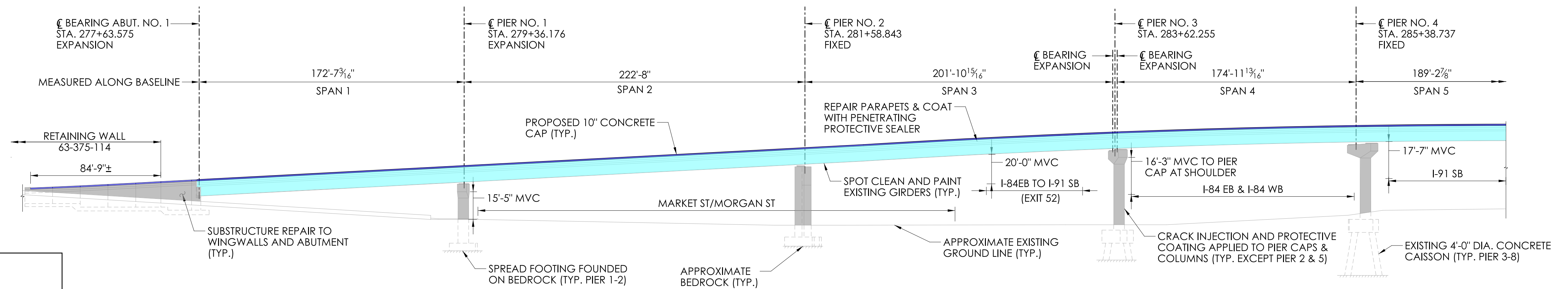


GENERAL PLAN

SCALE: 1"=40'



ELEVATION

SCALE: 1"=40'

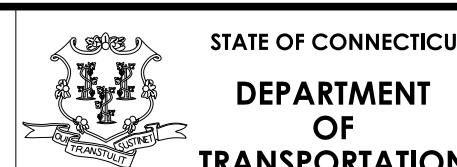
LEGEND:

	PROPOSED BRIDGE WEARING SURFACE
	PROPOSED APPROACH WORK
	PROPOSED CONCRETE PARAPET CAP
	PROPOSED CONCRETE BARRIER TRANSITION
	PROPOSED DECK JOINT
	RECONSTRUCTED END BLOCK TRANSITION
	PROPOSED IMPACT ATTENUATION SYSTEM
	PROPOSED METAL BEAM RAIL
	EXISTING SUBSTRUCTURE TO BE REPAIRED
	SUPERSTRUCTURE REHABILITATION

REV.	DATE	REVISION DESCRIPTION

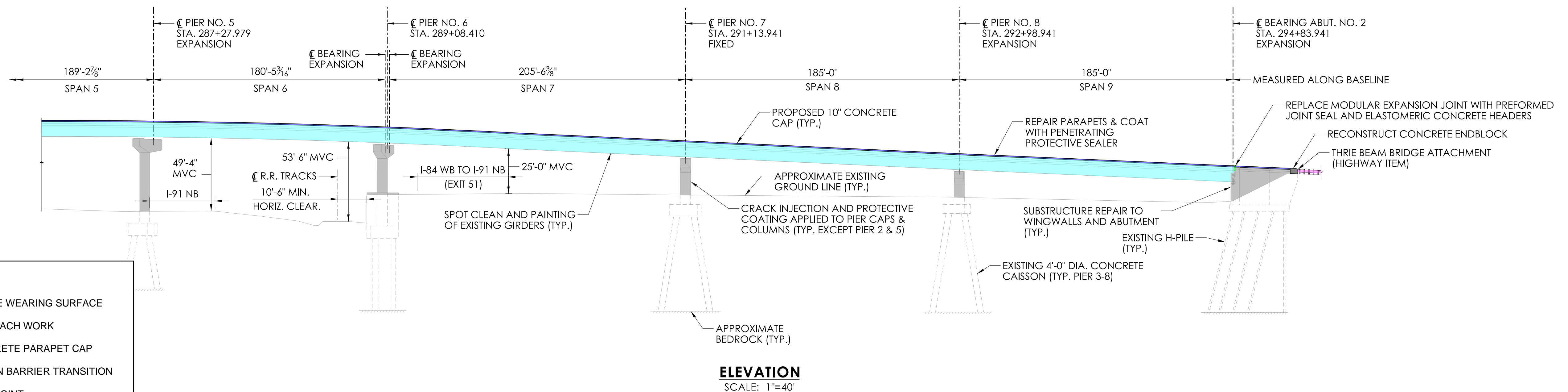
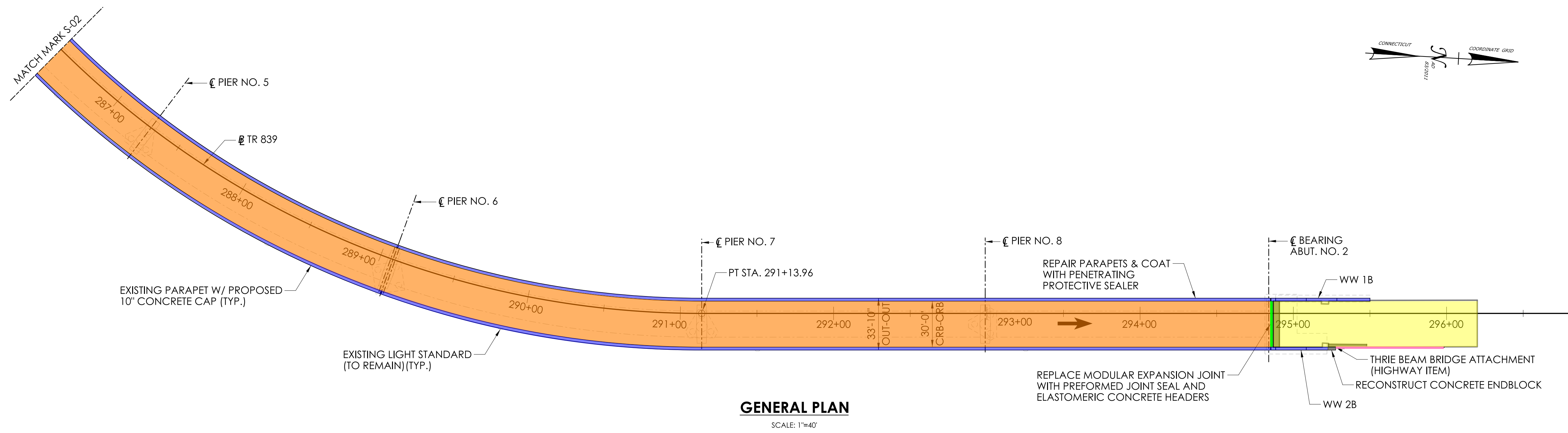
REHABILITATION STUDY REPORT

DESIGNER/DRAFTER: DEH CHECKED BY: RRC



PROJECT NUMBER: 0063-0731
 PROJECT DESCRIPTION: REHABILITATION OF BRIDGE NO. 05868 CARRYING I-84 EB TR 839 TO I-91 NB
 TOWN(S): HARTFORD
 DRAWING TITLE: GENERAL PLAN & ELEVATION I

DRAWING NO. S-02
 SHEET NO.



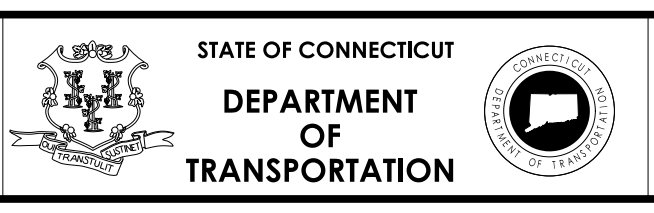
LEGEND:

	PROPOSED BRIDGE WEARING SURFACE
	PROPOSED APPROACH WORK
	PROPOSED CONCRETE PARAPET CAP
	PROPOSED MEDIAN BARRIER TRANSITION
	PROPOSED DECK JOINT
	RECONSTRUCTED END BLOCK TRANSITION
	PROPOSED IMPACT ATTENUATION SYSTEM
	PROPOSED METAL BEAM RAIL
	EXISTING SUBSTRUCTURE TO BE REPAIRED
	SUPERSTRUCTURE REHABILITATION

REV.	DATE	REVISION DESCRIPTION

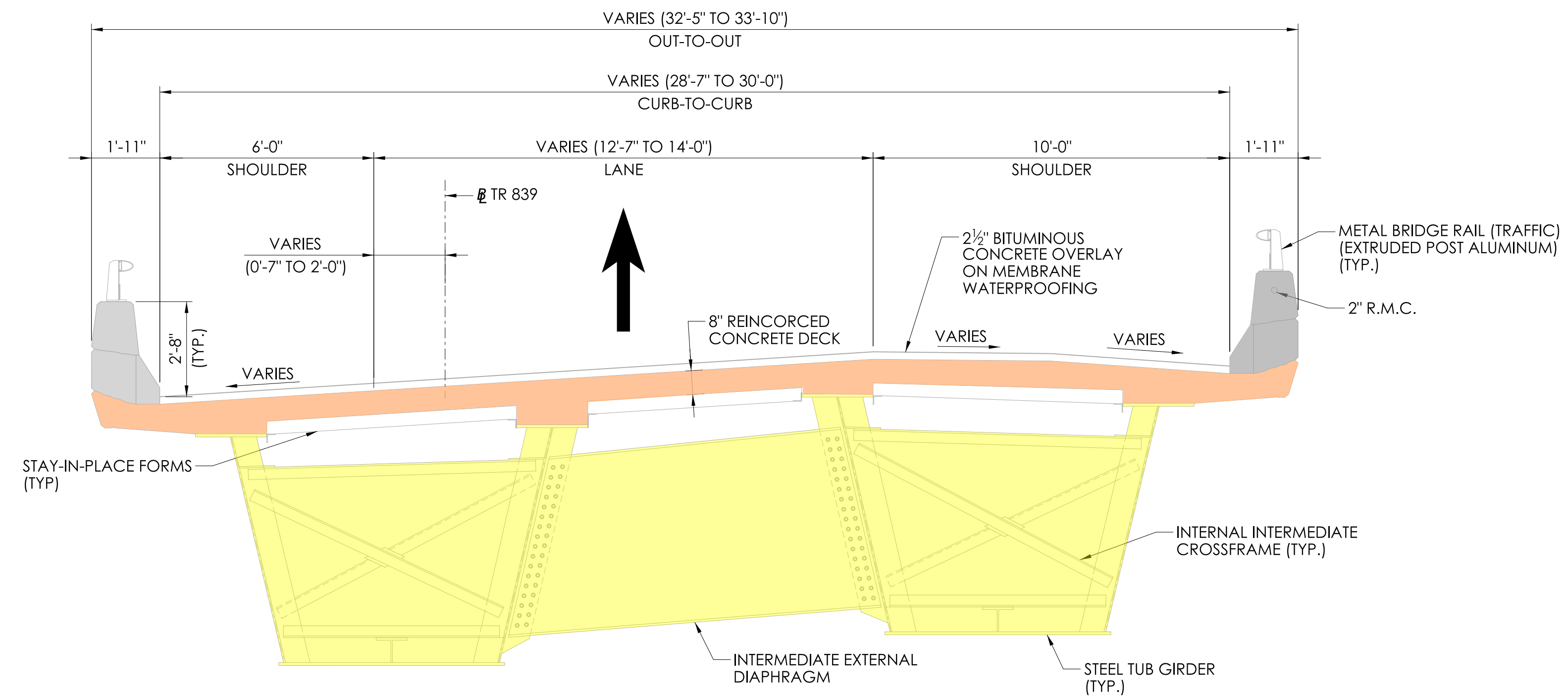
DESIGNER/DRAFTER: DEH CHECKED BY: RRC

SIGNATURE/BLOCK:

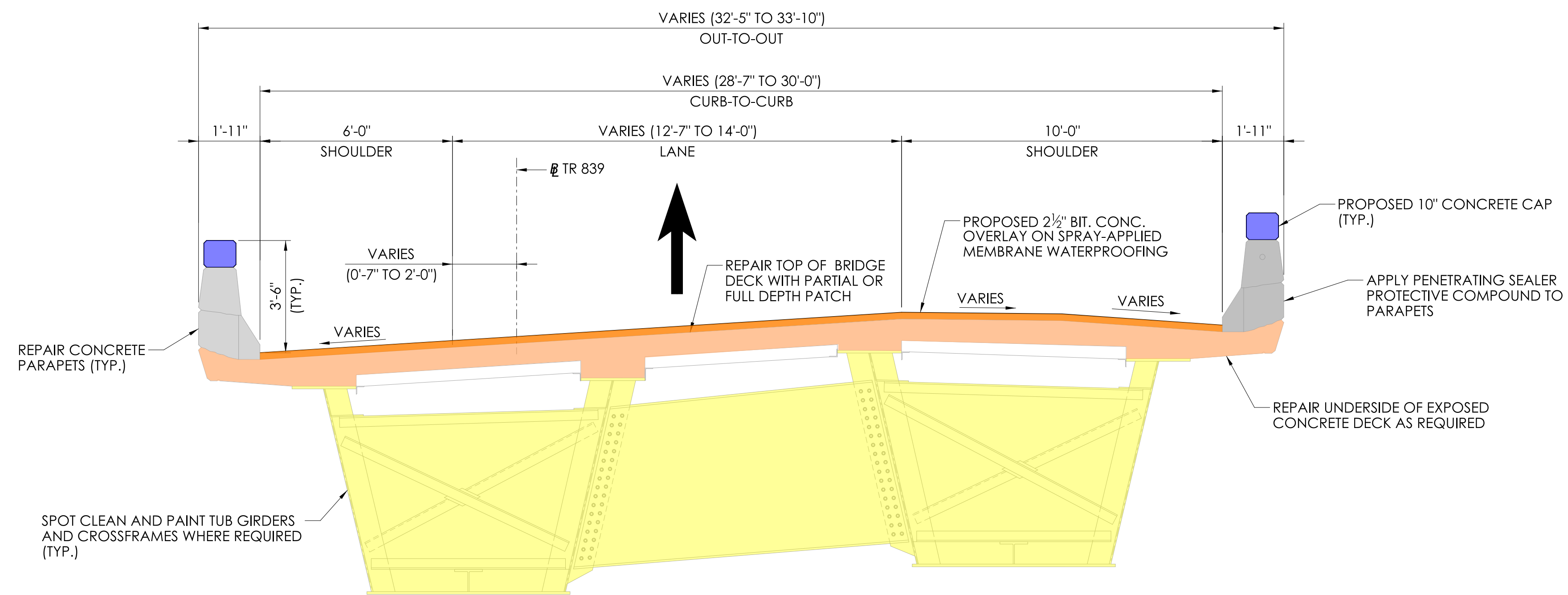


PROJECT NUMBER: 0063-0731
 PROJECT DESCRIPTION: REHABILITATION OF BRIDGE NO. 05868 CARRYING I-84 EB TR 839 TO I-91 NB
 TOWN(S): HARTFORD
 DRAWING TITLE: GENERAL PLAN & ELEVATION II

DRAWING NO. S-03
 SHEET NO.



EXISTING TYPICAL SECTION
SCALE: 3/8"=1'-0"



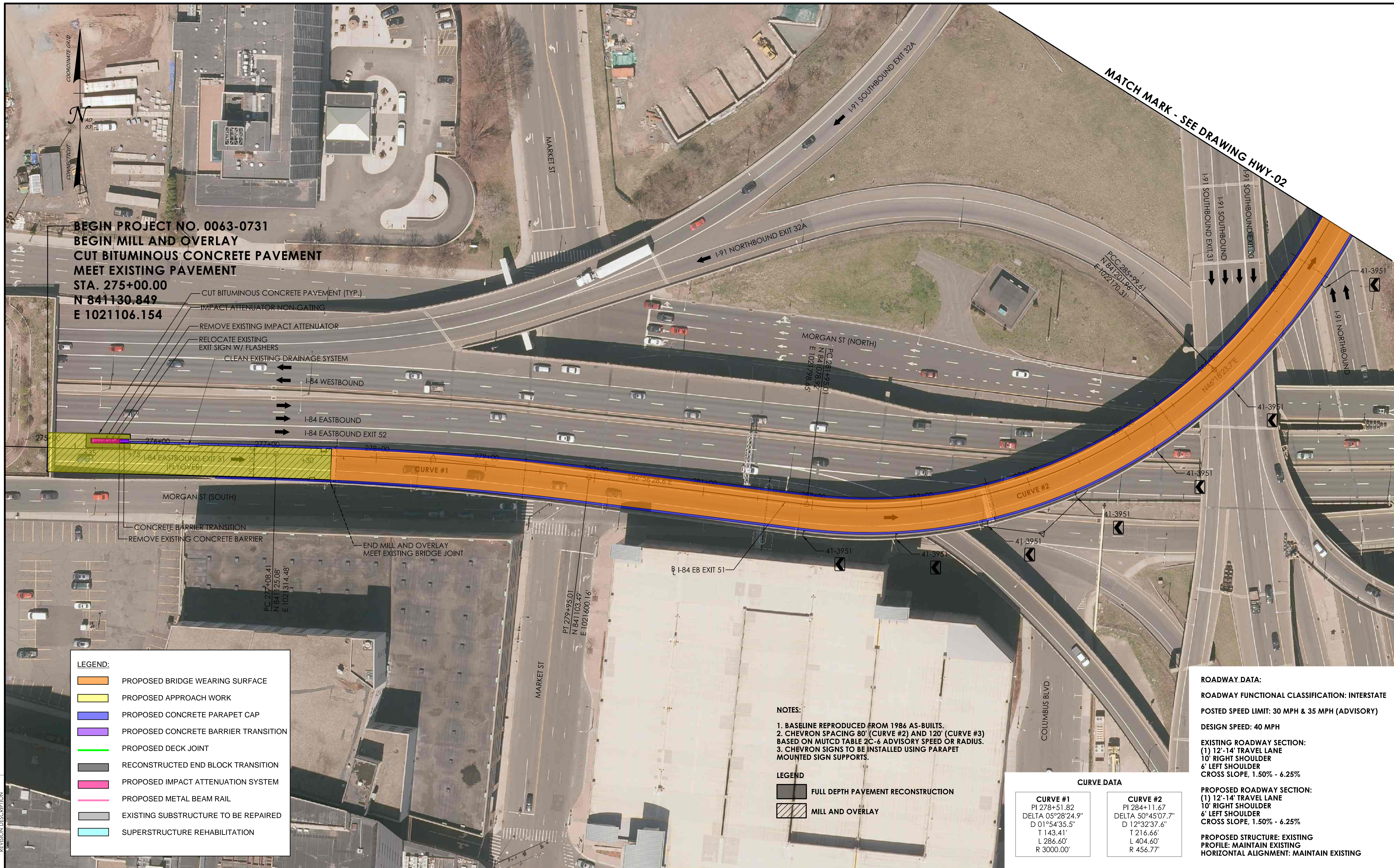
PROPOSED TYPICAL SECTION
SCALE: 3/8"=1'-0"

NOTE: LANE WIDTH VARIES FROM 12'-0" AT BASELINE P.C. STA. 277+08.40 TO 14'-0" AT PIER NO. 1 STA. 279+36.18. LANE WIDTH OF 14'-0" IS CONSTANT FROM PIER NO. 1 THROUGH ABUTMENT NO. 2.

LEGEND:

	PROPOSED BRIDGE WEARING SURFACE
	EXISTING BRIDGE DECK
	PROPOSED CONCRETE PARAPET CAP
	EXISTING PARAPETS AND BARRIERS
	EXISTING STRUCTURAL STEEL

REV.	DATE	REVISION DESCRIPTION



BEGIN PROJECT NO. 0063-0731
BEGIN MILL AND OVERLAY
CUT BITUMINOUS CONCRETE PAVEMENT
MEET EXISTING PAVEMENT
STA. 275+00.00
N 841130.849
E 1021106.154

- CUT BITUMINOUS CONCRETE PAVEMENT (TYP.)
- IMPACT ATTENUATOR NON-GATING
- REMOVE EXISTING IMPACT ATTENUATOR
- RELOCATE EXISTING EXIT SIGN W/ FLASHERS
- CLEAN EXISTING DRAINAGE SYSTEM
- I-84 WESTBOUND
- I-84 EASTBOUND
- I-84 EASTBOUND EXIT 52

- MORGAN ST (SOUTH)
- CONCRETE BARRIER TRANSITION
- REMOVE EXISTING CONCRETE BARRIER
- END MILL AND OVERLAY MEET EXISTING BRIDGE JOINT

LEGEND:

	PROPOSED BRIDGE WEARING SURFACE
	PROPOSED APPROACH WORK
	PROPOSED CONCRETE PARAPET CAP
	PROPOSED CONCRETE BARRIER TRANSITION
	PROPOSED DECK JOINT
	RECONSTRUCTED END BLOCK TRANSITION
	PROPOSED IMPACT ATTENUATION SYSTEM
	PROPOSED METAL BEAM RAIL
	EXISTING SUBSTRUCTURE TO BE REPAIRED
	SUPERSTRUCTURE REHABILITATION

NOTES:

1. BASELINE REPRODUCED FROM 1986 AS-BUILTS.
2. CHEVRON SPACING 80' (CURVE #2) AND 120' (CURVE #3) BASED ON MUTCD TABLE 2C-6 ADVISORY SPEED OR RADIUS.
3. CHEVRON SIGNS TO BE INSTALLED USING PARAPET MOUNTED SIGN SUPPORTS.

LEGEND

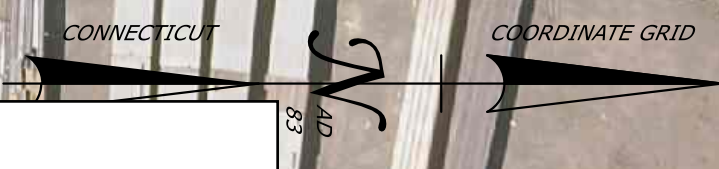
	FULL DEPTH PAVEMENT RECONSTRUCTION
	MILL AND OVERLAY

CURVE DATA

CURVE #1 PI 278+51.82 DELTA 05°28'24.9" D 01°54'35.5" T 143.41' L 286.60' R 3000.00'	CURVE #2 PI 284+11.67 DELTA 50°45'07.7" D 12°32'37.6" T 216.66' L 404.60' R 456.77'
---	--

ROADWAY DATA:
 ROADWAY FUNCTIONAL CLASSIFICATION: INTERSTATE
 POSTED SPEED LIMIT: 30 MPH & 35 MPH (ADVISORY)
 DESIGN SPEED: 40 MPH
 EXISTING ROADWAY SECTION:
 (1) 12'-14' TRAVEL LANE
 10' RIGHT SHOULDER
 6' LEFT SHOULDER
 CROSS SLOPE, 1.50% - 6.25%
 PROPOSED ROADWAY SECTION:
 (1) 12'-14' TRAVEL LANE
 10' RIGHT SHOULDER
 6' LEFT SHOULDER
 CROSS SLOPE, 1.50% - 6.25%
 PROPOSED STRUCTURE: EXISTING
 PROFILE: MAINTAIN EXISTING
 HORIZONTAL ALIGNMENT: MAINTAIN EXISTING

REV.	DATE	REVISION DESCRIPTION



LEGEND:

- PROPOSED BRIDGE WEARING SURFACE
- PROPOSED APPROACH WORK
- PROPOSED CONCRETE PARAPET CAP
- PROPOSED CONCRETE BARRIER TRANSITION
- PROPOSED DECK JOINT
- RECONSTRUCTED END BLOCK TRANSITION
- PROPOSED IMPACT ATTENUATION SYSTEM
- PROPOSED METAL BEAM RAIL
- EXISTING SUBSTRUCTURE TO BE REPAIRED
- SUPERSTRUCTURE REHABILITATION

MATCH MARK - SEE DRAWING HWY-01

**END PROJECT NO. 0063-0731
 END MILL AND OVERLAY
 CUT BITUMINOUS CONCRETE PAVEMENT
 MEET EXISTING PAVEMENT
 STA. 296+20.00
 N 842170.930
 E 1022323.012**

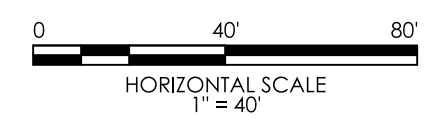
- MEET EXISTING GUIDERAIL
- METAL BEAM RAIL TRANSITION
350 TO MASH GUIDERAIL
- THREE-BEAM ATTACHMENT
- BITUMINOUS CONCRETE PARK CURBING
- REMOVE GRANITE STONE
TRANSITION CURBING
- REMOVE EXISTING BRIDGE
ATTACHMENT AND GUIDERAIL
- BRIDGE END BLOCK
(SEE STRUCTURE PLANS)
- BEGIN MILL AND OVERLAY
END FULL DEPTH PAVEMENT RECONSTRUCTION
- BEGIN FULL DEPTH PAVEMENT RECONSTRUCTION
MEET EXISTING BRIDGE JOINT

CURVE DATA

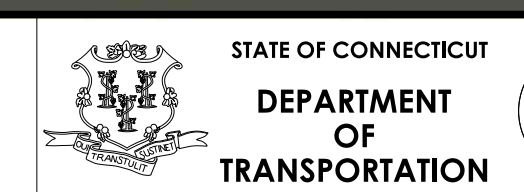
<p>CURVE #3 PI 288+74.20 DELTA 49°40'23.2" D 09°39'27.4" T 274.59' L 514.34' R 593.27'</p>	<p>CURVE #4 PI 300+11.13 DELTA 10°37'59.5" D 01°54'35.5" T 279.18' L 556.75' R 3000.00'</p>
---	--

REV.	DATE	REVISION DESCRIPTION

REHABILITATION STUDY REPORT

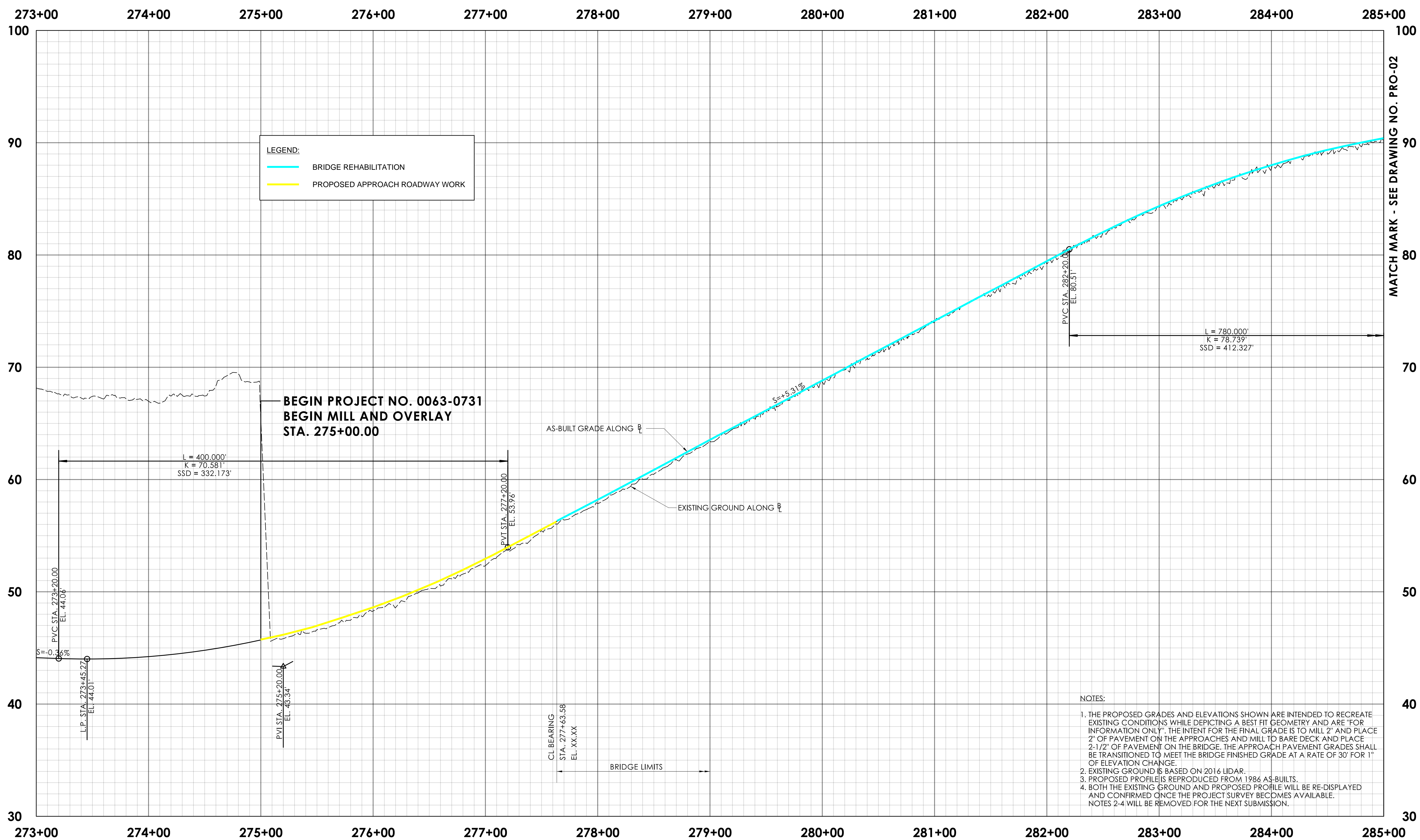


SIGNATURE/
BLOCK:



PROJECT NUMBER: 0063-0731
 PROJECT DESCRIPTION: REHABILITATION OF BRIDGE NO. 05868 CARRYING I-84 EB TR 839 TO I-91 NB
 TOWN(S): HARTFORD
 DRAWING TITLE: HIGHWAY PLAN

DRAWING NO.
HWY-02
SHEET NO.



LEGEND:
 BRIDGE REHABILITATION
 PROPOSED APPROACH ROADWAY WORK

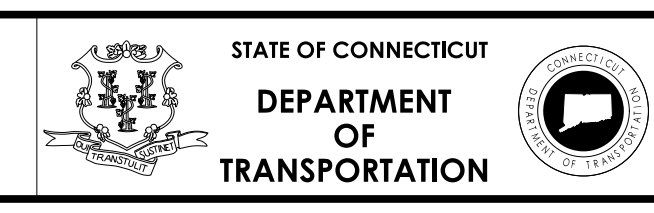
**BEGIN PROJECT NO. 0063-0731
 BEGIN MILL AND OVERLAY
 STA. 275+00.00**

NOTES:
 1. THE PROPOSED GRADES AND ELEVATIONS SHOWN ARE INTENDED TO RECREATE EXISTING CONDITIONS WHILE DEPICTING A BEST FIT GEOMETRY AND ARE "FOR INFORMATION ONLY". THE INTENT FOR THE FINAL GRADE IS TO MILL 2" AND PLACE 2" OF PAVEMENT ON THE APPROACHES AND MILL TO BARE DECK AND PLACE 2-1/2" OF PAVEMENT ON THE BRIDGE. THE APPROACH PAVEMENT GRADES SHALL BE TRANSITIONED TO MEET THE BRIDGE FINISHED GRADE AT A RATE OF 30' FOR 1" OF ELEVATION CHANGE.
 2. EXISTING GROUND IS BASED ON 2016 LIDAR.
 3. PROPOSED PROFILE IS REPRODUCED FROM 1986 AS-BUILTS.
 4. BOTH THE EXISTING GROUND AND PROPOSED PROFILE WILL BE RE-DISPLAYED AND CONFIRMED ONCE THE PROJECT SURVEY BECOMES AVAILABLE. NOTES 2-4 WILL BE REMOVED FOR THE NEXT SUBMISSION.

REV.	DATE	REVISION DESCRIPTION

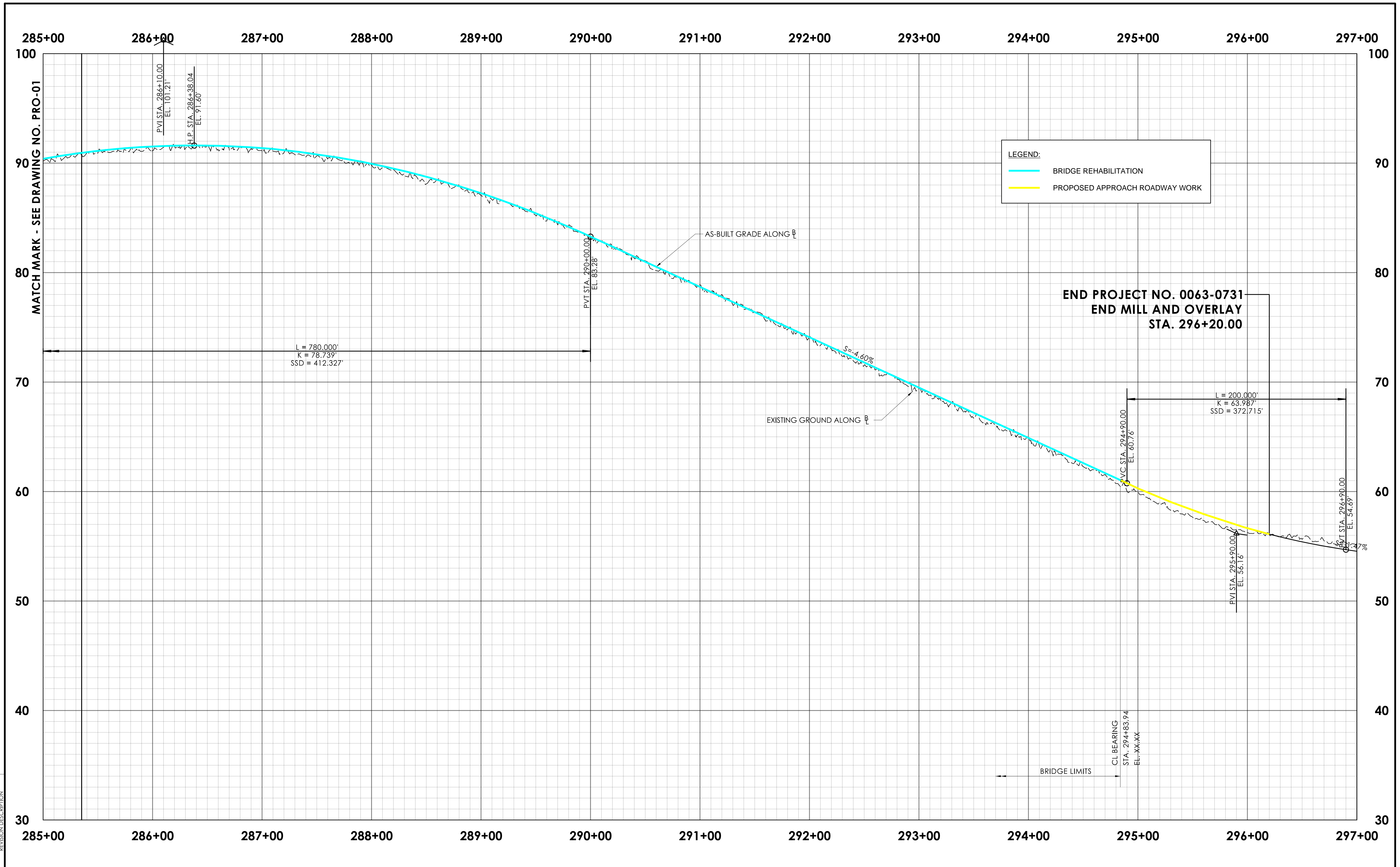
REHABILITATION STUDY REPORT
 DESIGNER/DRAFTER: BTK CHECKED BY: JDL
 HORIZONTAL SCALE IN FEET: 0 40 80
 VERTICAL SCALE IN FEET: 0 4 8

SIGNATURE/BLOCK:
CHA
 400 Capital Boulevard, Suite 301
 Hartford, CT 06103
 860.257.6527 • www.cha.com

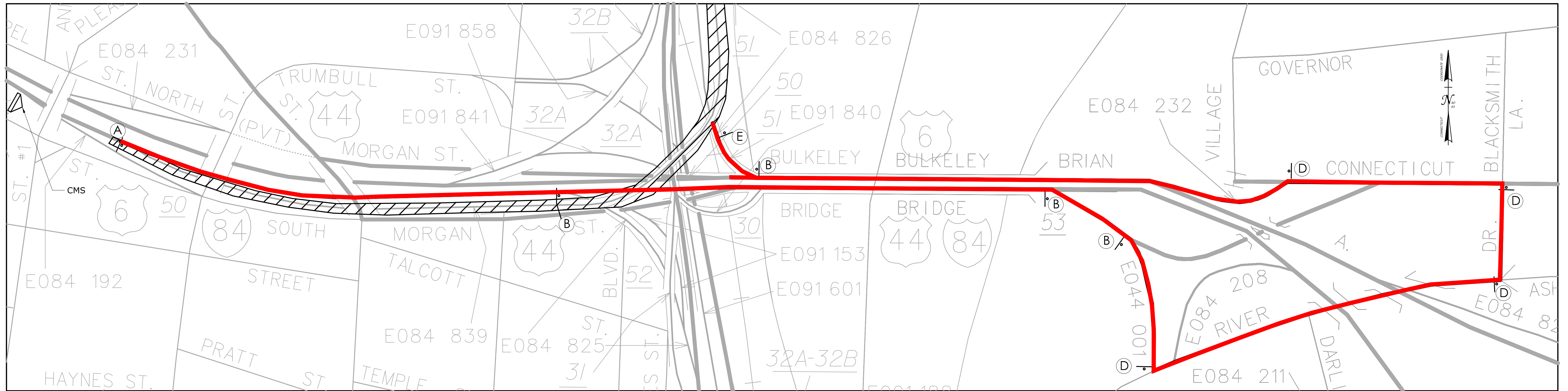


PROJECT NUMBER: 0063-0731
 PROJECT DESCRIPTION: REHABILITATION OF BRIDGE NO. 05868 CARRYING I-84 EB TR 839 TO I-91 NB
 TOWN(S): HARTFORD
 DRAWING TITLE: HIGHWAY PROFILE

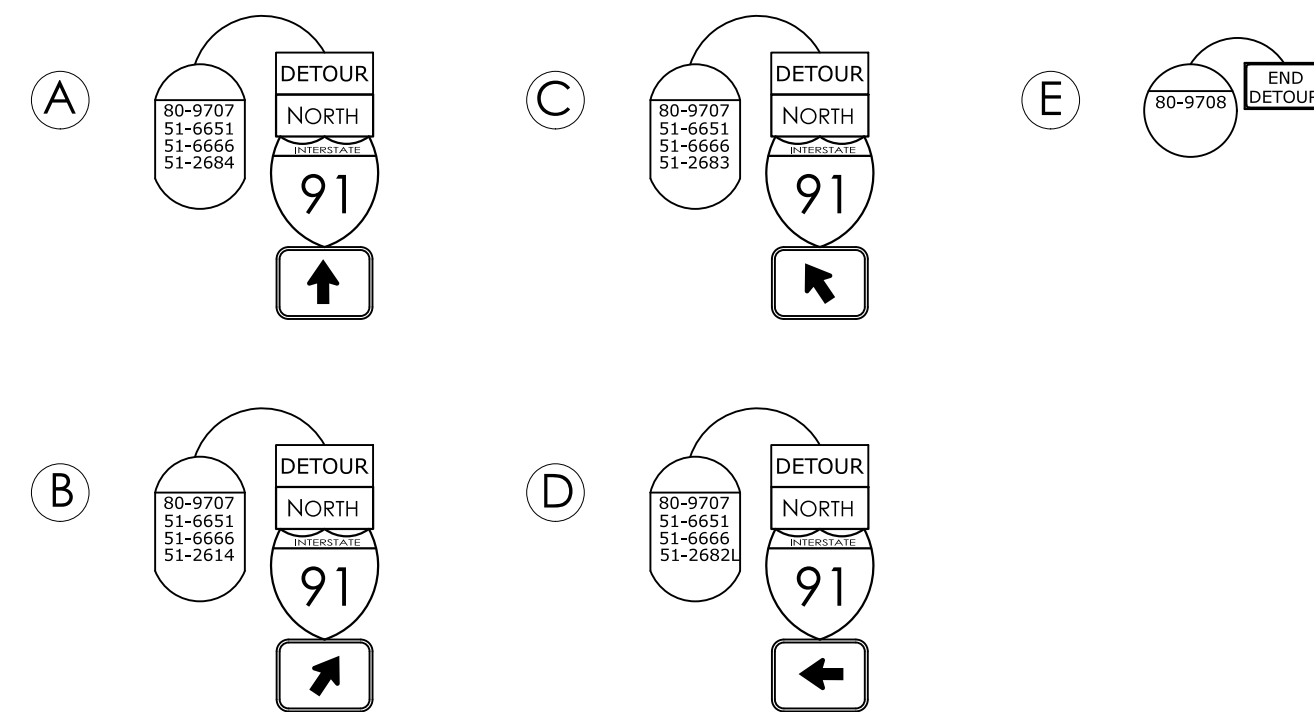
DRAWING NO. PRO-01
 SHEET NO.



REV.	DATE	REVISION DESCRIPTION



SIGN SUMMARY



DETOUR NOTES

1. ALL SIGN LOCATIONS INDICATED ON THE PLAN ARE APPROXIMATE; EXACT LOCATIONS SHALL BE VERIFIED IN THE FIELD BY THE ENGINEER.
2. DETOUR SIGNS SHALL BE COVERED WHEN THE DETOUR IS NOT IN OPERATION.
3. DETOUR SIGNS SHALL BE REMOVED WHEN THE DETOUR IS NO LONGER REQUIRED.
4. EXISTING SIGNS WHICH CONFLICT WITH THE DETOUR ROUTE SHALL BE COVERED, REMOVED, OR REVISED AS DIRECTED BY THE ENGINEER.
5. INSTALL POST-MOUNTED SIGNS IN ACCORDANCE WITH STANDARD SHEETS TR_1208-01 AND TR_1208-02.
6. THE COST OF THE DETOUR SIGNS SHALL BE PAID FOR UNDER ITEM NO. 1220027 - CONSTRUCTION SIGNS.
7. ACCESS AND EGRESS TO ALL STREETS AND DRIVEWAYS MUST BE MAINTAINED AT ALL TIMES.
8. AT LEAST TWO WEEKS PRIOR TO IMPLEMENTATION OF CONSTRUCTION STAGING, THE CONTRACTOR SHALL NOTIFY THE ENGINEER WHO SHALL THEN NOTIFY THE CITY OF NORWALK EMERGENCY SERVICES AND DEPARTMENT OF PUBLIC WORKS.
9. BARRICADE WARNING LIGHTS - HIGH INTENSITY SHALL BE MOUNTED ON ALL DIAMOND SHAPED POST-MOUNTED CONSTRUCTION SIGNS AND SIGNS MOUNTED TO TYPE III CONSTRUCTION BARRICADE CLOSING ROAD AND ARE PAYABLE UNDER ITEM NO. 0976002.
10. TWO WEEKS PRIOR TO INITIATING THE DETOUR, INSTALL SIGN "O" FROM THE SIGN SUMMARY. REMOVE SIGN "O" WHEN DETOUR IS INITIATED.
11. ANY EXISTING SIGNING DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT NO COST TO THE STATE.
12. CONSTRUCTION/DETOUR SIGNS ARE TO BE INSTALLED SO THAT THEY DO NOT BLOCK OR ARE NOT BLOCKED BY EXISTING SIGNS.

CHANGEABLE MESSAGE SIGN

PRIOR TO CONSTRUCTION MESSAGE 1		DURING CONSTRUCTION MESSAGE 2	
EXIT 51 TO BE CLOSED	MM/DD TO MM/DD	EXIT 51 CLOSED AHEAD	FOLLOW DETOUR

LEGEND

- WORK AREA
- DETOUR ROUTE
- SINGLE POST MOUNTED SIGN

REV.	DATE	REVISION DESCRIPTION