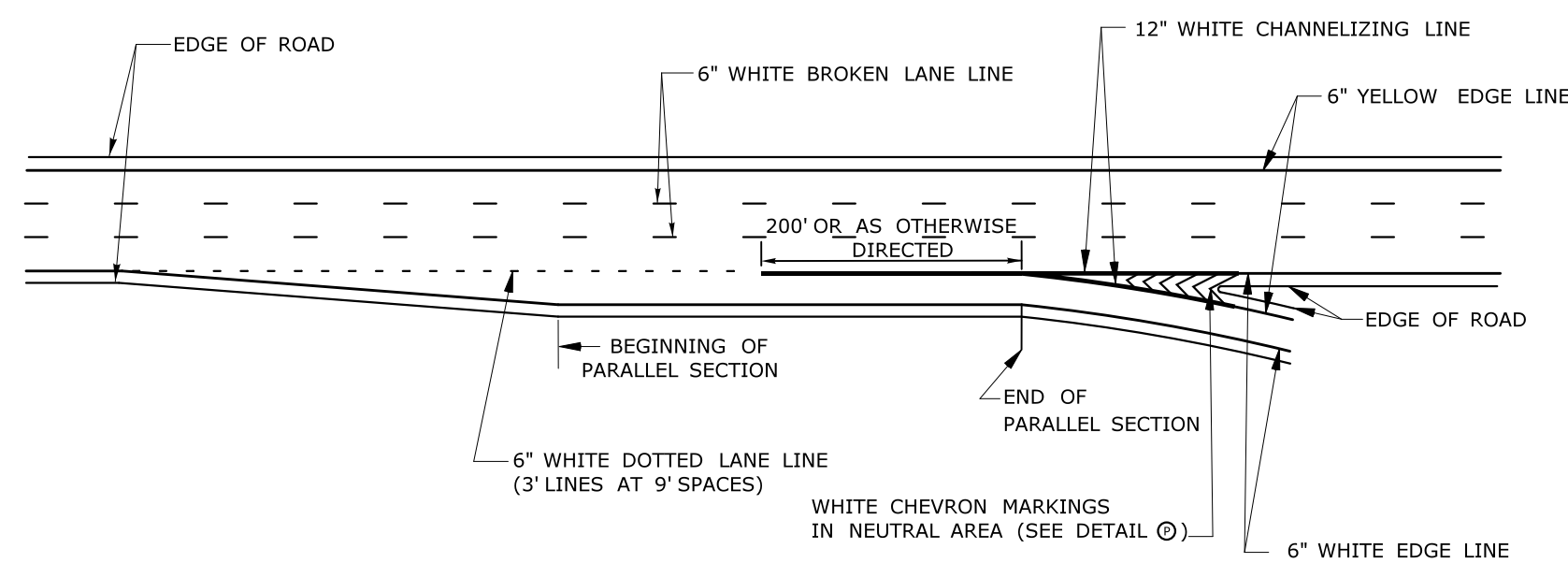
A TYPICAL PARALLEL ACCELERATION LANE ➡



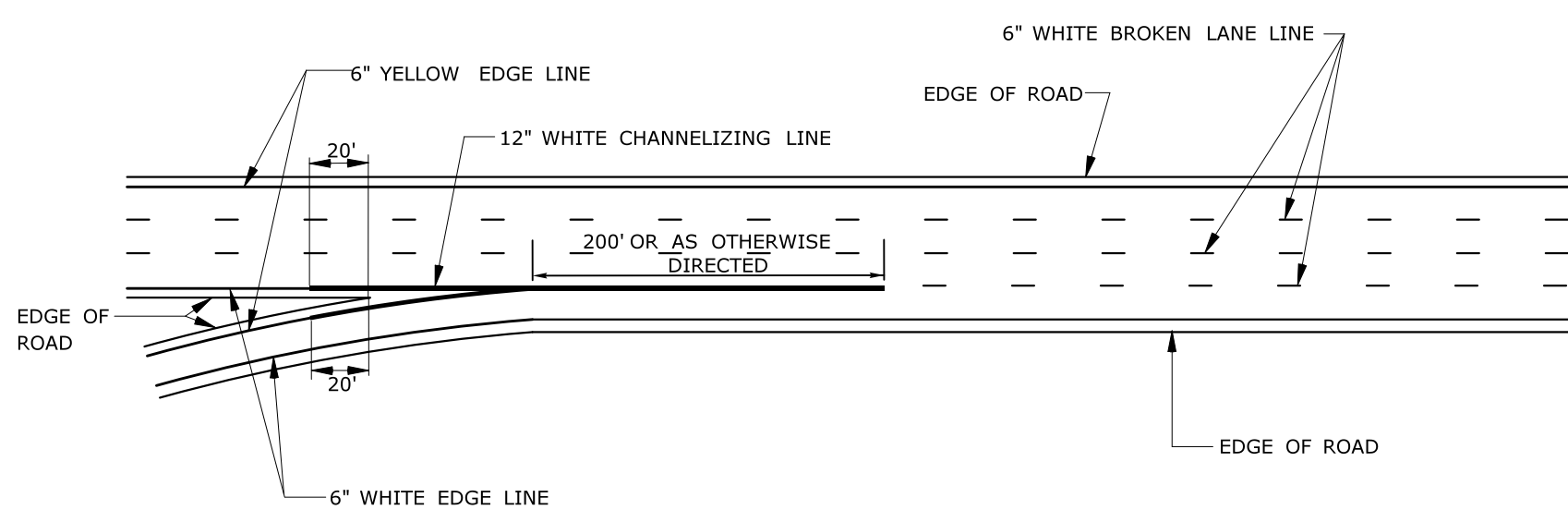
D TYPICAL TAPERED DECELERATION LANE ➡



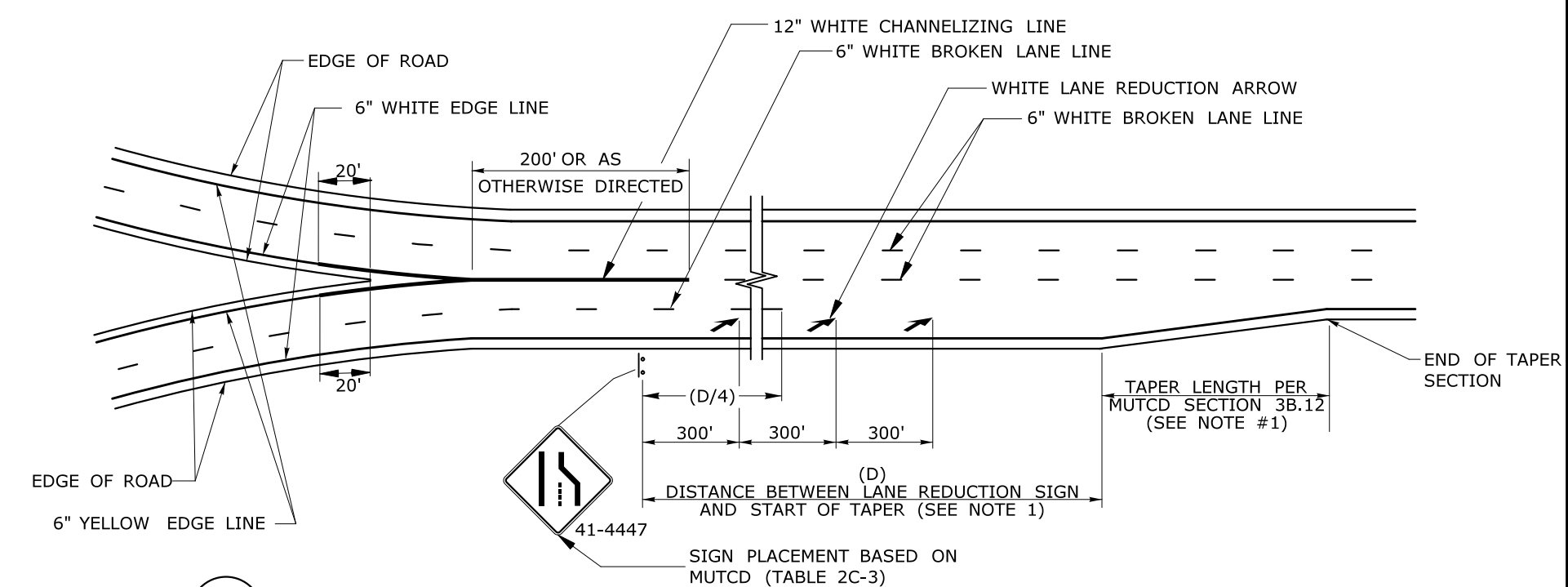
G TYPICAL MERGE TWO MAJOR ROADWAYS ➡



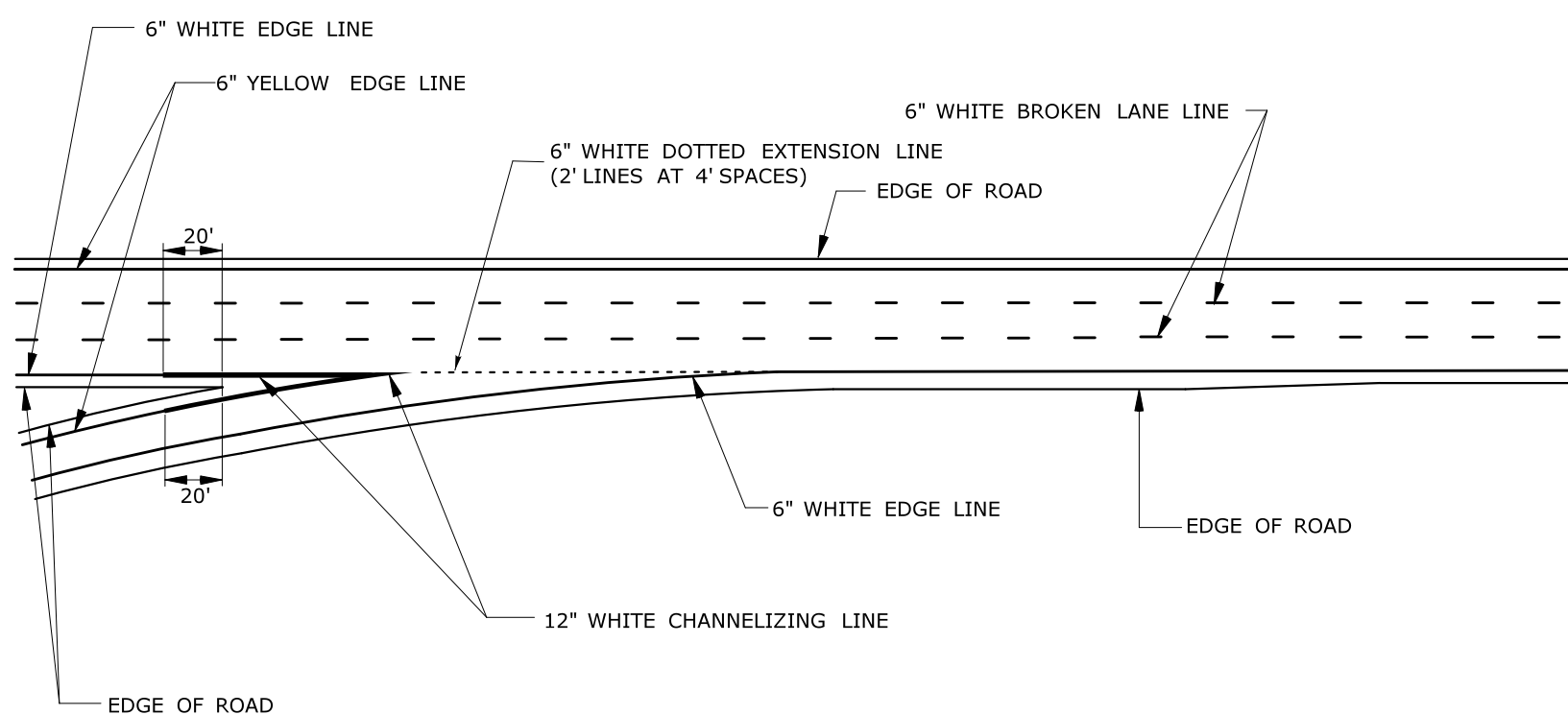
B TYPICAL PARALLEL DECELERATION LANE ➡



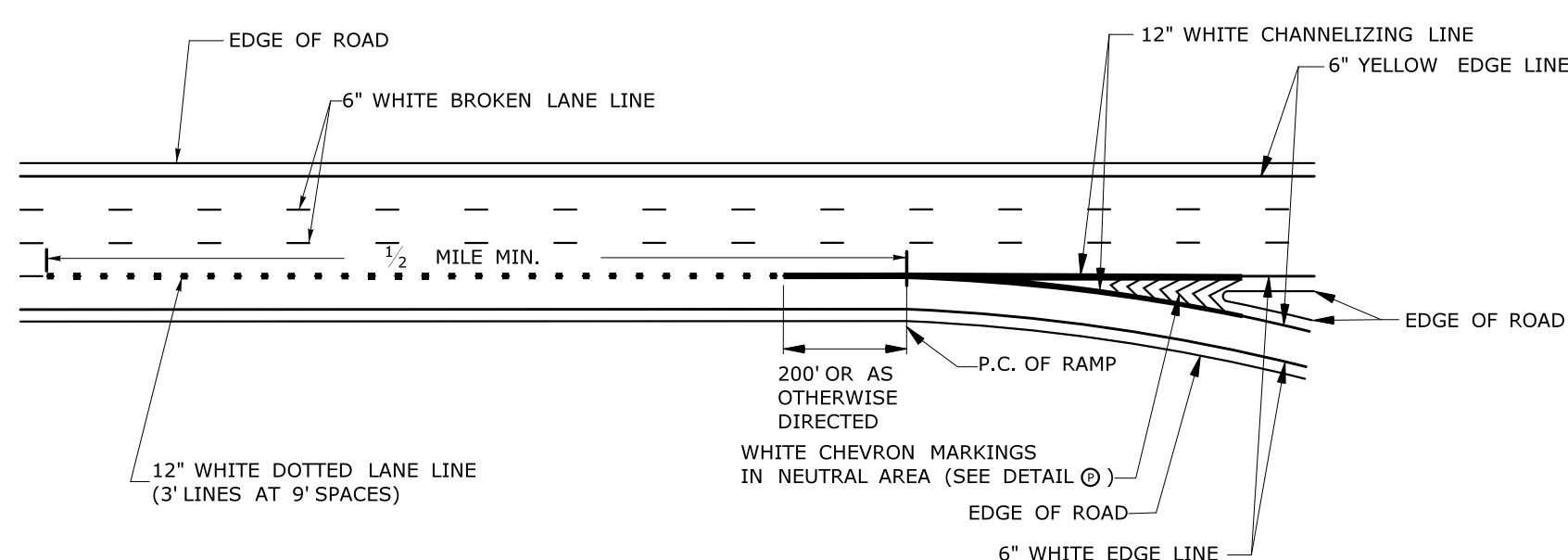
E TYPICAL LANE ADDITION ➡



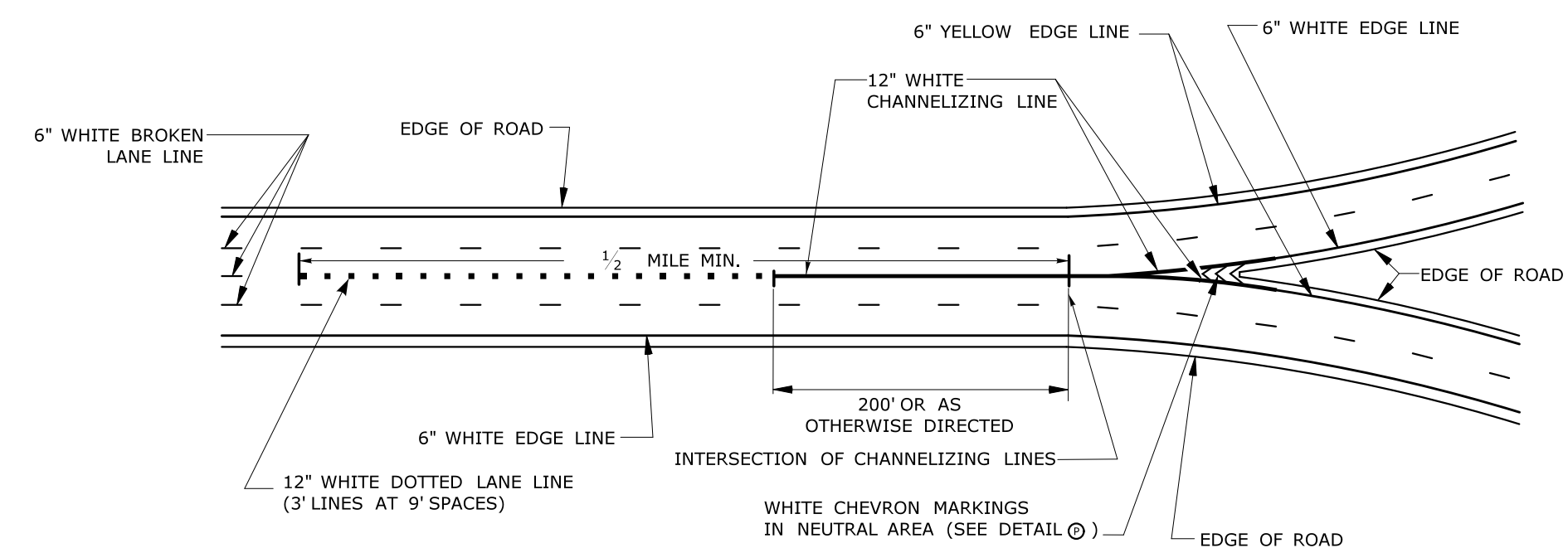
H TYPICAL MERGE TWO MAJOR ROADWAYS WITH LANE REDUCTION ➡



C TYPICAL TAPERED ACCELERATION LANE ➡
TREATMENT OF CLIMBING LANE - THROUGH ACCELERATION LANE ➡



F TYPICAL LANE DROP ➡

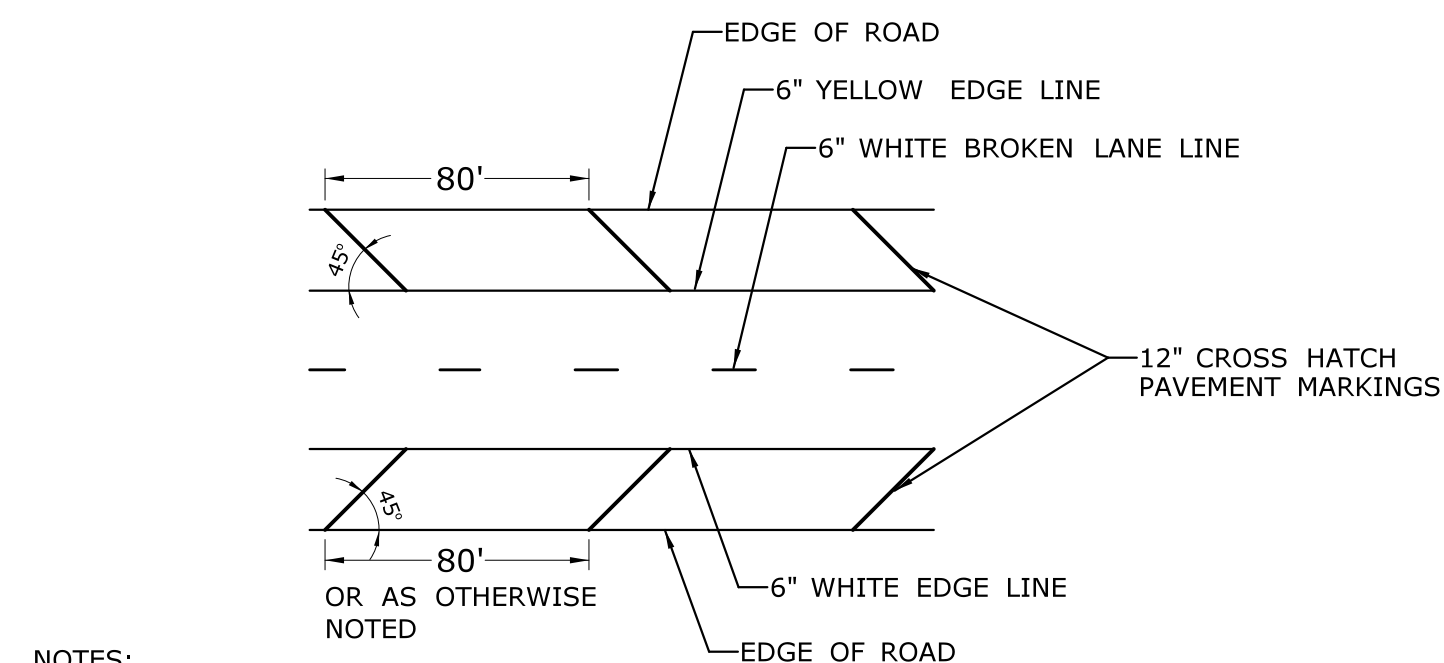
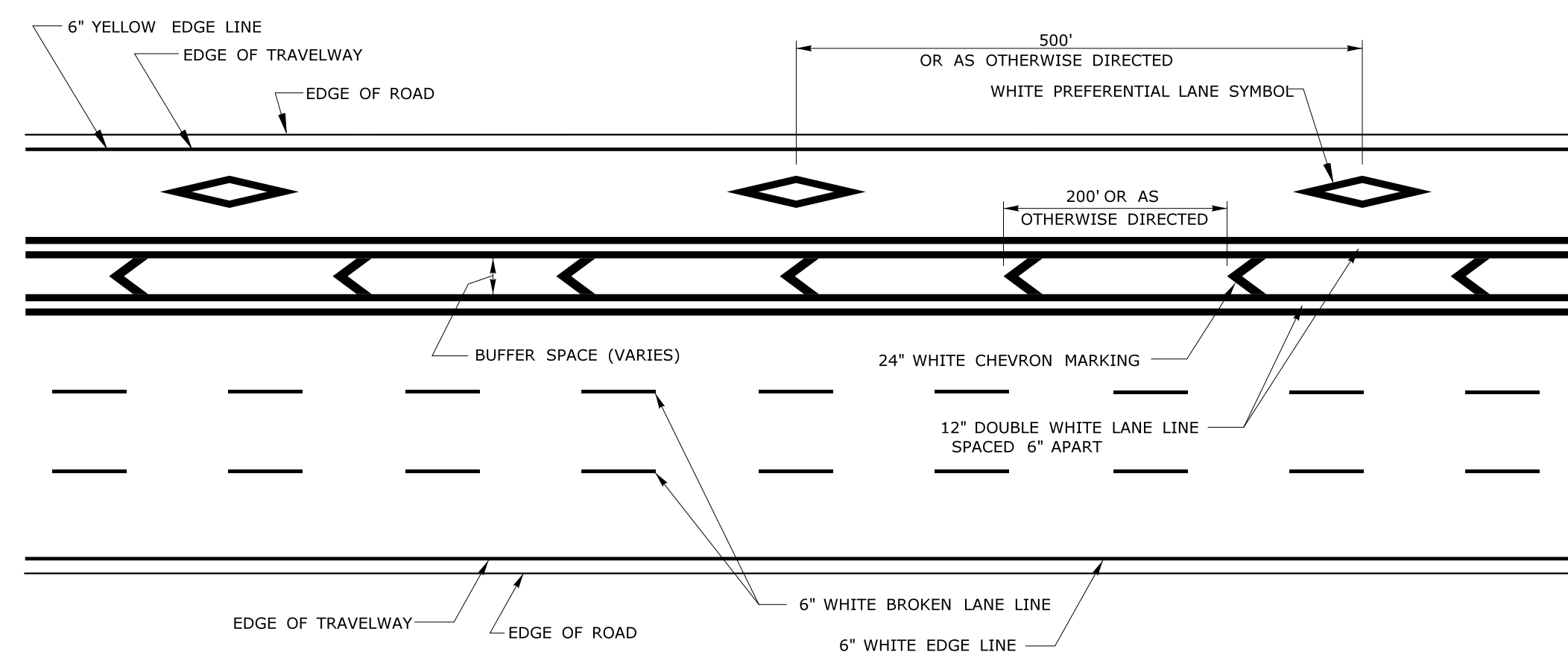
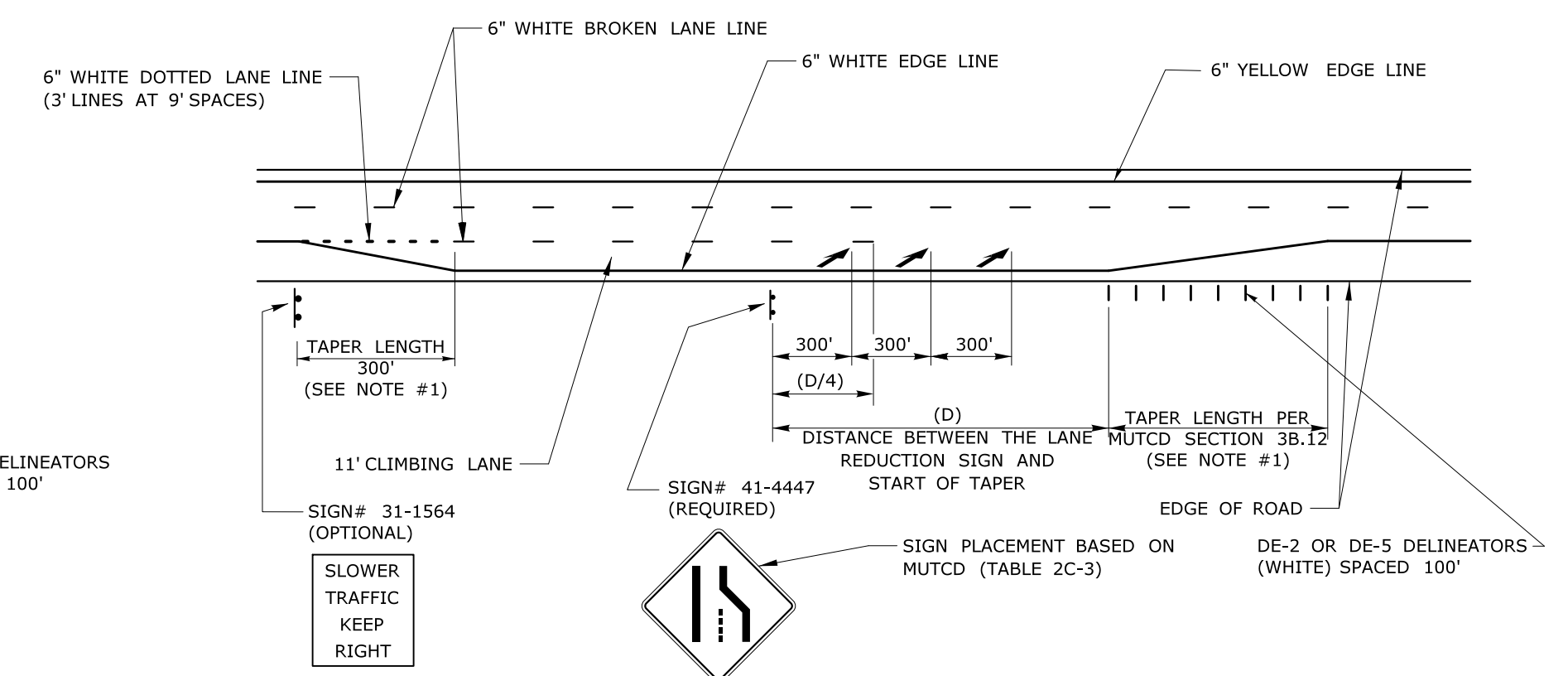
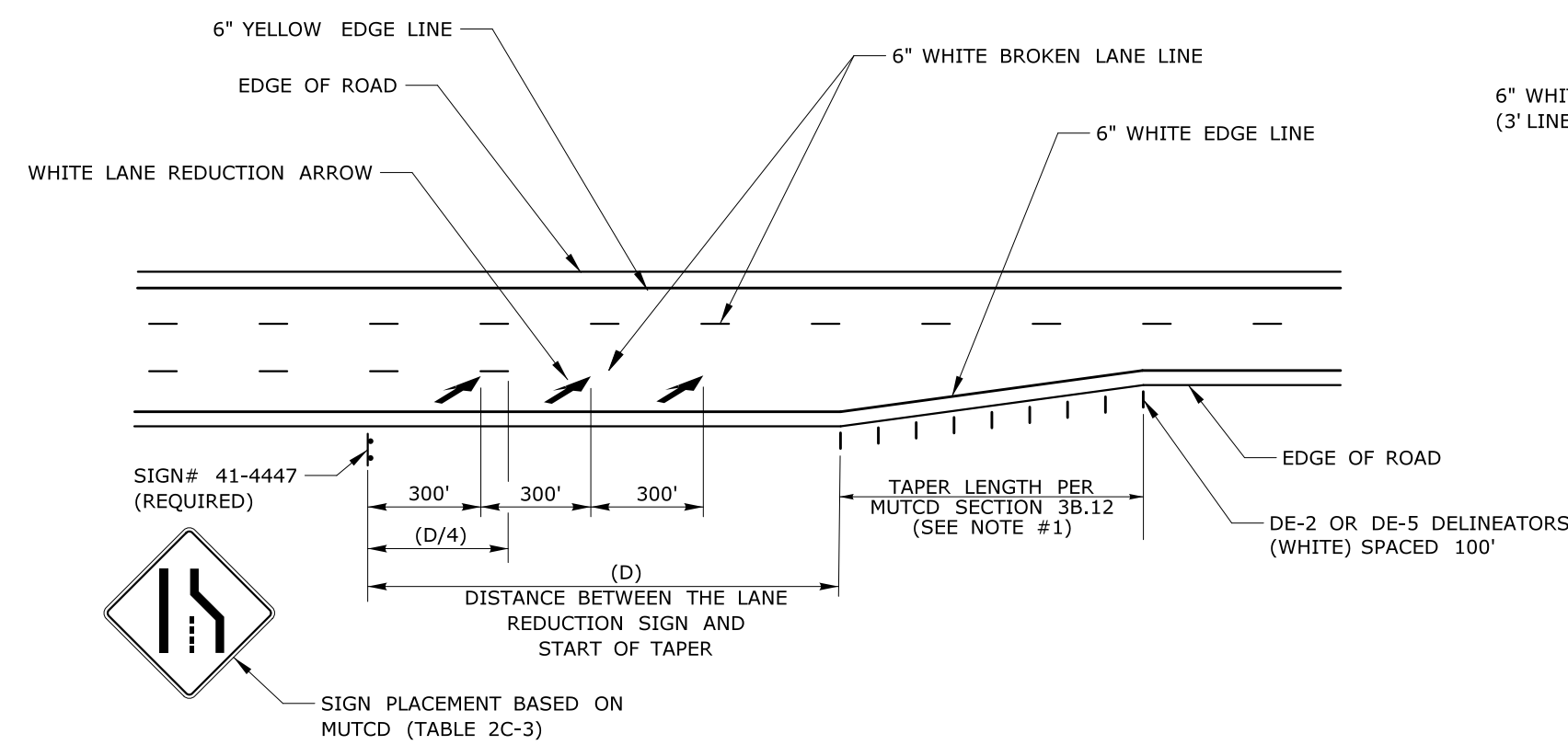
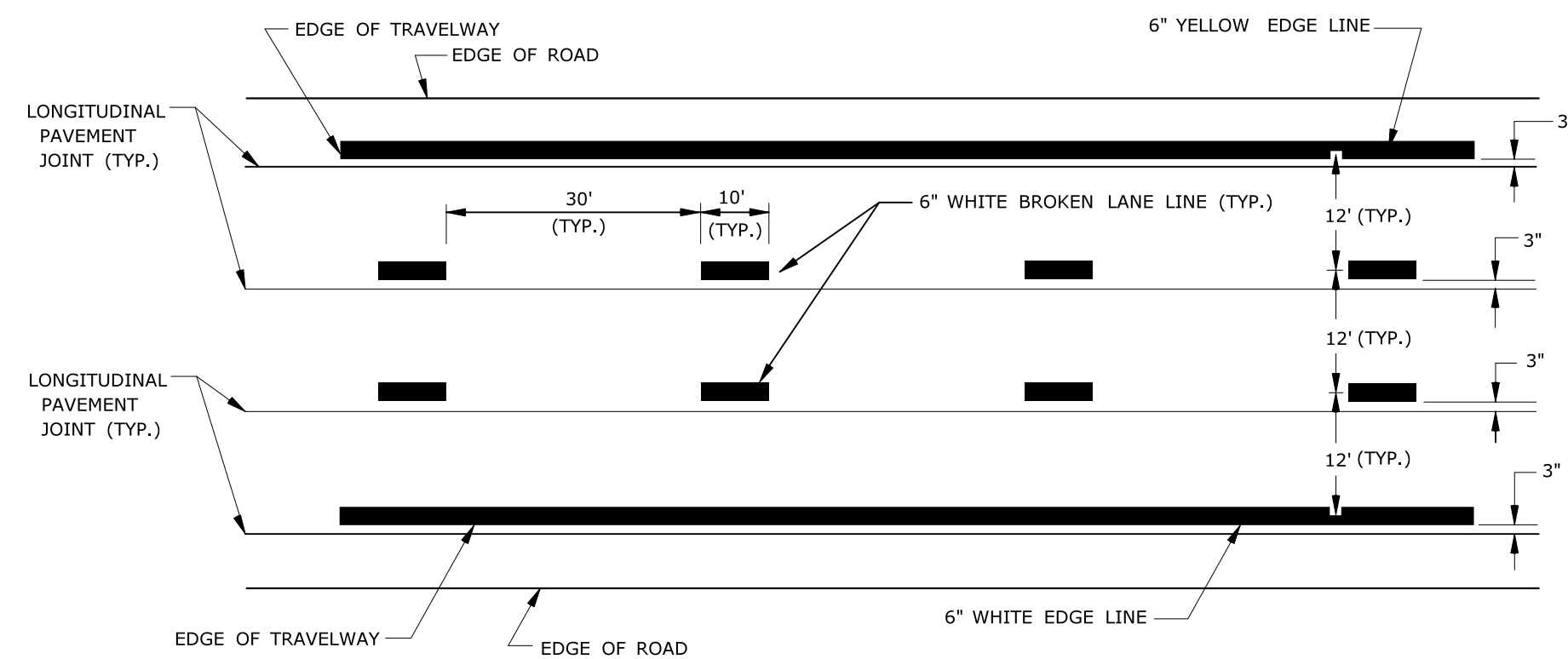


I MULTI LANE SPLIT ➡

GENERAL NOTES:

➡ DENOTES DIRECTION OF TRAVEL

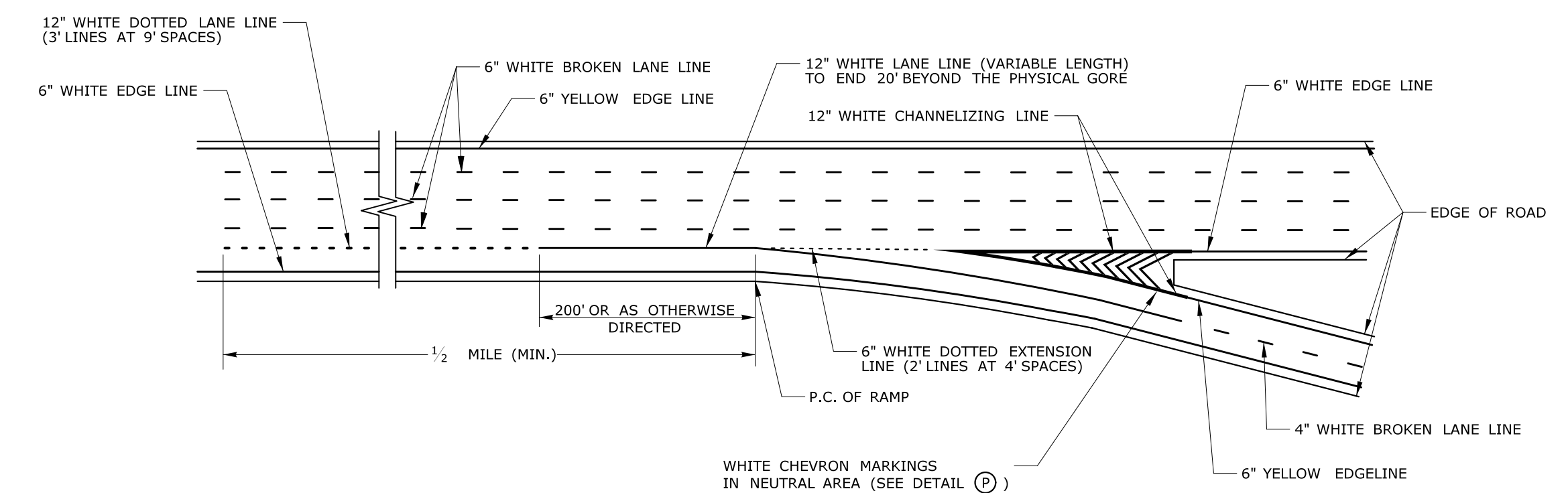
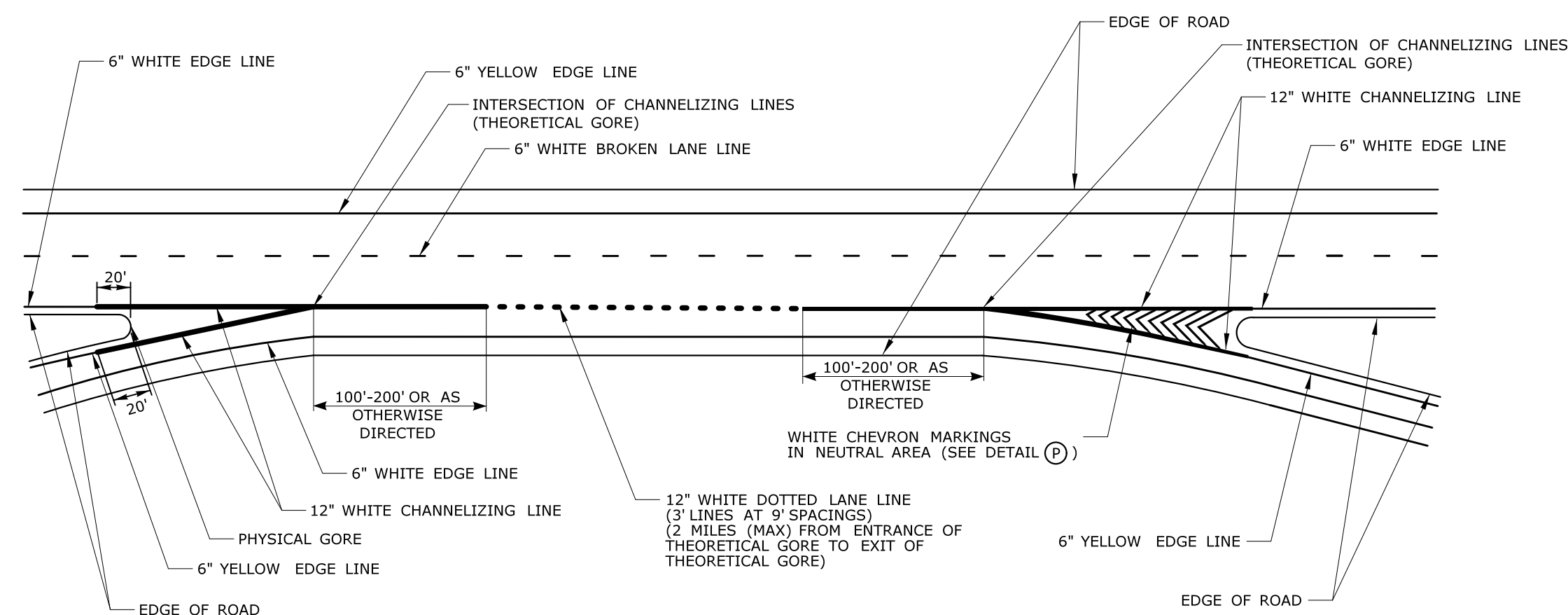
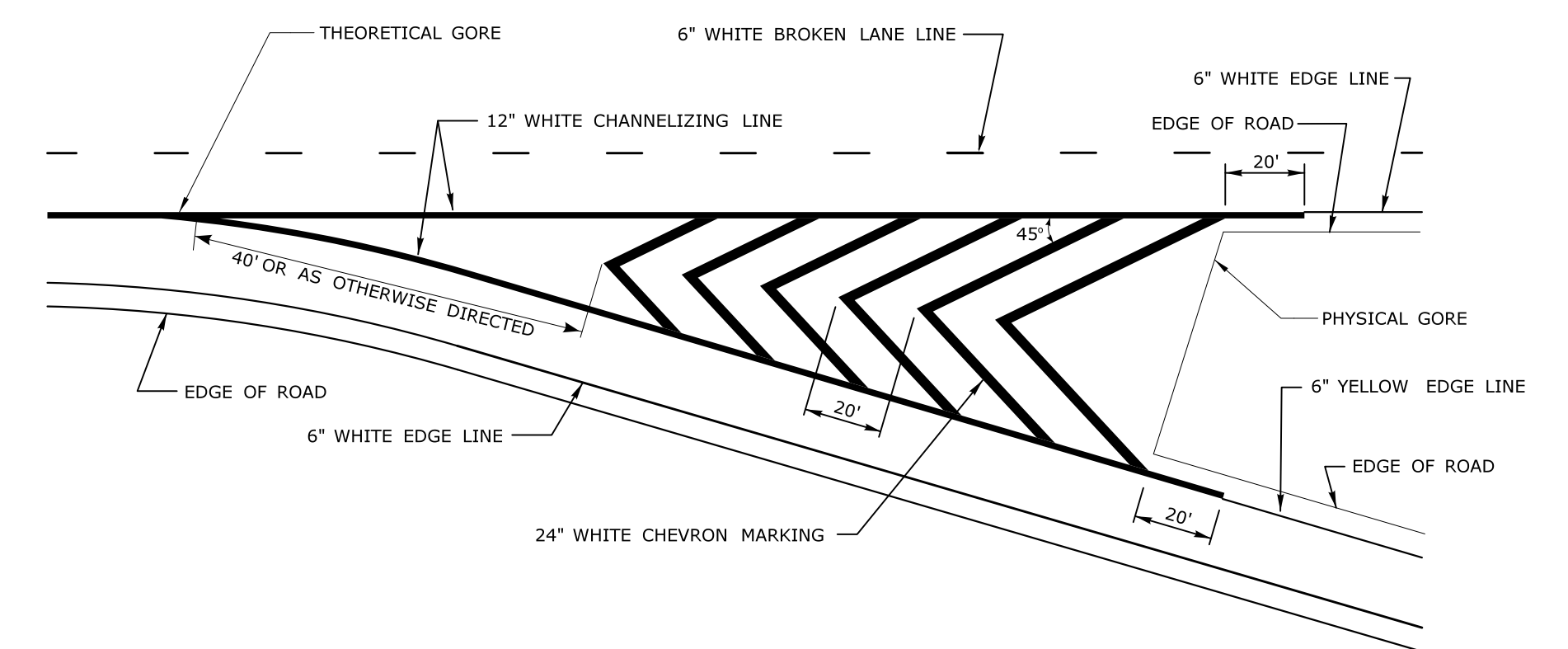
1. CONTACT THE ENGINEER TO REVISE DISTANCES IF NEEDED BASED ON FIELD CONDITIONS.
2. SEE STANDARD SHEET "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS" (SHEET #TR-1210.06) FOR DETAILS ① THRU ④.
3. SEE STANDARD SHEET "PAVEMENT MARKINGS FOR EXIT RAMP" (SHEET #TR-1210.07) FOR DETAILS ⑤ THRU ⑩.



NOTES:

CROSS HATCHING A SHOULDER ON AN EXPRESSWAY OR RAMP IS OPTIONAL AND SHOULD BE RESERVED TO SELECT LOCATIONS.

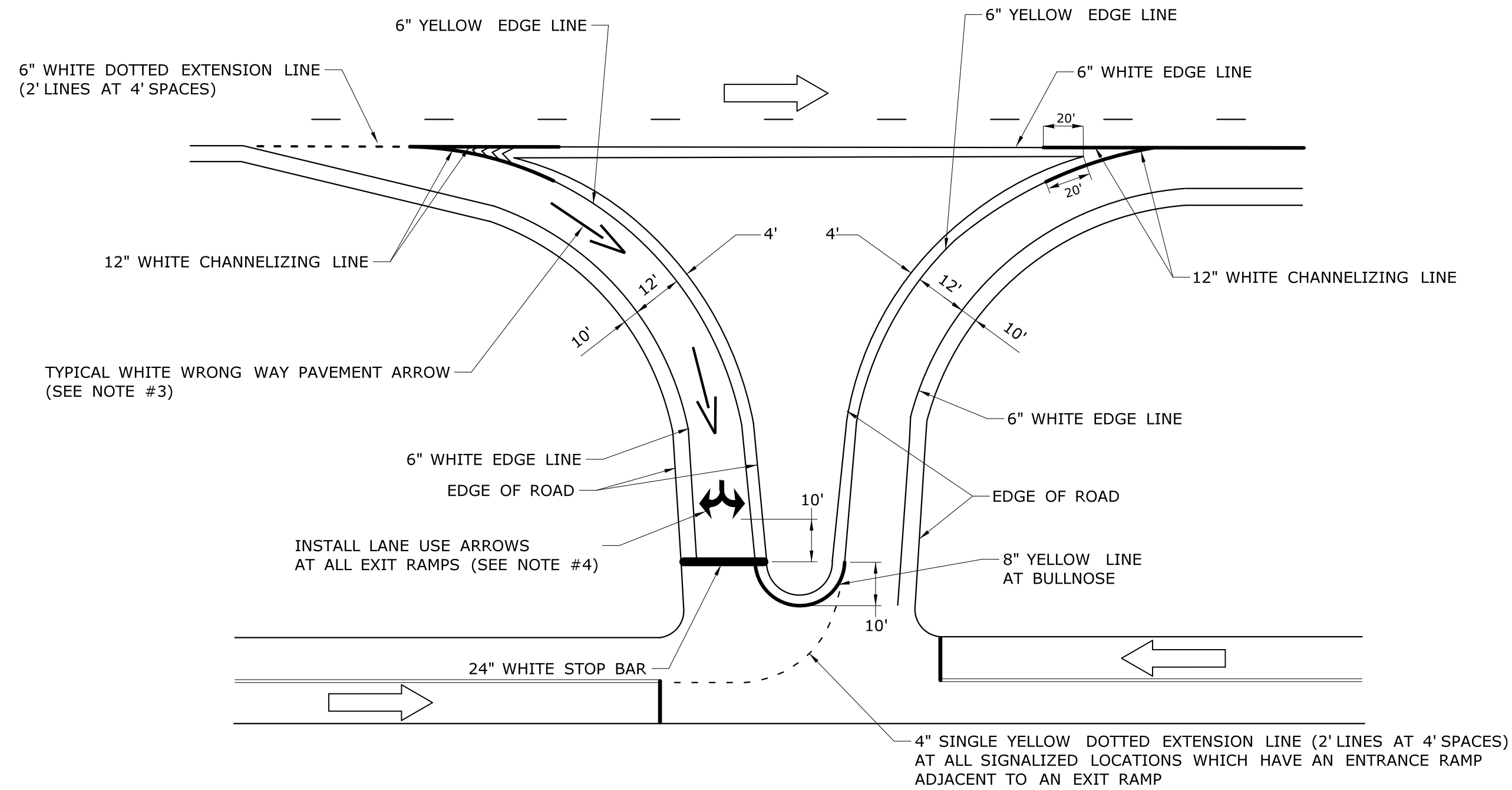
CROSS HATCH COLOR TO BE WHITE WHEN INSTALLED IN THE RIGHT-HAND SHOULDER OF AN EXPRESSWAY OR RAMP. CROSS HATCH COLOR TO BE YELLOW WHEN INSTALLED IN THE LEFT-HAND SHOULDER OF AN EXPRESSWAY OR RAMP.



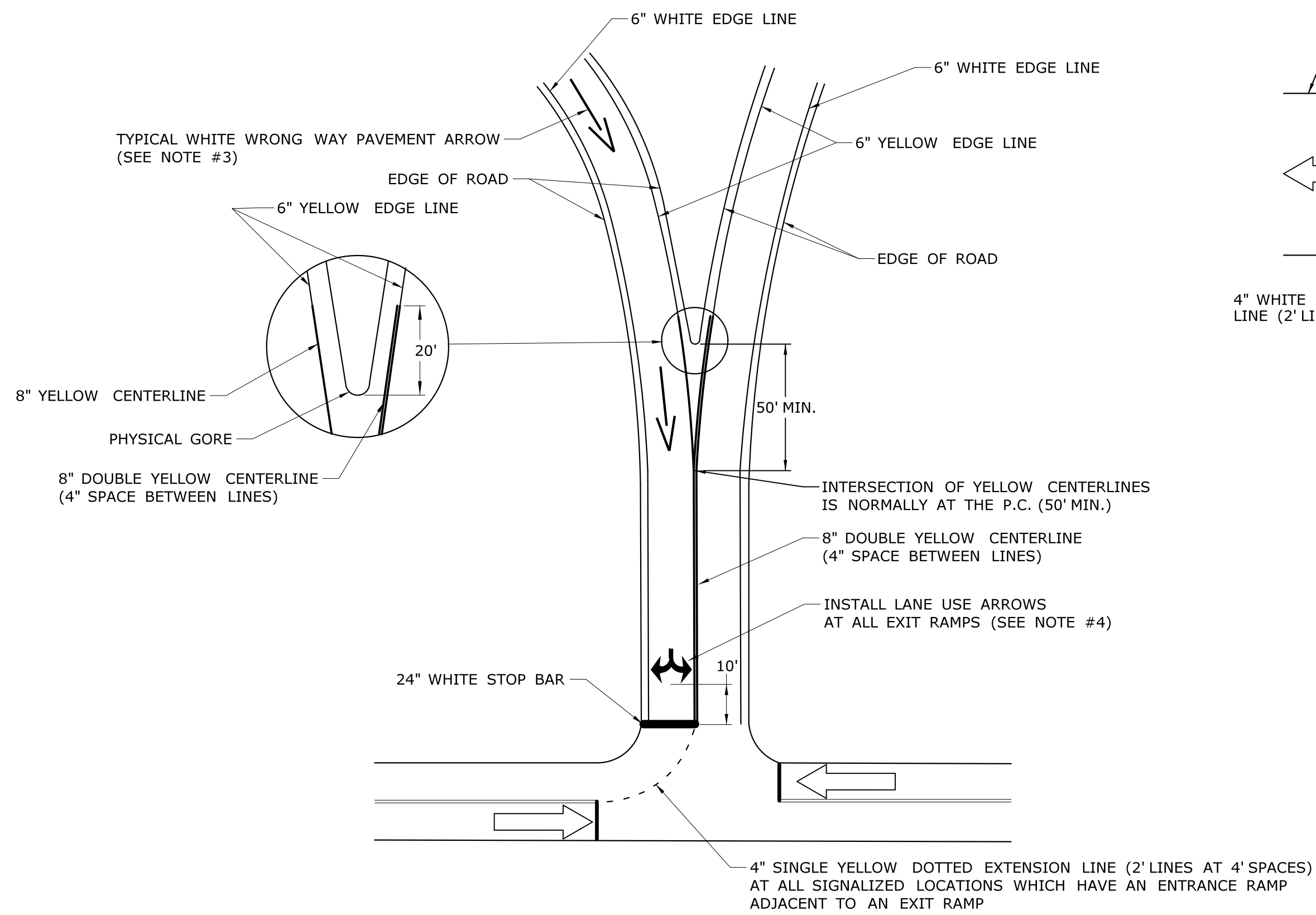
NOTES:

 DENOTES DIRECTION OF TRAVEL

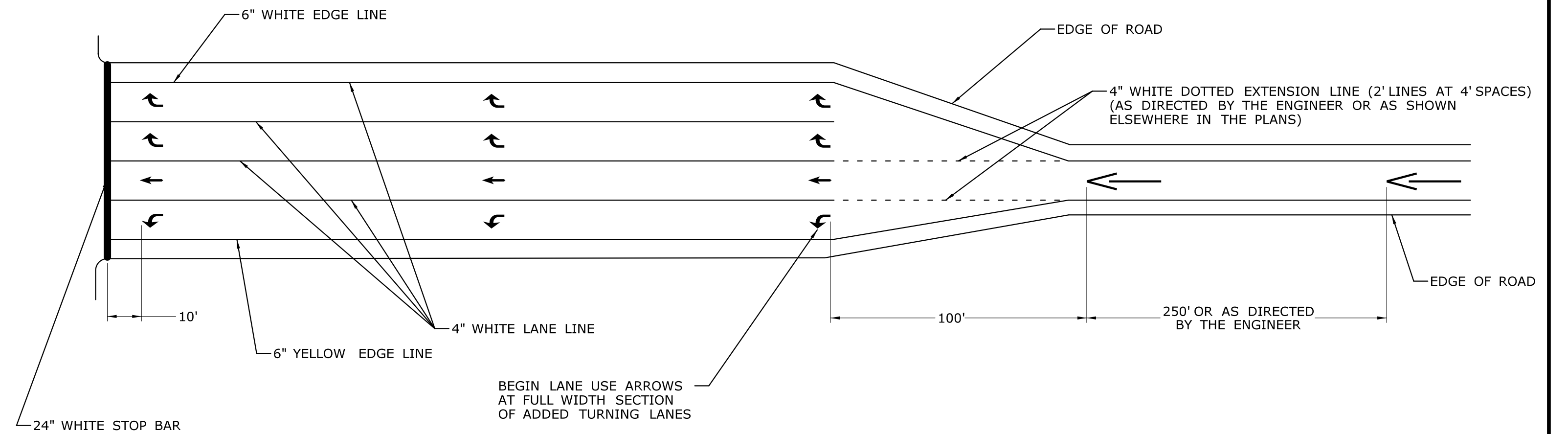
1. CONTACT THE ENGINEER TO REVISE DISTANCES IF NEEDED BASED ON FIELD CONDITIONS.
2. SEE STANDARD SHEET "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS" (SHEET # TR-1210.05) FOR DETAILS (A) THRU (I).
3. SEE STANDARD SHEET "PAVEMENT MARKINGS FOR EXIT RAMP" (SHEET # TR-1210.07) FOR DETAILS (R) THRU (U).



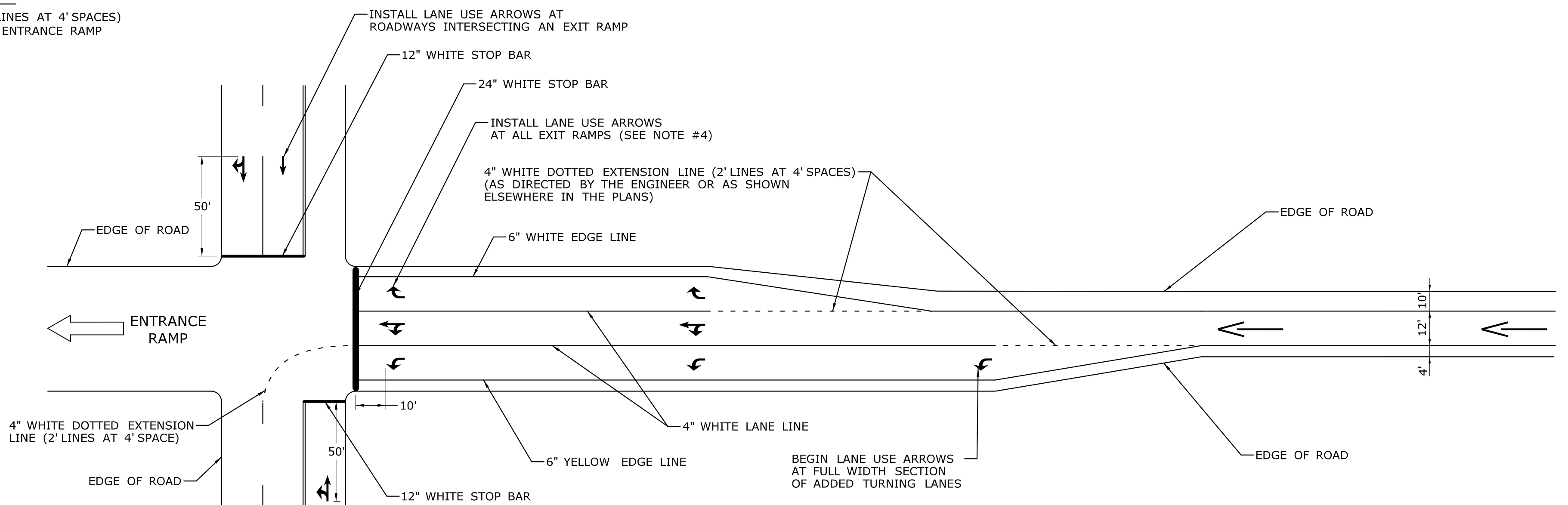
R TYPICAL RAMP TREATMENT ON ABUTTING RAMPs WITH MEDIAN



S TYPICAL RAMP TREATMENT ON ABUTTING RAMPs WITHOUT MEDIAN

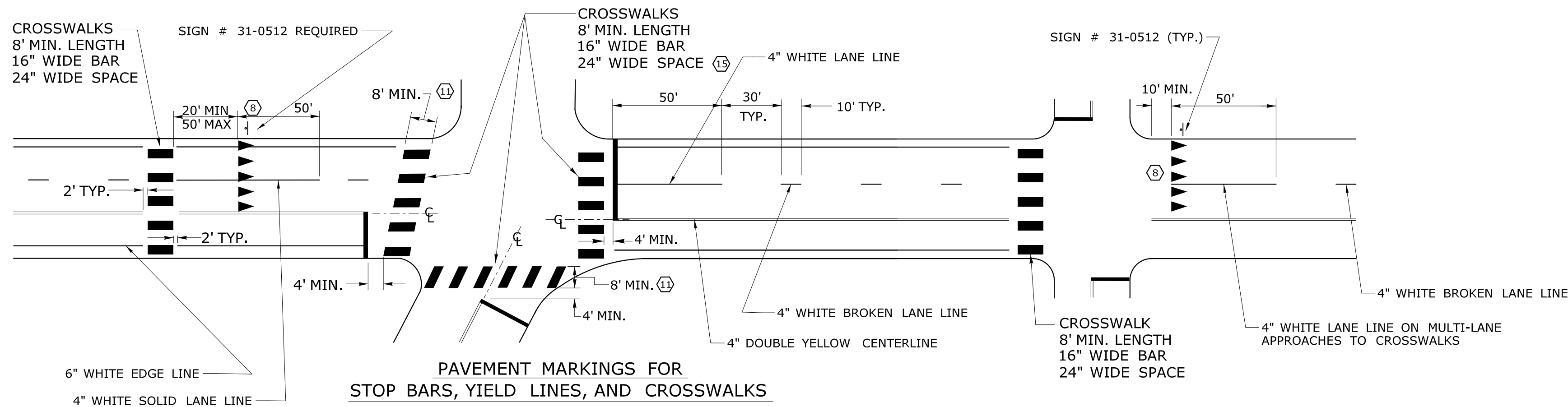
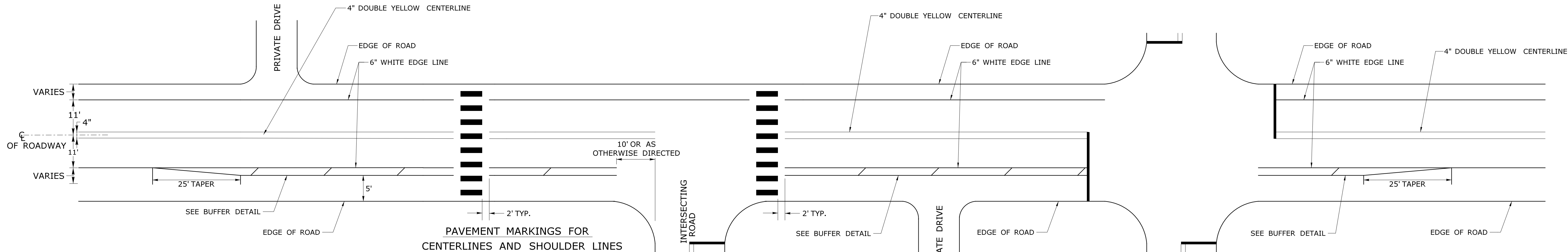


T TYPICAL RAMP TREATMENT FOR DOUBLE TURN LANES



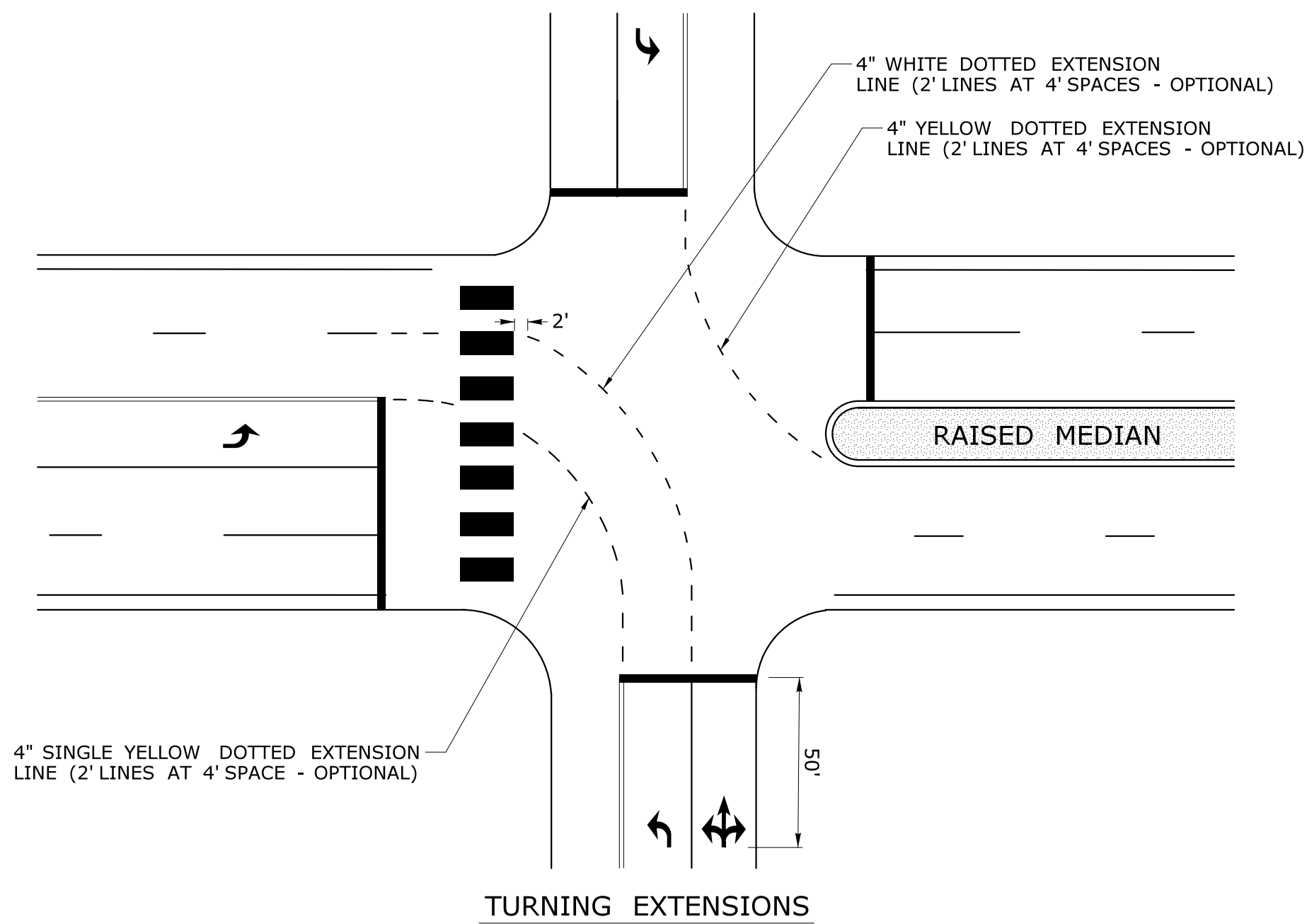
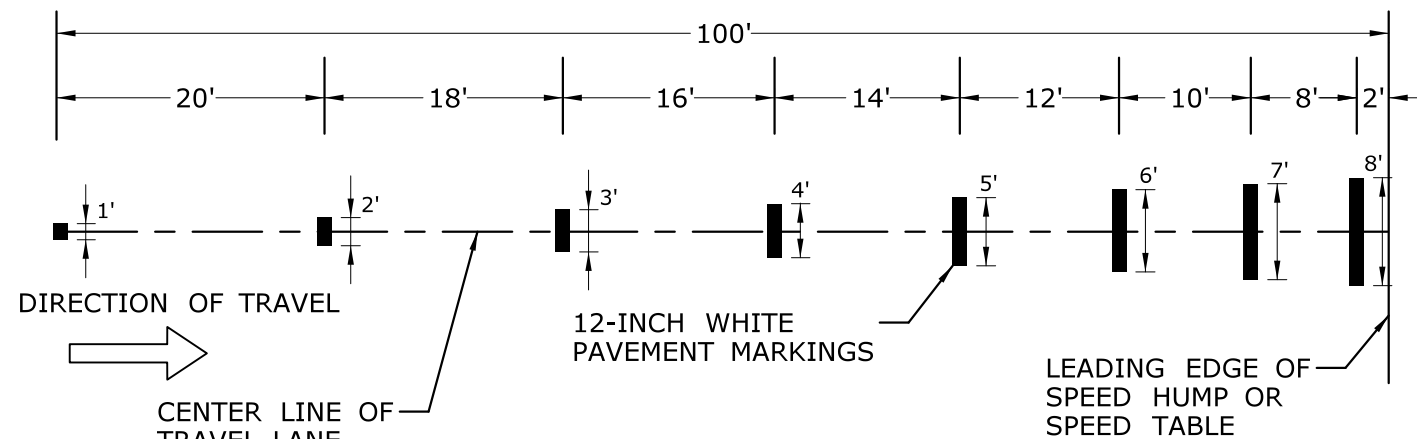
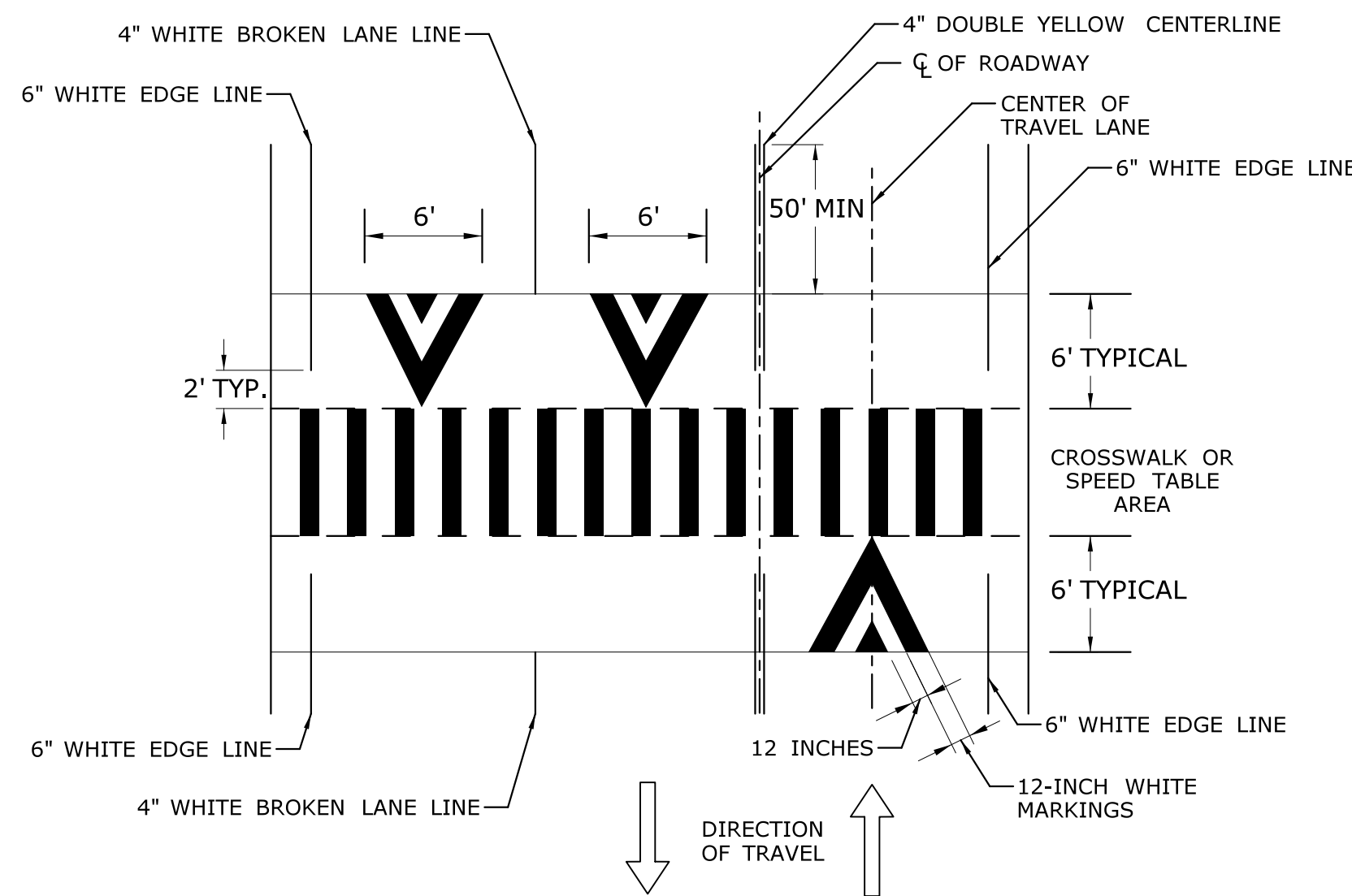
U TYPICAL RAMP TREATMENT FOR SHARED DOUBLE TURN LANES

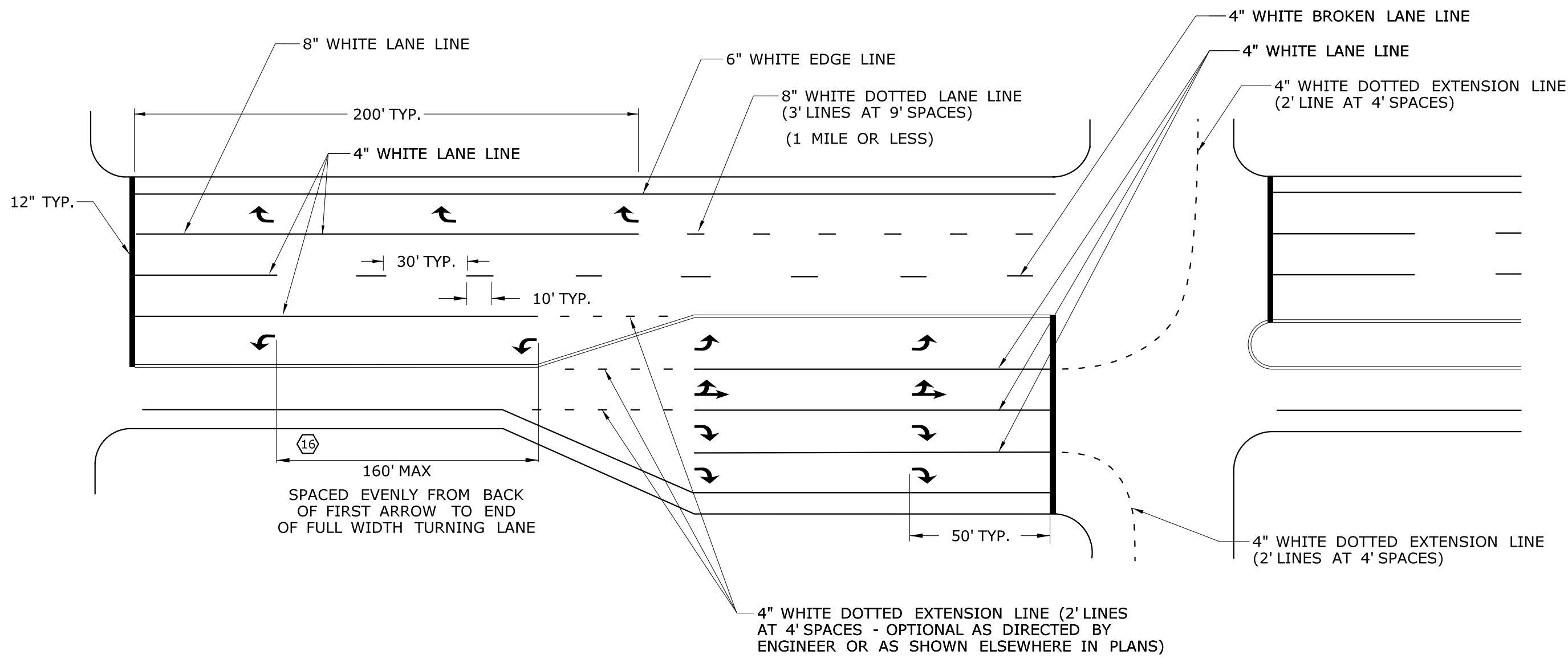
- NOTES :
- SEE DETAILS **J** AND **P** ON STANDARD SHEET "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS" (SHEET #TR-1210-06) FOR RELATIONSHIP OF DIRECTIONAL LINES AND NORMAL EDGE LINE TO EDGE OF TRAVELWAY.
 - TWO WHITE WRONG WAY PAVEMENT ARROWS SHOULD BE INSTALLED ON ALL EXIT RAMPs. THE FIRST SHALL BE INSTALLED 100' FROM THE LANE-USE ARROW(S), THE SECOND SHOULD BE 250'+ FROM THE FIRST WHERE SPACE PERMITS.
 - FOR EXIT RAMPs WITH MORE THAN ONE LANE, INSTALL LANE USE ARROWS IN EACH LANE. INSTALL SETs OF LANE USE ARROWS EQUALLY SPACED FROM THE STOP BAR TO THE BEGINNING OF THE MULTI LANE SECTION (160' MAX SPACING).
 - USE A 4" WHITE LANE LINE TO SEPARATE AN EXCLUSIVE LEFT (OR RIGHT) TURN LANE FROM AN ADJACENT THROUGH LANE OR AN ADJACENT SHARED THROUGH LANE.
 - USE A 4" WHITE LANE LINE TO SEPARATE DUAL EXCLUSIVE LEFT (OR RIGHT) TURN LANES.
 - DIMENSION FROM THE FIRST LANE USE ARROW TO STOP BAR IS FOR EXIT RAMPs ONLY.
 - SEE STANDARD SHEETS "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS" (SHEET #TR-1210-05 & SHEET #TR-1210-06) FOR DETAILS **A** THRU **Q**.



- NOTES:
STOP BARS AND YIELD LINES
1. STOP BARS AND YIELD LINES SHALL BE WHITE.
 2. STOP BARS SHALL BE 12" MIN. UNLESS OTHERWISE NOTED ON PLANS.
 3. STOP BARS TO BE PLACED A MINIMUM OF 4' IN ADVANCE OF THE NEAREST EDGE OF CROSSWALK AND SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY.
 4. IN THE ABSENCE OF A MARKED CROSSWALK THE STOP BAR SHOULD BE PLACED 90° THE CENTERLINE OF THE ROADWAY, AT THE DESIRED STOPPING POINT AT LEAST 5' AND NO MORE THAN 30' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.
 5. THE STOP SIGN SHOULD BE PLACED IN LINE WITH THE STOP BAR. HOWEVER, IF THE STOP SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO STOP, THE STOP BAR SHOULD BE PLACED AT THE STOPPING POINT.
 6. FOR STOP BARS AT RAMP SEE DETAILS "R", "S", "T", & "U" AND NOTES ON TRAFFIC STANDARD SHEET TR-1210 07 "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS EXIT RAMP".
 7. FOR YIELD LINE INSTALLATIONS, ONLY FULL TRIANGLES ARE TO BE INSTALLED.
 8. YIELD LINES SHALL ONLY BE INSTALLED ON UNCONTROLLED MULTI-LANE APPROACHES TO CROSSWALKS AND SHOULD BE LOCATED 20 TO 50 FEET IN ADVANCE OF THE NEAREST CROSSWALK LINE OR AS DIRECTED BY THE ENGINEER. WHERE A YIELD LINE EXISTS ON AN APPROACH TO A CROSSWALK, THE "YIELD TO PEDESTRIANS" SIGN IS REQUIRED.
 9. THE YIELD SIGN SHOULD BE PLACED IN LINE WITH A YIELD LINE. HOWEVER, IF THE YIELD SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO YIELD, THE YIELD LINE SHOULD BE PLACED AT THE YIELDING POINT.

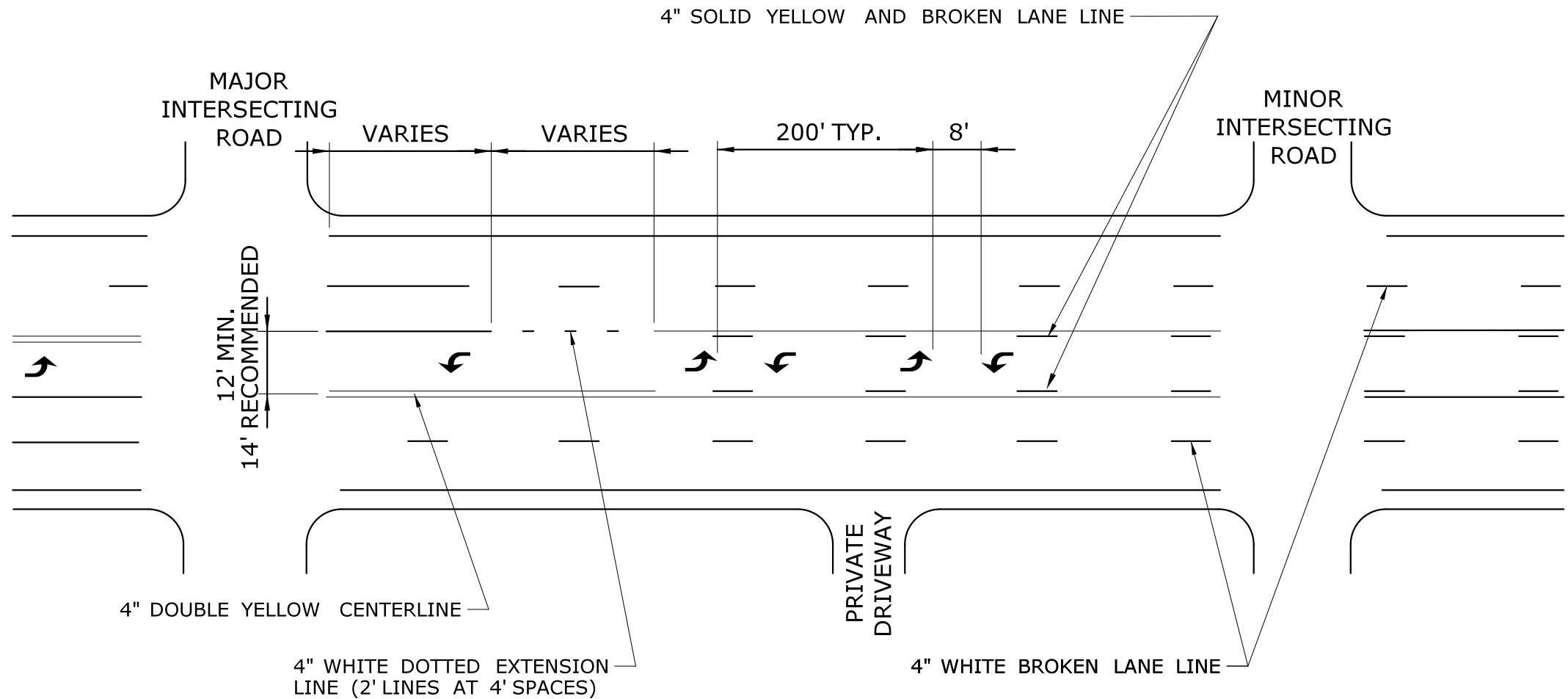
- CROSSWALKS
10. CROSSWALK MARKINGS SHALL BE WHITE.
 11. AT LOCATIONS WHERE THE CROSSWALK IS SKEWED, BARS TO BE PARALLEL TO ENDS OF BARS TO BE PARALLEL. THE LENGTH OF THE BARS WILL VARY DEPENDING ON THE ANGLE OF SKEW.
 12. BARS SHOULD BE NO CLOSER THAN 1' FROM EDGE OF ROAD.
 13. ONLY FULL LENGTH BARS ARE TO BE INSTALLED.
 14. DECORATIVE CROSSWALKS SHALL BE BANDED FROM CURB TO CURB WITH A MINIMUM 12" WIDE WHITE TRANSVERSE LINE ALONG EACH EDGE.
 15. 24" WIDE SPACE TO BE CENTERED ON YELLOW CENTERLINE.



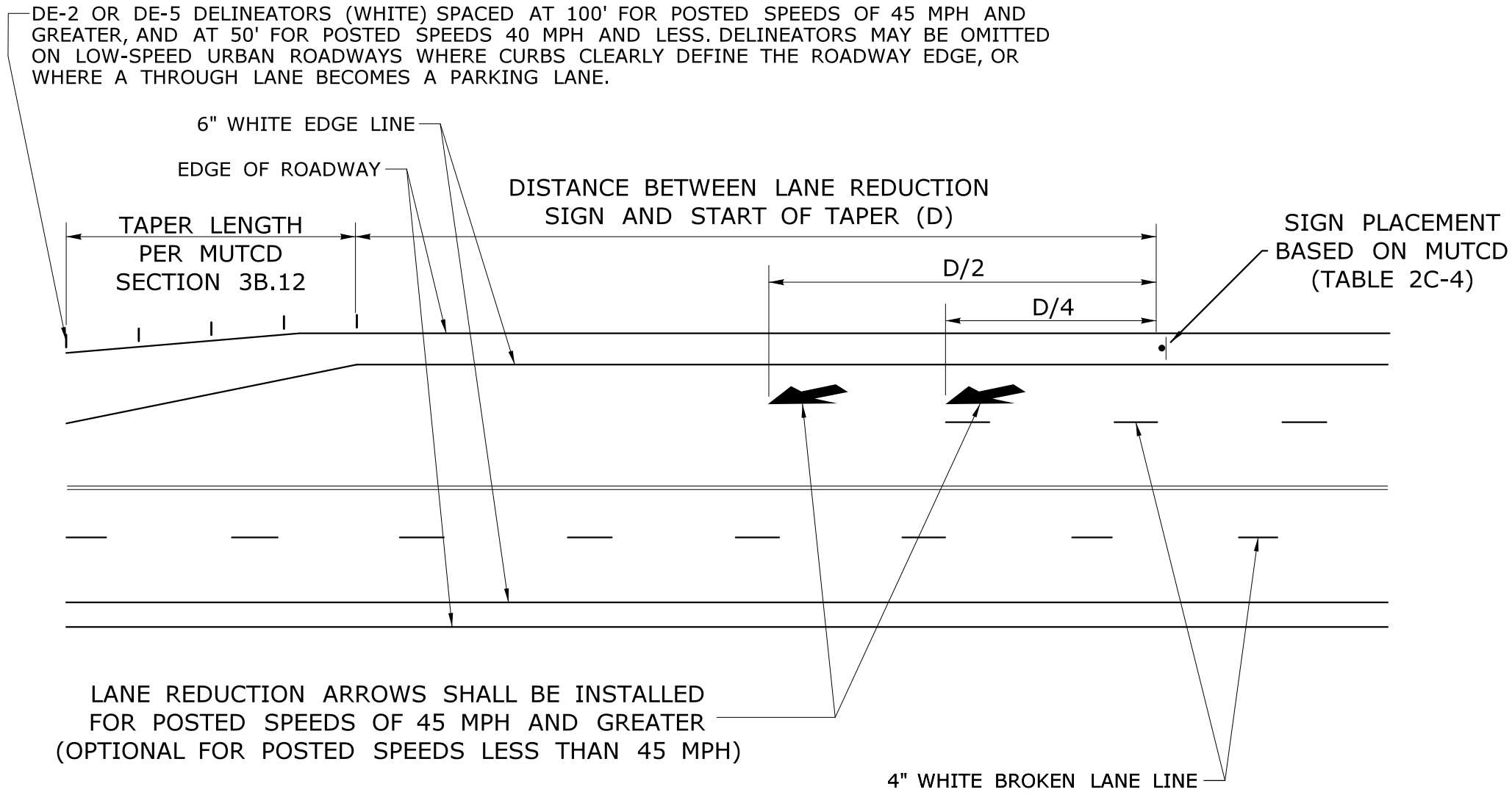


PAVEMENT MARKINGS FOR TURNING LANES
AND ROAD LANE-DROP

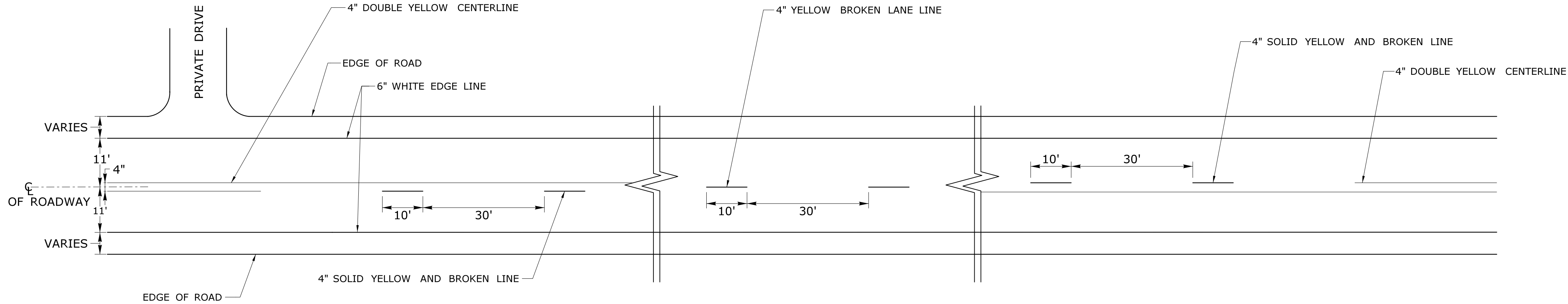
PAVEMENT MARKINGS FOR TURNING LANES
INSTALL AT LEAST TWO ARROWS PER LANE WHERE STORAGE LENGTH IS GREATER THAN 150 FEET.



TWO-WAY LEFT-TURN MARKINGS APPLICATIONS

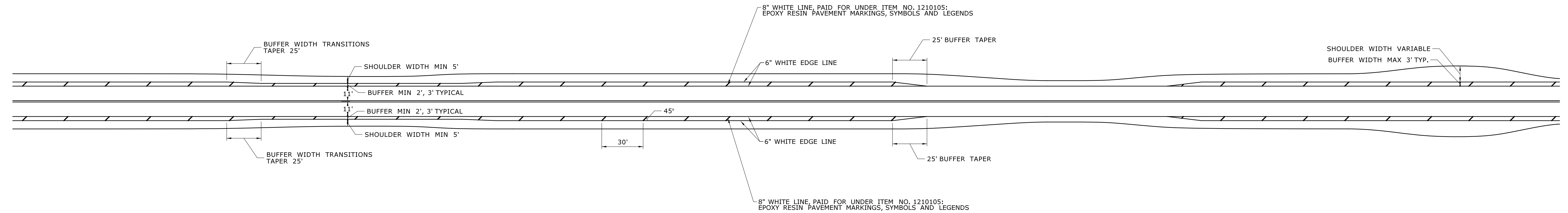


LANE REDUCTION TRANSITIONS

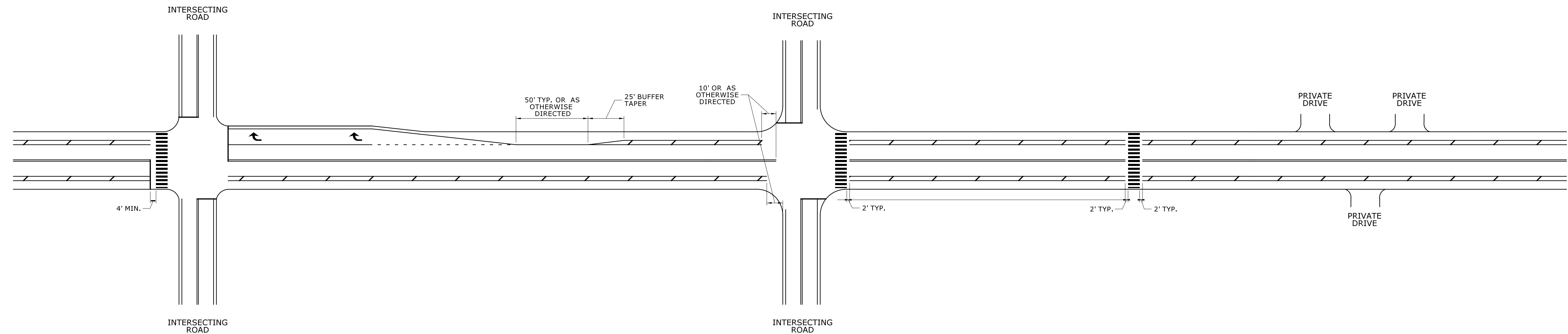


PAVEMENT MARKINGS
FOR PASSING ZONES

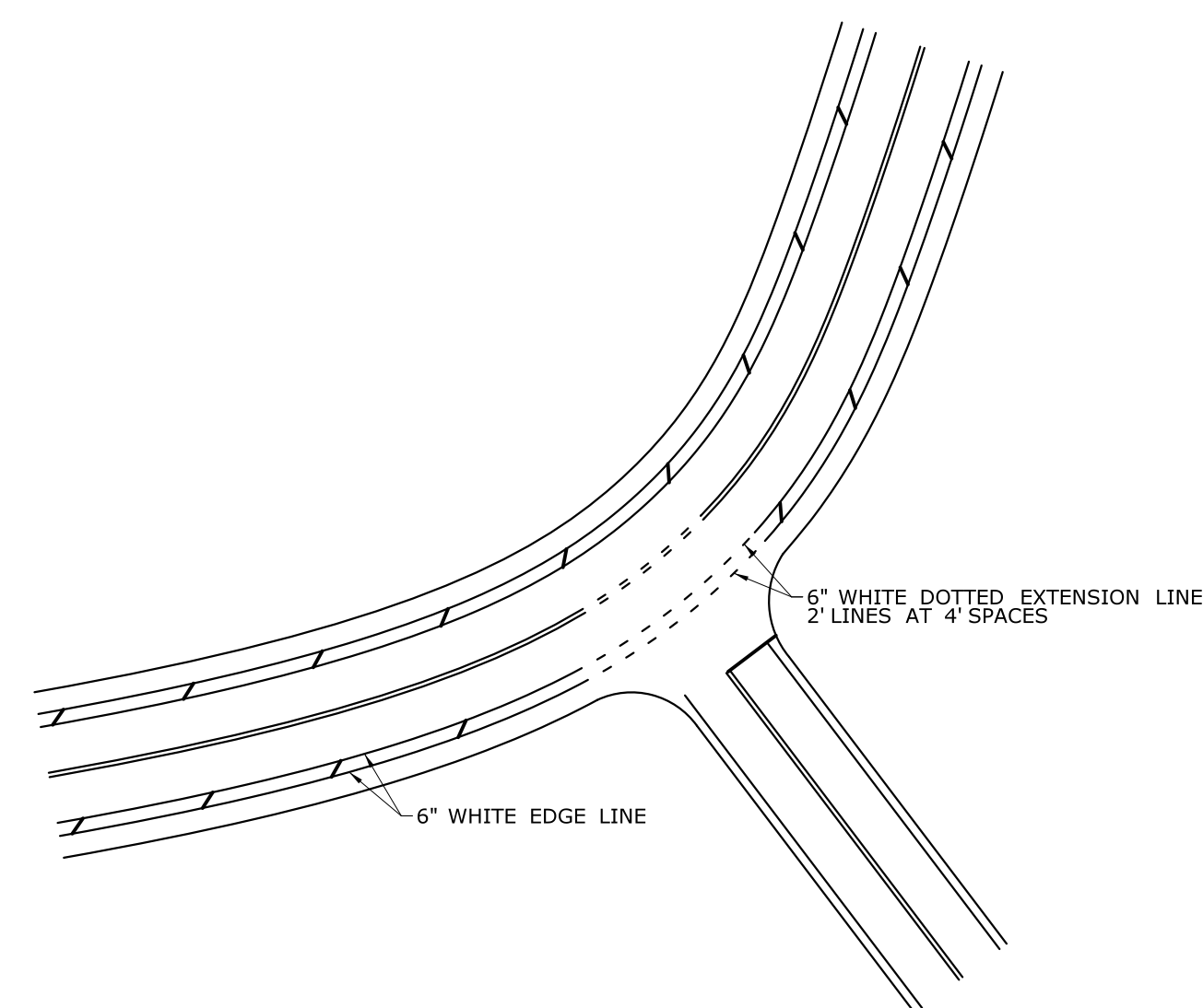
TYPICAL BUFFERED SHOULDER DETAIL



TYPICAL BUFFERED SHOULDER WITH INTERSECTING ROADWAYS



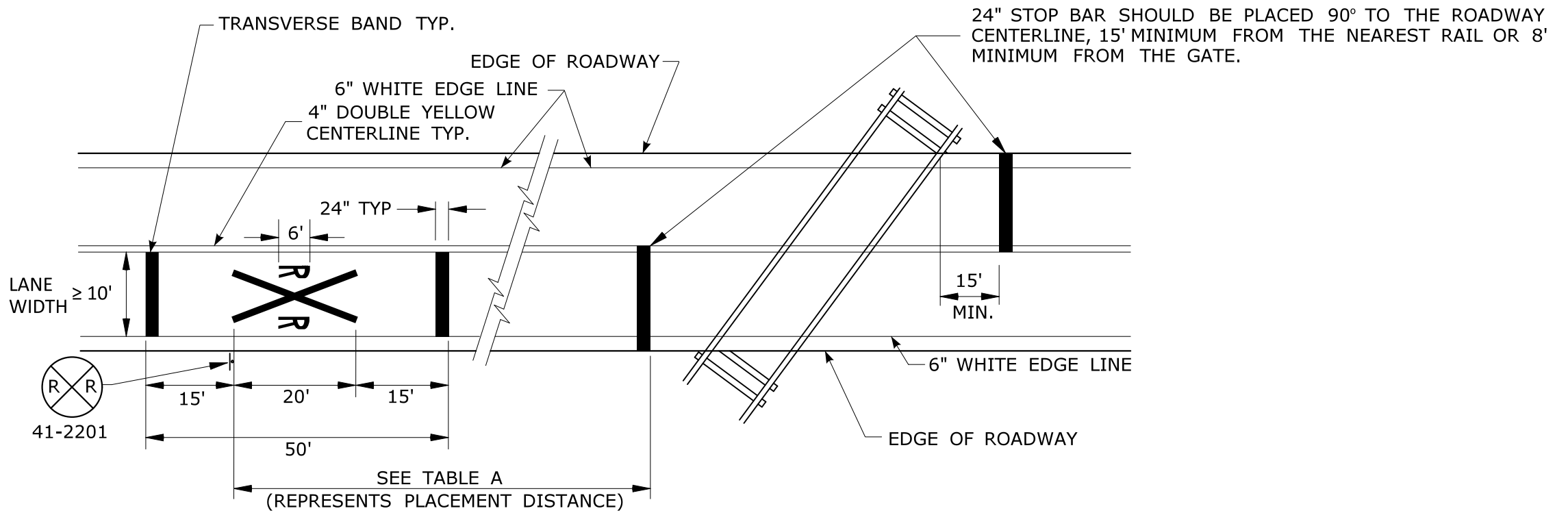
TYPICAL BUFFERED SHOULDER WITH INTERSECTING ROADWAY AT CURVE



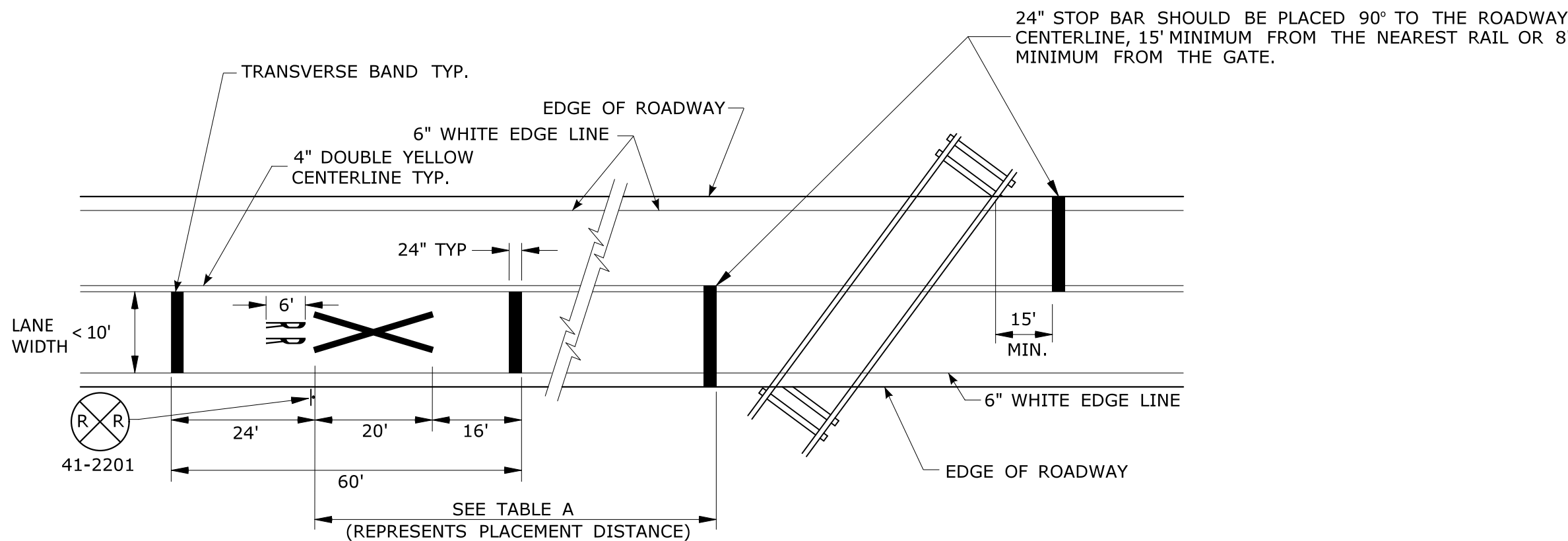
NOTES:

1. BUFFERED SHOULDERS SHOULD BE USED ON ROADWAYS WITH SPEED LIMITS ≤ 45 MPH.
2. REFER TO GUIDE SHEET TR-1210_04 FOR PAVEMENT MARKING LINE AND BUFFER DETAILS.

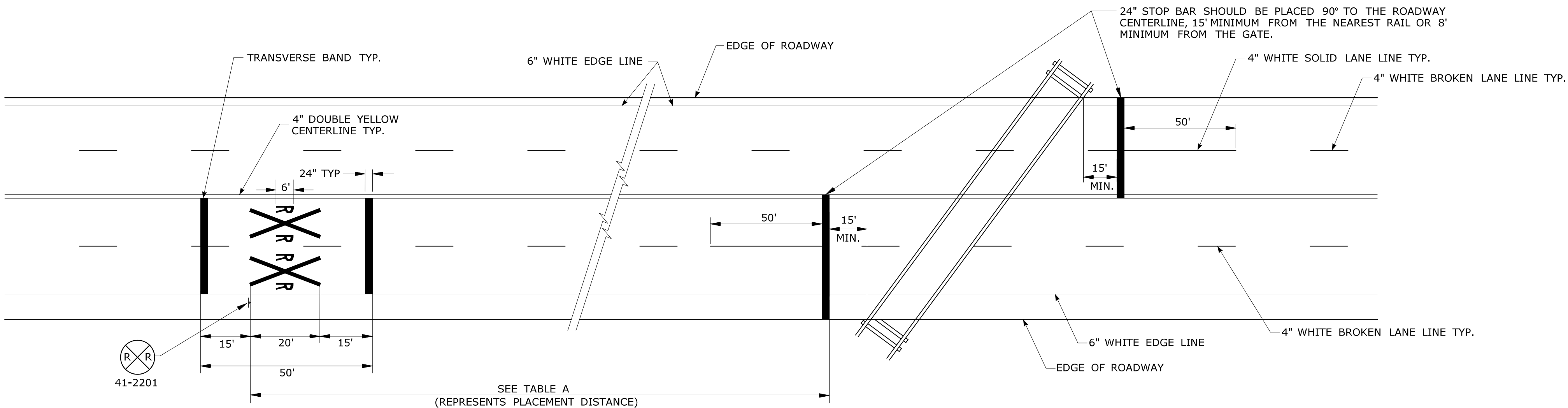
TYPICAL RAILROAD GRADE CROSSING DETAIL (LANE WIDTH ≥ 10')



TYPICAL RAILROAD GRADE CROSSING DETAIL (LANE WIDTH < 10')

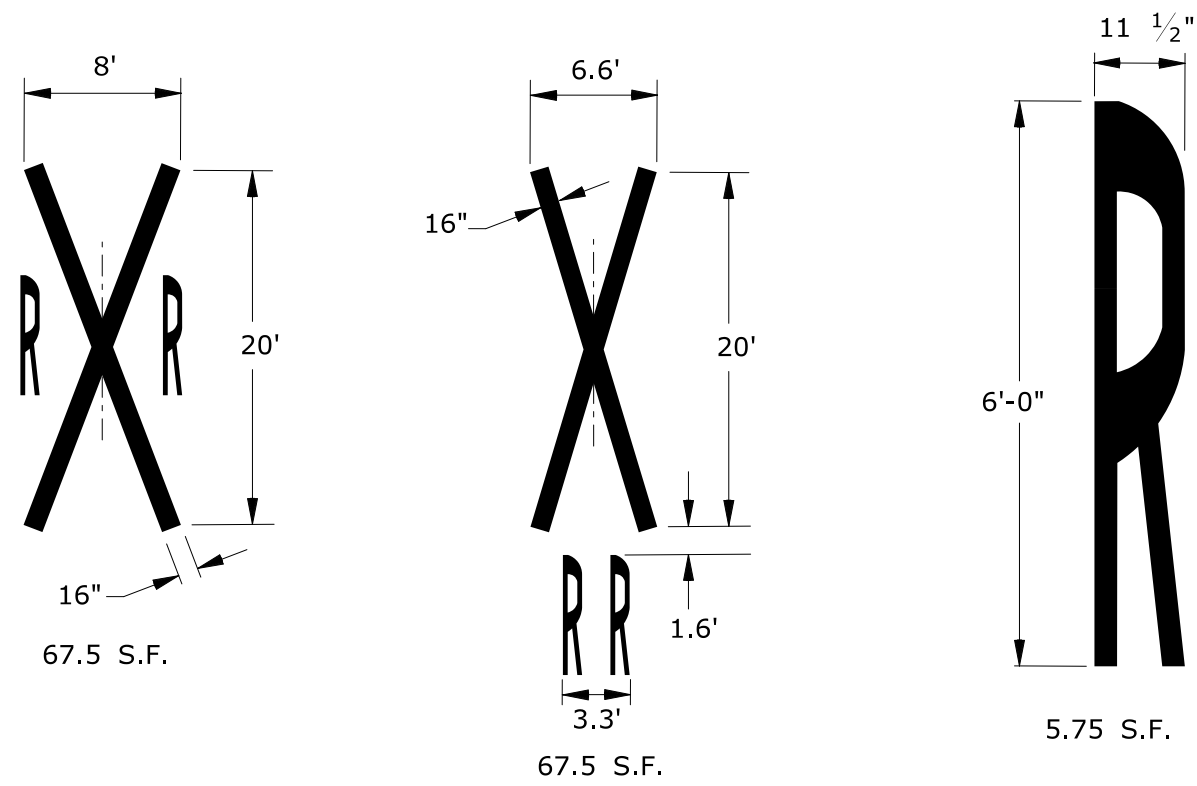


TYPICAL RAILROAD GRADE CROSSING DETAIL (MULTIPLE LANES)



NOTES:

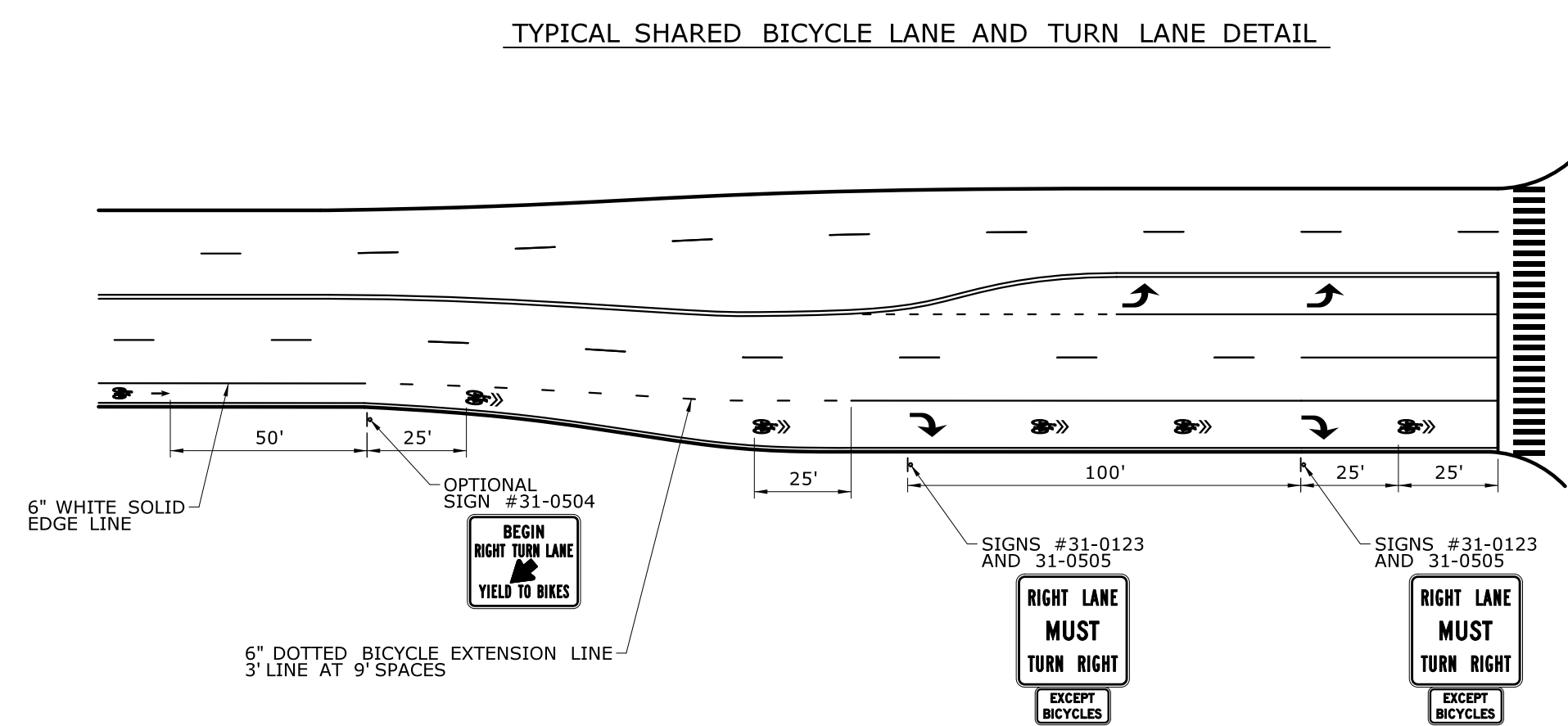
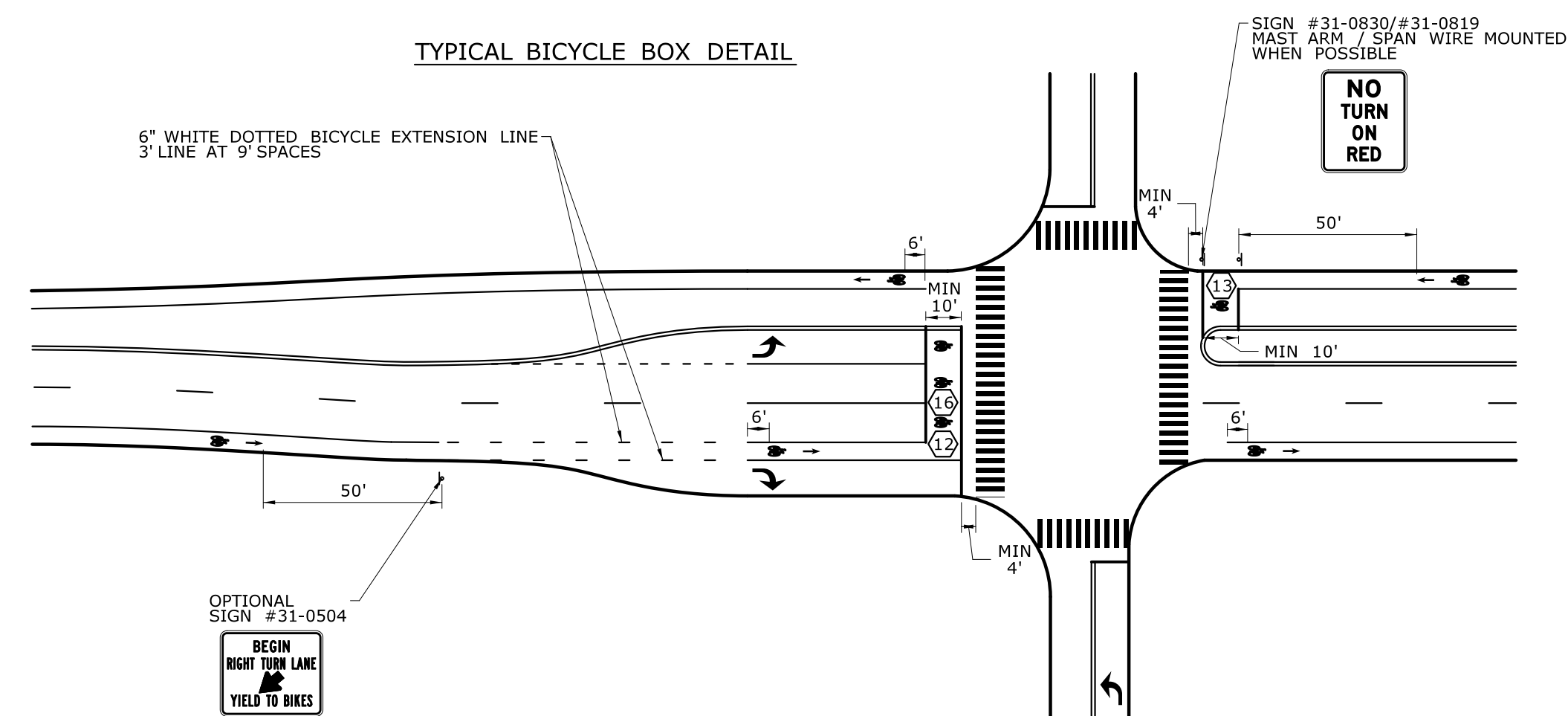
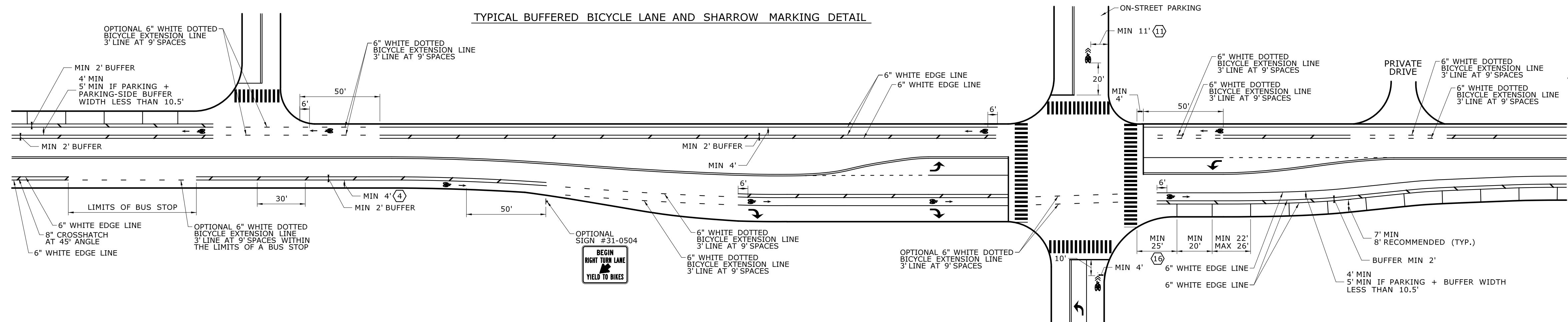
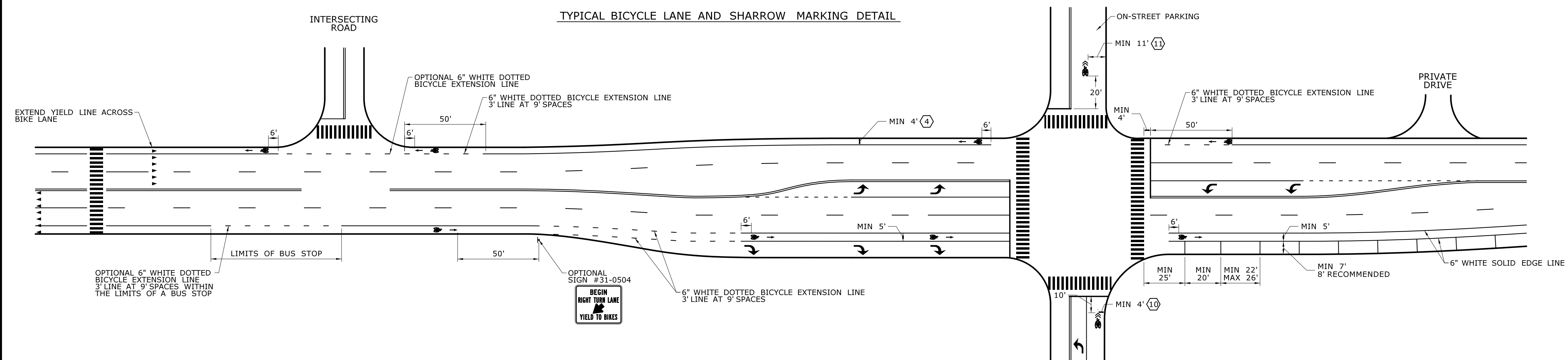
- RAILROAD MARKINGS SHALL BE WHITE.
- ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS THE APPROACH LANES AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE. SEE DETAIL "TYPICAL RAILROAD GRADE CROSSING DETAIL (MULTIPLE LANES)".
- LONGITUDINAL PAVEMENT MARKINGS SHALL BE INSTALLED CONTINUOUSLY ACROSS THE TRACKS.
- IF A YIELD AHEAD OR A STOP AHEAD SIGN IS INSTALLED ON THE APPROACH TO THE CROSSING, THE 41-2201 SIGN SHALL BE INSTALLED UPSTREAM FROM THE YIELD AHEAD OR STOP AHEAD SIGN.



NOTE: THE AREA (S.F.) OF PAVEMENT MARKING SYMBOLS IS APPROXIMATE.

TABLE A	
POSTED OR 85TH PERCENTILE SPEED M.P.H.	MINIMUM DISTANCE ⁽¹⁾ FT.
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645

⁽¹⁾ ADJUST DISTANCE AS NEEDED FOR CROSSINGS LOCATED LESS THAN THE MINIMUM DISTANCE FROM A PARALLEL ROADWAY.



- NOTES:

1. IF USED, BICYCLE PAVEMENT MARKINGS AND GREEN PAVEMENT TO BE OWNED AND MAINTAINED BY THE MUNICIPALITY.
- ④ 5 FEET WIDE BICYCLE LANES SHALL BE USED IF IMMEDIATELY ADJACENT TO A CURB, GUIDE RAIL, OR OTHER VERTICAL SURFACES.
5. PAVEMENT MARKINGS USED ON BIKEWAYS SHALL BE RETROREFLECTIVE.
6. BICYCLE LANE MARKINGS SHOULD BE SPACED AT INTERVALS NO GREATER THAN 500 FT.
7. IF USED, SHARED LANE MARKINGS SHOULD BE SPACED AT INTERVALS NO GREATER THAN 250 FT.
8. SHARED LANE MARKINGS SHOULD NOT BE PLACED ON ROADWAYS THAT HAVE A SPEED LIMIT ABOVE 35 MPH, EXCEPT IN SHARED TURN LANES.
9. AREA OF PAVEMENT MARKING SYMBOLS AS INDICATED IS APPROXIMATE.
- ⑩ ON STREETS WITHOUT ON-STREET PARKING AND WITH OUTSIDE TRAVEL LANES LESS THAN 14 FEET WIDE, THE CENTERS OF THE SHARED LANE MARKINGS SHOULD BE AT LEAST 4 FEET FROM THE FACE OF THE CURB, OR FROM THE EDGE OF THE PAVEMENT WHERE THERE IS NO CURB.
- ⑪ ON STREETS WITH ON-STREET PARKING THE CENTERS OF THE SHARED LANE MARKINGS SHOULD BE AT LEAST 11 FEET FROM THE FACE OF THE CURB, OR FROM THE EDGE OF THE PAVEMENT WHERE THERE IS NO CURB.
- ⑫ COUNTDOWN PEDESTRIAN SIGNAL HEADS ARE REQUIRED FOR BICYCLE BOXES LOCATED ACROSS MULTILANE APPROACHES. THE PEDESTRIAN CHANGE INTERVAL SHALL BE DISPLAYED WITHOUT THE NEED FOR ACTIVATION.
- ⑬ TURNS ON RED SHALL BE PROHIBITED FROM ANY LANE WHERE A BICYCLE BOX IS PROVIDED.
14. REFER TO GUIDE SHEET TR-1210.04 FOR PAVEMENT MARKING LINE DETAILS.
- 15 ONE BIKE SYMBOL PER BIKE BOX REQUIRED. ONE BIKE SYMBOL PER LANE RECOMMENDED.
- ⑮ REDUCTION TO 10' ALLOWED FOR INTERSECTIONS ON CITY OF NEW HAVEN OWNED ROADS WITH CURB BUMP-OUTS, REFER TO CONNECTICUT STATE STATUTE SECTION 14-251.

