



**Connecticut Siting Council
Docket No. 272**

**Development & Management Plan
for the**

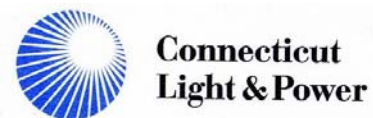
**Middletown-Norwalk
345-kV Transmission Line Project**

**Segment 4a – Singer Substation to
Fairfield/Westport Town Line**

Volume 3 of 3

May 2006

Revised Approved Plan





**TRAFFIC INVENTORY REPORT FOR
MAINTENANCE AND PROTECTION OF TRAFFIC**

**MIDDLETOWN TO NORWALK
345-kV TRANSMISSION PROJECT**

CITY OF BRIDGEPORT, CONNECTICUT

Prepared for:



**Connecticut
Light & Power**

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Meriden, Connecticut

May, 2006

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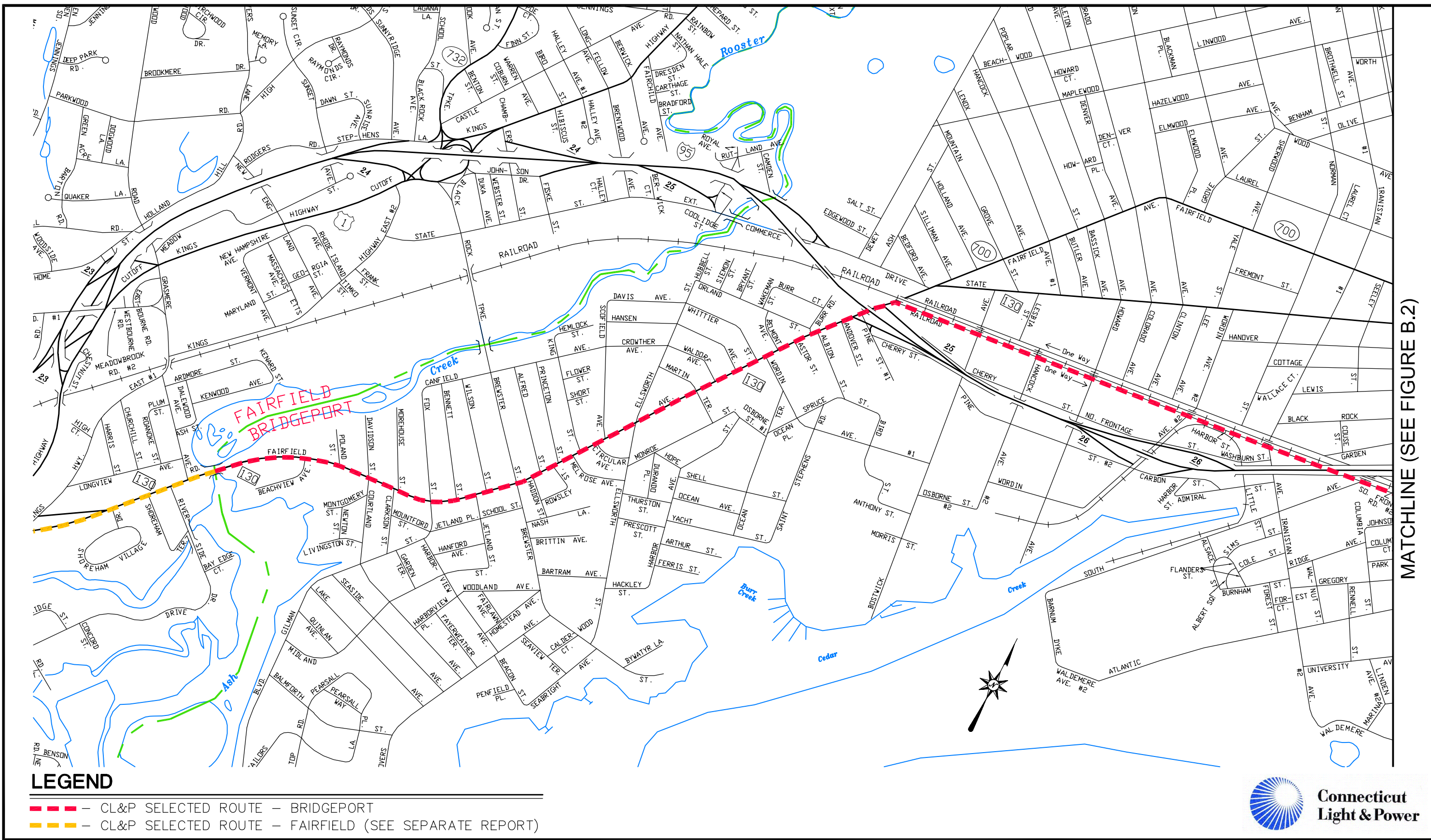
I. INTRODUCTION

The Connecticut Light & Power Company (CL&P) in conjunction with the United Illuminating Company will be constructing approximately 23 miles of a double circuit 345 kilo-volt underground transmission line through Norwalk, Westport, Fairfield, Bridgeport, Stratford and Milford. As approved by the Connecticut Siting Council, most of the route is in the public right-of-way, primarily along the State Highway System.

This report, prepared for CL&P, focuses on the 3.3 miles of transmission line located in the City of Bridgeport as illustrated in Figures B.1 and B.2. The remaining municipalities will be addressed in separate documents. The selected route travels along Route 130 (Fairfield Avenue) from the Fairfield/Bridgeport Town line, where it crosses Ash Creek, and then continues along State and local roads to the Singer Substation. Specifically, it runs along Route 130 (Fairfield Avenue), to Railroad Avenue (Eastbound), to Broad Street and across private property to the new Singer Sub-Station (located north of Henry Street). The route from the Singer Substation to the Bridgeport/Stratford border will be designed and constructed by the United Illuminating Company, and is not included in this report.

This report provides a recommended strategy for maintenance and protection of traffic. The strategy includes the locations where Connecticut Department of Transportation (ConnDOT) Maintenance Traffic Control Plans will be utilized; the locations where more specific maintenance and protection of traffic control plans will be developed; and the recommended hours of operation. Recommendations are based on a detailed field

inventory of the selected routes, traffic volumes, the type and duration of construction, and data compiled from local and State agencies. Specifically, local and State agencies were contacted for pertinent traffic data, roadway improvement projects, development projects, yearly local events, transit and bus routes and other data that may affect maintenance and protection of traffic planning. This report discusses the traffic/transportation environment along the route, the proposed construction, other construction projects such as public roadway improvement projects and major traffic generators, key locations, and traffic issues. Traffic issues include hours of operation, lane closures, need for detours, and areas where on-street parking will be affected. It forms the basis for the development of detailed traffic control plans (TCP) and detailed maintenance and protection of traffic report to be implemented for construction of the underground transmission line segment through the City of Bridgeport.



MATCHLINE (SEE FIGURE B.2)

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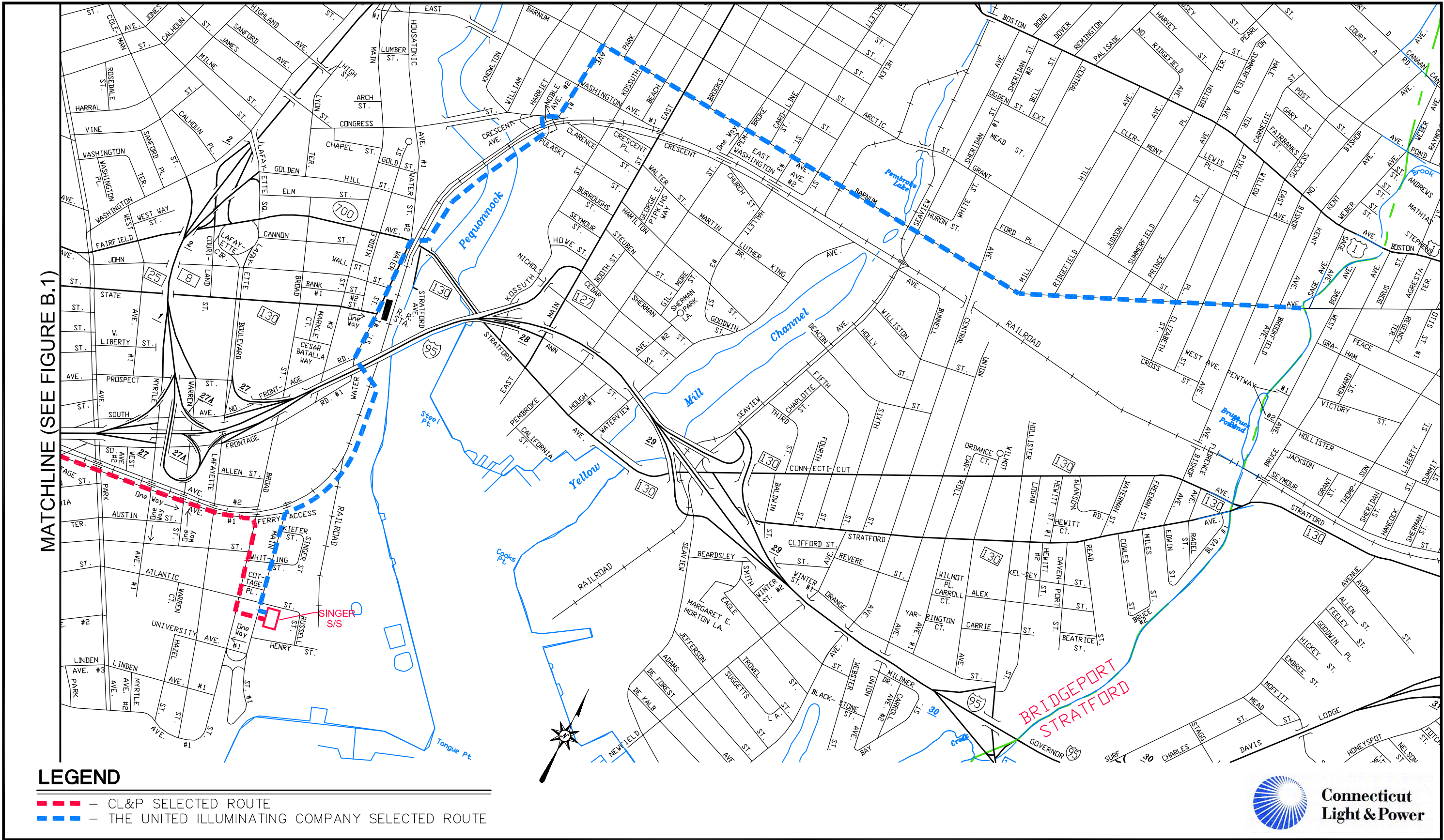
SELECTED ROUTE

MIDDLETOWN TO NORWALK
345-kV TRANSMISSION PROJECT
BRIDGEPORT, CONNECTICUT

Scale 1"=1000'
Project No. 05C1314
Date 4/17/06
CAD File 015_TRPT05C1314 FIG B1_050817

FIGURE B.1

XREF(s): NONE



MATCHLINE (SEE FIGURE B.1)

LEGEND

- - - - CL&P SELECTED ROUTE
- - - - THE UNITED ILLUMINATING COMPANY SELECTED ROUTE



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SELECTED ROUTE

MIDDLETOWN TO NORWALK
345 kV TRANSMISSION PROJECT
BRIDGEPORT, CONNECTICUT

Scale 1"=1000'
Project No. 05C1314
Date 4/27/06
CAD File 015_TRPT05C1314 FIG B2_050817

XREF(s): NONE



FIGURE B.2

II. CORRIDOR INVENTORY AND DESCRIPTION

An investigation of the existing traffic/transportation conditions of the roadways along the selected transmission line route formed the basis for preparing a recommended strategy for maintenance and protection of traffic. This investigation included a detailed field reconnaissance and preliminary research of pertinent planning and traffic data at local and State agencies.

Selected Transmission Line Route

As illustrated in Figures B.1 and B.2, the selected route runs from the Fairfield/Bridgeport town line to the Singer Substation, and then from the Singer Substation to the Bridgeport/Stratford town line. Essentially, from west to east, the selected route crosses Ash Creek adjacent to the Route 130 bridge at the Fairfield/Bridgeport town line, travels along Route 130 (Fairfield Avenue), to Railroad Avenue (Eastbound), to Broad Street and across private property to the new Singer Substation (located north of Henry Street). The route continues from the Singer Substation to the Bridgeport/Stratford town line. This segment will be designed and constructed by the United Illuminating Company, and is not included in this report.

For description and maintenance and protection of traffic purposes, the route was divided into segments of generally uniform characteristics. The following graphics/charts are included in this report to aid in the understanding of these characteristics:

- Figures B.3 and B.4, located in the body of the report, summarize traffic signal locations along the selected route as well as average daily traffic volumes (ADT's).

- A route inventory sheet is located in Appendix II. The sheet summarizes in tabular format items such as number of travel lanes, roadway widths, speed limit, sidewalk and on-street parking locations, illumination, bus routes, ADT's, peak hour volumes, traffic signal locations and abutting land use types.
- Figures B.8 through B.10, located in Appendix II, pictorially summarize land use classification along the route as well as typical roadway widths.
- Hourly traffic volume graphs for selected locations can be found in Appendix III.
- Aerial photographs of each signalized intersection are provided in Appendix IV.
- Aerial photographs of each vault are provided in Appendix VI.

A. Route 130 (Fairfield Avenue) from Fairfield/Bridgeport town line to Railroad Avenue.

The selected transmission line route travels along 1.5 miles of Route 130 (Fairfield Avenue) in Bridgeport from the Bridgeport/Fairfield town line to Railroad Avenue. This segment of Route 130 (Fairfield Avenue) is a northeast/southwesterly State-maintained facility that provides two travel lanes in each direction and is divided by a median. The roadway has the following characteristics:

- The typical curb-to-curb width for each direction of travel is 28 feet.
- The typical median width is 5 feet.
- Posted speed limit ranges between 30 miles per hour and 25 miles per hour:
 - 30 miles per hour between Bridgeport/Fairfield Town line and Wordin Avenue.
 - 25 miles per hour between Wordin Avenue and Railroad Avenue.
- Illumination and sidewalks are provided along both sides of the facility for the entire 1.5-mile stretch.

- Streetscaping is provided along both sides of the facility, as well as along the raised median throughout the entire 1.5-mile section, except in the areas of intersection with the I-95 SB (Exit 25) off ramp and Railroad Avenue (EB).
- Bus stops are provided.
- On-street parking is permitted.

Eight signalized intersections (all State-maintained) are also located along this roadway segment and include in order from west to east the following:

1. Route 130 (Fairfield Avenue) at Davidson Street/Courtland Avenue
2. Route 130 (Fairfield Avenue) at Fox Street/Gilman Street
3. Route 130 (Fairfield Avenue) at Brewster Street
4. Route 130 (Fairfield Avenue) at Ellsworth Street
5. Route 130 (Fairfield Avenue) at Wordin Avenue/Whittier Street
6. Route 130 (Fairfield Avenue) at Orland Street/Albion Street/Burr Road (a 5-legged intersection)
7. Route 130 (Fairfield Avenue) at Pine Street/I-95 northbound Exit 25 on-ramp
8. Route 130 (Fairfield Avenue) at I-95 Exit 25 southbound off-ramp

These signals are part of a closed loop signal system.

An unsignalized crosswalk is provided at the intersection of Route 130 with Melrose Street, near the Black Rock Senior Center. A school crosswalk is provided at the signalized Route 130 (Fairfield Avenue) intersection with Whittier Street and Wordin Avenue. The following schools are located on or have access provided by Whittier Street and Wordin Avenue:

- Howe Annex, an elementary school is located along Whittier Street.
- The Longfellow School has access from Route 130 (Fairfield Avenue) provided via Wordin Avenue.
- The Covenant of Mary Immaculate has access from Route 130 (Fairfield Avenue) provided via Wordin Avenue.

Land use is a mix between commercial/retail, businesses and residential. Land uses of specific interest include the following:

- The Bridgeport Garden Apartments located along the southerly side of Route 130 (Fairfield Avenue) between Brewster Street and Melrose Street.
- The Black Rock Senior Center located at the northwest corner of Route 130 (Fairfield Avenue) and King Street, across from Melrose Street.
- The United House of Prayer for All People located within the southeastern corner of the Route 130 (Fairfield Avenue) at Ellsworth Street intersection.
- The Community Center located within the northeastern corner of the Route 130 (Fairfield Avenue) at Ellsworth Street intersection.
- The Cathedral of Faith located along the south side of Route 130 (Fairfield Avenue) between Martin Terrace and Wordin Avenue.
- The U.S. Post Office located at the southwesterly corner of the Route 130 (Fairfield Avenue) at Wordin Avenue intersection.
- The Bridgeport Fire Department has access to and from Route 130 (Fairfield Avenue) via Wordin Avenue.

Average daily traffic volumes (ADT's) vary along the segment. The ADT is 15,500 vehicular trips just east of the Bridgeport/Fairfield Town line, and 17,500 vehicular trips just east of Waldorf Avenue. ADT's near the Interstate 95 Exit 25 interchange range from 22,400 vehicular trips to 23,800 vehicular trips.



Photo 1. Route 130 (Fairfield Avenue) looking northeast at Poland Street.



Photo 2. Route 130 (Fairfield Avenue) looking northeast between Jetland Street and Brewster Street. Notice the raised streetscaped median and on-street parking.



Photo 3. Route 130 (Fairfield Avenue) looking northeast at Ellsworth Street intersection.



Photo 4. Route 130 (Fairfield Avenue) looking southwest, east of Ellsworth Street.



Photo 5. Route 130 (Fairfield Avenue) looking northeast at intersection with Orland Street.



Photo 6. Route 130 (Fairfield Avenue) looking northeast at I-95 SB off-ramp intersection.

B. Railroad Avenue from Route 130 (Fairfield Avenue) to Broad Street

Railroad Avenue is a municipal road, approximately 1.5 miles long, and runs essentially east/west from Route 130 (Fairfield Avenue) to Broad Street. The roadway is split by the Amtrak/Metro North Railroad, thus forming a pair of one-way roads; Railroad Avenue one-way eastbound and Railroad Avenue one-way westbound. The selected route for the transmission line will be constructed along the eastbound section of Railroad Avenue. Therefore, this report is limited to that segment. Railroad Avenue (eastbound) has the following characteristics:

- Roadway width varies from 17 to 33 feet, with typical widths of 18 feet and 30 feet.
- The posted speed limit is 25 miles per hour.
- Illumination is provided along the southerly side of the facility.
- Sidewalks are provided in some areas, see route inventory in Appendix II for more details.
- On-street parking occurs in the residential area located between South Avenue and Park Avenue.
- The majority of the roadway is in poor condition.
- Portions of the road include abandoned railroad tracks.

Eleven signalized intersections (City-maintained) are located along Railroad Avenue (eastbound) and include the following:

1. Railroad Avenue (eastbound) at Bostwick Avenue
2. Railroad Avenue (eastbound) at Hancock Avenue
3. Railroad Avenue (eastbound) at Howard Avenue
4. Railroad Avenue (eastbound) at Wordin Avenue

5. Railroad Avenue (eastbound) at Iranistan Avenue
6. Railroad Avenue (eastbound) at South Avenue
7. Railroad Avenue (eastbound) at Park Avenue
8. Railroad Avenue (eastbound) at Myrtle Avenue (one-way street southbound)
9. Railroad Avenue (eastbound) at Warren Avenue (one-way street northbound)
10. Railroad Avenue (eastbound) at Lafayette Street
11. Railroad Avenue (eastbound) at Broad Street

Aerial photographs of the signalized intersections are provided in Appendix IV. Note that each location is actually one half of a double signalized intersection with the corresponding westbound Railroad Avenue intersection.

The side street crossings under the railroad have sub-standard vertical clearances:

- Broad Street: 12'-4"
- Lafayette Street: 11'-3"
- Warren Street: 11'-3"
- Myrtle Avenue: 11'-2"
- Park Avenue: 12'-7"
- South Avenue: 11'-6"
- Iranistan Avenue: 13'-2"
- Wordin Avenue: Not Posted (unknown)
- Howard Avenue: 11'-9"
- Hancock Avenue: 11'-9"

- Bostwick Avenue: 10'-10"
- Fairfield Street: 12'-10"

Land use is a mix between business, industrial, a small portion of retail and residential. There are also vacant buildings. The land use area of specific concern is the apartment complex located between South Avenue and Park Avenue.

ADT volumes along Railroad Avenue (eastbound) are very low ranging from 250 to 1,100 vehicular trips.



Photo 7. Railroad Avenue (EB) looking eastbound toward Bostwick Avenue. The roadway width in this area is approximately 17 feet.



Photo 8. Railroad Avenue (EB) looking eastbound toward the Howard Avenue intersection. Note the poor pavement condition.



Photo 9. Railroad Avenue (EB) looking eastbound at Howard Avenue intersection. Note the railroad tracks to the right of Railroad Avenue (EB).



Photo 10. Railroad Avenue (EB) looking eastbound at Iranistan Avenue.



Photo 11. Railroad Avenue (eastbound) looking east toward Park Avenue. The residential apartment complex and on-street parking is located to the right in the photo.



Photo 12. Railroad Avenue (eastbound) looking east at Myrtle Avenue.



Photo 13. Railroad Avenue (eastbound) looking eastbound at its terminus with Broad Street.

C. Broad Street to the Singer Substation

The selected transmission line route travels southbound along approximately 0.2 miles of Broad Street in Bridgeport, from Railroad Avenue (eastbound) to private property located east of Broad Street. The proposed route then travels east (approximately 0.1 mile) through private property and crosses Main Street to the new Singer Substation (located north of Henry Street).

Broad Street and Main Street, both municipal roadways, run north/south and provide one lane in each direction.

- Roadway widths from curb to curb are typically 30 feet and 36 feet, for Broad Street and Main Street, respectively.
- Sidewalks and illumination are provided along both sides of these roads.
- On-street parking is permitted.

Land uses along Broad Street and Main Street are mostly residential with some retail and institutional use. Specific land uses of concern include the following:

- A church located at the corner of Broad Street and Gregory Street.
- Bridgeport University campus located adjacent to Broad Street, specifically a dormitory building across from University Avenue.
- The Ballpark at Harbor Yard, home of the Bridgeport Bluefish minor league baseball team, and the Arena at Harbor Yard located along the easterly side of Broad Street, and north of the Amtrak/Metro North Railroad.

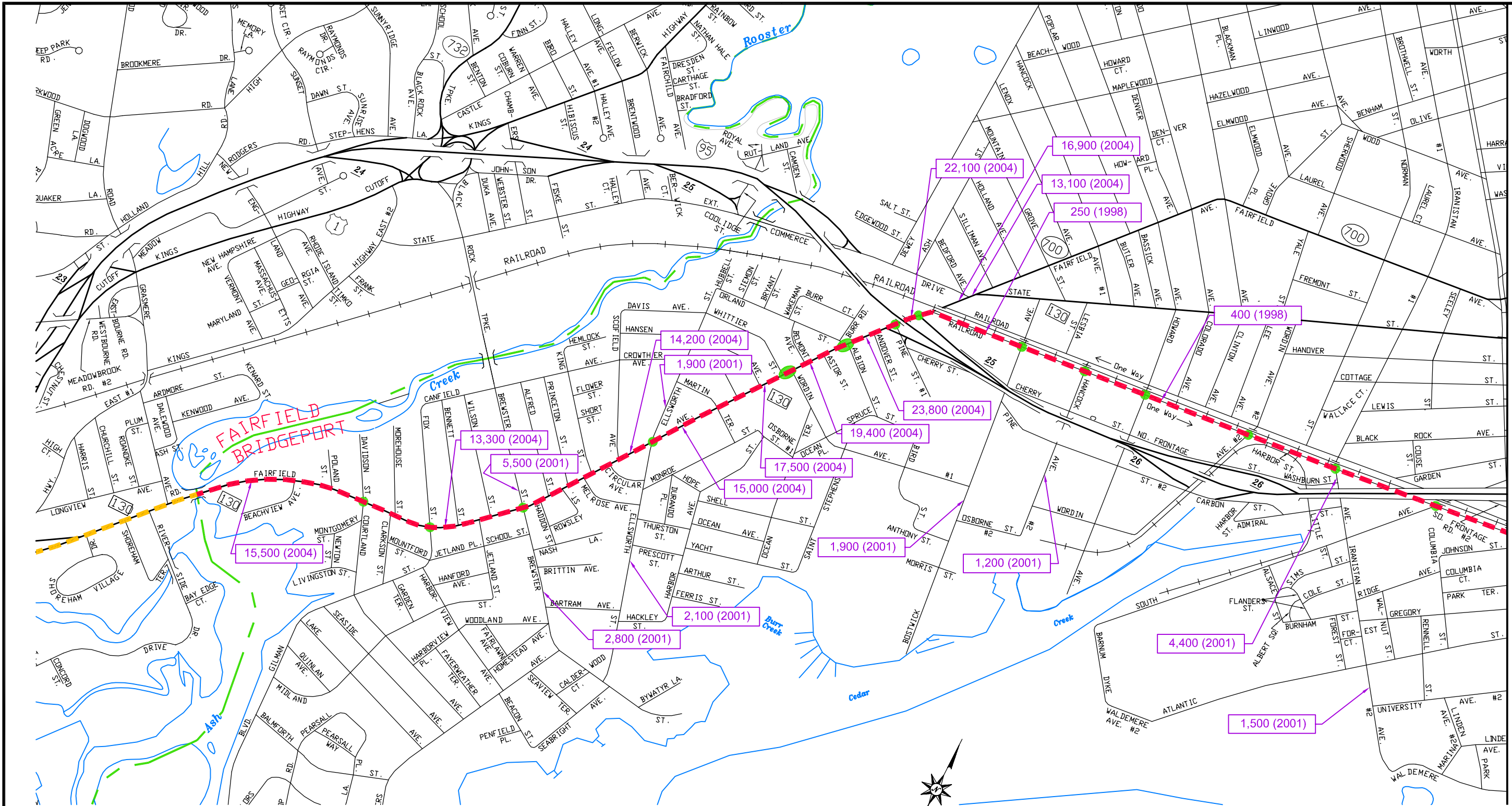
Broad Street carries an ADT of approximately 1,800 vehicular trips and Main Street an ADT of 1,500 vehicular trips.



Photo 14: Broad Street looking south toward Whiting Street. Notice the Church on the corner of Broad Street and Gregory Street.



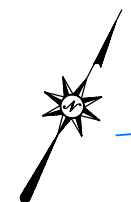
Photo 15. Main Street looking northbound, north of University Avenue.



MATCHLINE (SEE FIGURE B.4)

LEGEND

- XX,XXX: ADT
- (XXXX): YEAR
- - TRAFFIC SIGNAL
- CL&P SELECTED ROUTE - BRIDGEPORT
- CL&P SELECTED ROUTE - FAIRFIELD (SEE SEPARATE REPORT)



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TRAFFIC SIGNAL LOCATIONS AND ADT'S

MIDDLETOWN TO NORWALK
345 kV TRANSMISSION PROJECT
BRIDGEPORT, CONNECTICUT

Scale 1"=1000'
Project No. 05C1314
Date 4/17/06
CAD File 015_TRPT05C1314 FIG B3B4_050817

FIGURE B.3

XREF(s): NONE

III. WORK BY OTHERS

Public Roadway Improvement Projects

The following are state projects that are proposed in the immediate vicinity of the CL&P 345-kV Transmission Line project area in Bridgeport and the anticipated start dates:

- State Project #15-296, Route 130 – Reconstruction of Fairfield Avenue, June 2006.

The following are state projects that are proposed in the general vicinity of the CL&P 345-kV Transmission Line project area in Bridgeport and the anticipated start dates:

- State Project #15-272, I-95 Reconstruction, currently in construction.
- State Project #15-271, I-95 – Reconstruction of Exits 25-28, Warren Street to Yellow Mill, currently in construction.
- State Project #15-288, Seaview Avenue – Reconstruction from I-95 to Route 1, start date to be determined.
- State Project #15-311, City-wide Traffic Signal Modernization, September 2006
- State Project #15-291, Barnum Avenue – Replacement and Modernization to Closed Loop System, currently in progress.

The following are projects outlined in the City's Five Year Capital Plan Fiscal Year 2006-2010 and in the general vicinity of the CL&P 345-kV Transmission Line project area:

- City-wide Roadway Paving, 2006
- City-wide Lighting Programs, 2006

Development Projects

The Village at Black Rock Condominiums (STC # 015-0502-07) a 130,000 square foot residential development has been approved by the City of Bridgeport and the State Traffic Commission (STC). The development is to be located within the northeast quadrant of the intersection of Route 130 (Fairfield Avenue) with Poland Street. Off-site improvements are limited to widening of an existing median opening near the developments Route 130 (Fairfield Avenue) emergency access in order to accommodate emergency vehicles.

IV. CONSTRUCTION SEQUENCE AND UNDERSTANDING

The construction of the 345-kV underground transmission line is a five (5) step process.

The steps are performed sequentially, but not necessarily continuously. Therefore, periods of no visible construction activity or traffic disruption may occur between steps.

1. Splice-vault Excavation and Installation.

Large splice vaults will be installed at intervals of approximately 1,650 feet on center. The vaults are installed in pairs, with 11 pairs anticipated in Bridgeport along the CL&P section of the transmission line. ConnDOT has requested that no vaults be located within the travel way along the State roadways. Each vault measures 32' long, 9' wide and 10' high. An excavation of about 36' long, 14' deep and 13' wide is needed for each vault.

For any vaults in the roadway, the duration of construction for each pair is expected to be 7-14 days working nights only, or 2-4 days working 24 hours around the clock. Depending on the exact location and the task being performed, 2-3 traffic lanes will have to be closed for installation. The actual installation of the pre-cast concrete vault sections will require the use of a crane, which itself needs effectively two lanes for the outriggers and swing clearance. This installation will typically occur in the timeframe of one night. Backfilling, etc., may require a narrower work area. Should the excavation have to remain "open" when work is not in progress, protective barrier will be required if traffic is not maintained in the lane(s) of the excavation, and a special design for bridging the excavation if traffic is maintained in the lane(s) of the excavation.

2. Duct-bank Excavation and Installation.

The pavement will be saw cut to the width of the excavation. The excavation and duct bank will be approximately 4' wide with a minimum 30" deep cover. The depth of the trench will vary depending on underground conflicts. The duct bank will contain 10 conduits and will be encased in concrete, cast in place. The trench will be backfilled and temporary pavement installed. In general, two travel lanes will be required for this work. Steel plates will be required if the trench cannot be backfilled at the end of the work day and the travel lanes must be opened. It is anticipated that 50-150 feet of duct-bank installation can be done per day per crew.

3. Cable Pulling.

Cable reel carts and pulling machines will be set up over the vaults. Assuming a normal work shift, it is anticipated that six (6) work-days will be required to pull cable between each pair of vaults. In general, one to two lanes of traffic will have to be closed for this activity.

4. Cable Splicing.

This is a time consuming activity requiring a controlled environment in the vaults. As such, a specially designed trailer is parked over the vault. Cable splicing will require 24 days per set of vaults, assuming a 12-hour work shift. For vault locations within the roadway, one to two traffic lanes will be occupied by this activity.

5. Pavement Restoration

The final task is to restore the pavement. The trench will be temporarily repaired in accordance with temporary trench repair details to be developed. Typically two travel lanes will be occupied by this operation. At completion of the project, a mill and overlay will be constructed on State and local roadways in accordance with ConnDOT and City standards to a width agreed to by CL&P, ConnDOT and the City, within limits as set forth in the State's Encroachment Permit and the City's Road Opening Permit.

V. RECOMMENDATIONS FOR MAINTENANCE AND PROTECTION OF TRAFFIC

This project is a utility infrastructure improvement. However, from the perspective concerning the impact of construction on traffic, two of the construction elements, vault and duct bank installation, are similar to major roadway corridor reconstruction and thus have the need for detailed maintenance and protection of traffic procedures. Although the cable pulling and splicing may be less intrusive than the duct bank and vault construction, the location and duration dictate the need to address maintenance and protection of traffic.

This section of the report is divided into two parts; General Recommendations applicable to the entire project; and Specific Recommendations developed for the individual areas of work.

General Recommendations

1. Temporary traffic control plans shall be developed in accordance with the Manual on Uniform Traffic Control Devices (MUTCD), Part 6, and ConnDOT specifications.
2. Where appropriate, the ConnDOT Maintenance Traffic Control Plans shall be used. Non-standard traffic control plans shall be developed where the ConnDOT Maintenance Traffic Control Plans do not apply, and will be submitted for review and approval by ConnDOT. Any Contractor-requested revisions must be submitted to ConnDOT for review and approval at least 30 days prior to implementation.
3. Traffic control devices shall meet the requirement of NCHRP Report 350, Recommended Procedures for the Safety Performance Evaluation of Highway Features.
4. All flaggers shall be in accordance with the requirements under Section 9.74 "Trafficperson" in the State of Connecticut Department of Transportation Standard Specifications for Roads, Bridges and Incidental Construction, Form

816 and shall have completed training through ATSSA (American Traffic Safety Services Association) or other organizations, as approved by ConnDOT.

5. The Contractor shall have at least one person trained as a work zone safety supervisor through ATSSA, or other organization approved by ConnDOT.
6. The Contractor shall maintain access for emergency vehicles through the work zone at all times.
7. Access accommodations shall be made for pedestrians at all times. Pedestrian access to businesses shall be maintained during those times when the businesses are open unless permission is granted from the business owner to close access.
8. The Contractor shall maintain vehicular access to and egress from all commercial and residential driveways. The Contractor will be allowed to close driveways to perform the required work during those periods when the businesses are closed unless permission is granted from the business owner to close the driveway during business hours. If a temporary closure of a residential driveway is necessary, the Contractor shall coordinate with the owner to determine the time period of the closure.
9. Roadway occupancy will be scheduled during off-peak hours where possible, and where necessary, at night. Local noise ordinances will be investigated for nighttime activities.
10. The need to maintain traffic signal operation, including detection and interconnect is important, particularly along high volume routes. Temporary detection may require the use of means other than loops, such as microwave or video in areas of poor pavement. Use of a traffic signal contractor on stand-by duty during new construction activities should be provided.
11. During night work, existing roadway lighting must be maintained. Temporary lighting may have to be provided.
12. The contractor shall only excavate a length of roadway that can be completed, including paving, in one workday (or work night) during the allowable period. If necessary, due to limited allowable hours, steel plates may be used to bridge the excavation. See General Recommendation #13.
13. Steel plates will be required if the duct-bank trench cannot be backfilled at the end of the allowable work period. No more than 300 feet of trench length shall be plated. A waiver will be required for this length and is expected to be requested for this project. Such plates shall meet the ConnDOT requirements for steel plates and be inspected and maintained on a daily basis.

14. The Contractors' work schedule should be coordinated on a daily basis, with at a minimum: ConnDOT inspection personnel, the Bridgeport Traffic Operations Center, and local police and fire departments.
15. The Contractors' work schedule shall be made available on a weekly basis to other impacted road users and local officials, such as: local elected officials, public works personnel, emergency service providers, hospitals, public transit providers, Board of Education transportation coordinators, US Postal Service, etc.
16. For any roadway closure, a construction sign should be installed in both directions in the vicinity of the closure two weeks prior to notify motorists of the date(s) of the construction.
17. If there is more than one alternating one-way traffic operation at one time on a roadway, then there shall be at least one mile between signing patterns.

Specific Recommendations

For all nighttime work in the City of Bridgeport, a waiver from the City's noise control regulations will be sought from the City. Nighttime hours are from:

- Monday – Friday, 12:00 a.m. to 7:00 a.m. and 6:00 p.m. to 12:00 a.m.
- Saturday - Sunday: 12:00 a.m. to 9:00 a.m. and 6:00 p.m. to 12:00 a.m.

See Appendix V for the full text of the Bridgeport Noise ordinance.

A. Route 130 (Fairfield Avenue)

1. CL&P will utilize extra crews within each traffic pattern and construct simultaneously in multiple traffic patterns so as to expedite construction along Route 130 with the goal of completing construction in this corridor by the Wednesday before Thanksgiving, November 22, 2006.
2. The Contractor will not be allowed to perform any work that will interfere with the existing number of lanes of traffic, including turning lanes at intersections, on-street parking, and sidewalks on:

Between Fairfield-Bridgeport Town Line and Albion Street:

- Monday - Friday: 6:00 a.m. to 9:00 a.m.
3:00 p.m. to 6:00 p.m.
- Saturday - Sunday: 10:00 a.m. to 4:00 p.m.

Between Albion Street and Railroad Avenue:

- Monday - Friday: 6:00 a.m. to 9:00 a.m.
2:00 p.m. to 6:00 p.m.
 - Saturday - Sunday: 10:00 a.m. to 4:00 p.m.
2. When actively working, during the following periods, the Contractor will be allowed to close one direction of Route 130 (Fairfield Avenue) and maintain and protect two way traffic operations on a paved travel path not less than 22 feet in width on the other side, with on-street parking prohibited, in accordance with a ConnDOT Maintenance Traffic Control Plan to be modified for a raised median (see Appendix XIII for ConnDOT Maintenance Traffic Control Plans). At least 48 hours notice for prohibition of parking shall be posted.

Between Fairfield-Bridgeport Town Line and Albion Street:

- Monday – Friday: 12:00 a.m. to 6:00 a.m.
9:00 a.m. to 3:00 p.m.
6:00 p.m. to 12:00 a.m.
- Saturday - Sunday: 12:00 a.m. to 10:00 a.m.
4:00 p.m. to 12:00 a.m.

Between Albion Street and Railroad Avenue:

- Monday – Friday: 12:00 a.m. to 6:00 a.m.
9:00 a.m. to 2:00 p.m.
6:00 p.m. to 12:00 a.m.
 - Saturday - Sunday: 12:00 a.m. to 10:00 a.m.
4:00 p.m. to 12:00 a.m.
3. When actively working, during the following periods, the Contractor will be allowed to close one lane on Route 130 (Fairfield Avenue) in each direction and maintain one lane of traffic operations in each direction with a paved travel way of not less than 11 feet in each direction in accordance with a ConnDOT Maintenance Traffic Control Plan to be modified for a raised median (see Appendix XIII for ConnDOT Maintenance Traffic Control Plans). On-street parking will be prohibited if permitted by the City. At least 48 hours notice for prohibition of parking shall be posted.

Between Fairfield-Bridgeport Town Line and Albion Street:

- Monday – Friday: 12:00 a.m. to 6:00 a.m.
9:00 a.m. to 3:00 p.m.
6:00 p.m. to 12:00 a.m.

- Saturday - Sunday: 12:00 a.m. to 10:00 a.m.
4:00 p.m. to 12:00 a.m.

Between Albion Street and Railroad Avenue:

- Monday – Friday: 12:00 a.m. to 6:00 a.m.
9:00 a.m. to 2:00 p.m.
6:00 p.m. to 12:00 a.m.
- Saturday - Sunday: 12:00 a.m. to 10:00 a.m.
4:00 p.m. to 12:00 a.m.

4. When actively working, during the following periods, the Contractor will be allowed to close one median (inside) lane on Route 130 (Fairfield Avenue) in one direction only and maintain one curbside (outside) lane of traffic operations in that direction. On-street parking will be prohibited if allowed by the City. The paved travel path for this one direction shall be not less than 18 feet in width. In the opposite direction of travel, no lane closures shall be permitted and on-street parking shall remain.

Or, when actively working during the following periods, the Contractor will be allowed to close one curbside (outside) lane in one direction only and maintain one median side (inside) lane in that direction on a paved travel path not less than 11 feet wide. On-street parking in this direction shall be prohibited with at least 48 hours notice for prohibition of parking. In the opposite direction of travel, no lane closures shall be permitted and on-street parking shall remain.

These configurations shall be in accordance with a ConnDOT Maintenance Traffic Control Plan to be modified for a raised median (see Appendix XIII for ConnDOT Maintenance Traffic Control Plans).

Between Fairfield-Bridgeport Town Line and Albion Street:

- Monday – Friday: 12:00 a.m. to 6:00 a.m.
9:00 a.m. to 3:00 p.m.
6:00 p.m. to 12:00 a.m.
- Saturday - Sunday: 12:00 a.m. to 10:00 a.m.
4:00 p.m. to 12:00 a.m.

Between Albion Street and Railroad Avenue:

- Monday – Friday: 12:00 a.m. to 6:00 a.m.
9:00 a.m. to 2:00 p.m.
6:00 p.m. to 12:00 a.m.
- Saturday - Sunday: 12:00 a.m. to 10:00 a.m.
4:00 p.m. to 12:00 a.m.

5. When actively working, during the following periods, the Contractor will be allowed to maintain alternating one-way traffic operations with on-street parking prohibited. At least 48 hours notice for prohibition of parking shall be posted.

Between Fairfield-Bridgeport Town Line and Ellsworth Street:

- Monday – Friday: 12:00 a.m. to 6:00 a.m.
8:00 p.m. to 12:00 a.m.
- Saturday - Sunday: 12:00 a.m. to 6:00 a.m.
8:00 p.m. to 12:00 a.m.

Between Ellsworth Street and Albion Street:

- Monday – Friday: 12:00 a.m. to 6:00 a.m.
9:00 p.m. to 12:00 a.m.
- Saturday - Sunday: 12:00 a.m. to 10:00 a.m.
4:00 p.m. to 12:00 a.m.

Between Albion Street and Railroad Avenue:

- Monday – Friday: 12:00 a.m. to 6:00 a.m.
10:00 p.m. to 12:00 a.m.
- Saturday - Sunday: 12:00 a.m. to 6:00 a.m.
10:00 p.m. to 12:00 a.m.

7. In order to mitigate noise impacts, the Contractor will not be allowed to perform any sawcutting work on Route 130 during nighttime hours. The allowable hours for sawcutting activities are:

- Monday – Friday from 9:00 a.m. to 3:00 p.m.

8. A school crosswalk is located along Route 130 (Fairfield Avenue) between Wordin Avenue and Whittier Avenue. Children cross with a crossing guard at this location. The Contractor will maintain the school crosswalk during times to be coordinated with the Principals of the nearby schools; Howe Annex Elementary School, Longfellow School, and Covenant of Mary Immaculate.

9. The I-95 Exit 25 northbound on-ramp intersection with Route 130 (Fairfield Avenue) is located along the transmission line route. The contractor shall maintain access to the ramp at all times. A Trafficperson will be present during all construction periods to direct traffic.

10. The I-95 Exit 25 southbound off-ramp intersection with Route 130 (Fairfield Avenue) is located along transmission line route. The contractor shall maintain access from

the ramp to Route 130 at all times. A Trafficperson will be present during all construction periods to direct traffic.

11. The following locations traffic control signal equipment will be impacted by construction activities. Disturbed “local” loop detectors shall be restored or temporary detection must be provided within 24 hours.

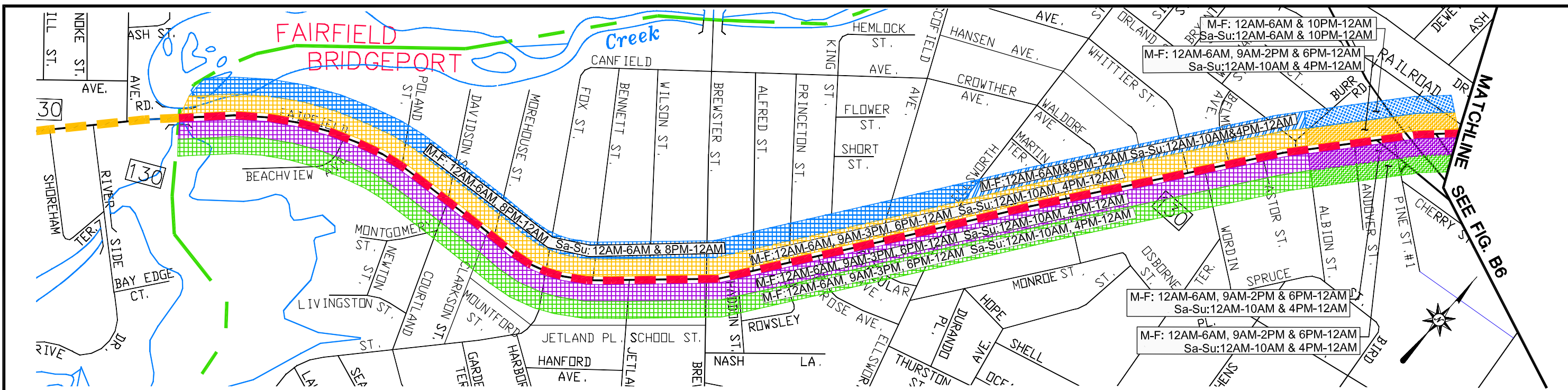
- Route 130 (Fairfield Avenue) at Davidson Street/Courtland Avenue (State Int. #015-341) - Potentially replace conduit and wiring located across the easterly leg of the Route 130 intersection during the duct bank installation process.
- Route 130 (Fairfield Avenue) at Fox Street/Gilman Street (State Int. #015-240) – Replace conduit and wiring located across the easterly Route 130 leg of the intersection during the vault construction process.
- Route 130 (Fairfield Avenue) at Brewster Street (State Int. #015-241) – Potentially replace conduit and wiring across the westerly Route 130 leg of the intersection during the duct bank installation process. Replace system detector (SD1) located along the westbound side of Route 130 (Fairfield Avenue) west of Brewster Street during both the duct bank installation and mill and overlay processes.
- Route 130 (Fairfield Avenue) at Ellsworth Street (State Int. #015-242) – Potentially replace conduit and wiring across the westerly Route 130 leg of the intersection during the duct bank installation phase.
- Route 130 (Fairfield Avenue) at Whittier Street/Wordin Avenue (State Int. #015-349) – Potentially replace conduit and wiring located across the westerly Route 130 leg of the intersection during the duct bank installation process. Replace system detectors (SD1 and SD2) located along Route 130 (Fairfield Avenue) westbound west of Whittier Street during both the duct bank installation and mill and overlay phases.
- Route 130 (Fairfield Avenue) at Orland Street/Burr Road/Albion Street (State Int. #015-244) – Potentially replace conduit and wiring located across the easterly Route 130 leg of the intersection during the duct bank installation process. Replace system detector (SD1) located along Route 130 (Fairfield Avenue) westbound west of Orland Street during the duct bank installation and mill and overlay phases.

See Figure B.5 for Allowable Work Hours Map.

See Section E for vault recommendations.

B. I-95 Ramps (Exit 25) at Route 130

1. The Contractor will not be allowed to perform any work that will interfere with the existing traffic operations:
 - Monday – Friday: 6:00 a.m. to 9:00 a.m.
3:00 p.m. to 6:00 p.m.
 - Saturday - Sunday: 10:00 a.m. to 9:00 p.m.
2. The I-95 Exit 25 northbound on-ramp intersection with Route 130 (Fairfield Avenue) is located along the proposed transmission line route. The Contractor shall maintain access to the ramp at all times. A Trafficperson will be present during all construction periods to direct traffic.
3. The I-95 Exit 25 southbound off-ramp intersection with Route 130 (Fairfield Avenue) is located along the transmission line route. The Contractor shall maintain access from the ramp to Route 130 at all times. A Trafficperson will be present during all construction periods to direct traffic.
4. At the following locations, traffic control signal equipment will be impacted by construction activities. Disturbed “local” loop detectors shall be restored or temporary detection must be provided within 24 hours.
 - Route 130 (Fairfield Avenue) at Pine Street and I-95 Northbound on-ramp (State Int. #015-245) – Potentially replace conduit and wiring located across the easterly Route 130 leg of the intersection during the duct bank installation phase. Replace local loop detectors (D3) located within the westbound Route 130 left turn lane during both the duct bank installation and mill and overlay phases.
 - Route 130 (Fairfield Avenue) at I-95 Southbound off-ramp (State Int. #015-246) – Protect the mast arm and traffic controller located within the southeasterly corner of the intersection during the duct bank installation phase. Potentially replace the conduit and wiring located across the I-95 Southbound off-ramp, across the easterly Route 130 leg of the intersection and across Railroad Avenue during the duct bank installation phase.



ONE LANE - EACH DIRECTION

WORK HOURS:

- RTE. 130 FROM TOWN LINE TO ALBION ST.:
MONDAY-FRIDAY: 12:00AM-6:00AM, 9:00AM-3:00PM,
6:00PM-12:00AM
SATURDAY-SUNDAY: 12:00AM-10:00AM, 4:00PM-12:00AM
- RTE. 130 FROM ALBION ST. TO RAILROAD AVE.:
MONDAY-FRIDAY: 12:00AM-6:00AM, 9:00AM-2:00PM,
6:00PM-12:00AM
SATURDAY-SUNDAY: 12:00AM-10:00AM, 4:00PM-12:00AM

WORK IN INNER EB & WB LANES -FOUR LANE UNDIVIDED HIGHWAY

ONE LANE - ALTERNATING TRAFFIC

WORK HOURS:

- RTE. 130 FROM TOWN LINE TO ELLSWORTH ST.:
MONDAY-FRIDAY: 12:00AM-6:00AM, 8:00PM-12:00AM
SATURDAY-SUNDAY: 12:00AM-6:00AM, 8:00PM-12:00AM
- RTE. 130 FROM ELLSWORTH ST. TO ALBION STREET.:
MONDAY-FRIDAY: 12:00AM-6:00AM, 9:00PM-12:00AM
SATURDAY-SUNDAY: 12:00AM-10:00AM, 4:00PM-12:00AM
- RTE. 130 FROM ALBION ST. TO RAILROAD AVE.:
MONDAY-FRIDAY: 12:00AM-6:00AM, 10:00PM-12:00AM
SATURDAY-SUNDAY: 12:00AM-6:00AM, 10:00PM-12:00AM

WORK IN TRAVEL LANE AND SHOULDER - TWO LANE HIGHWAY
ALTERNATING ONE-WAY TRAFFIC OPERATIONS
MAINTENANCE TRAFFIC CONTROL PLAN 13

TWO LANES - ONE DIRECTION

WORK HOURS:

- RTE. 130 FROM TOWN LINE TO ALBION ST.:
MONDAY-FRIDAY: 12:00AM-6:00AM, 9:00AM-3:00PM,
6:00PM-12:00AM
SATURDAY-SUNDAY: 12:00AM-10:00AM, 4:00PM-12:00AM
- RTE. 130 FROM ALBION ST. TO RAILROAD AVE.:
MONDAY-FRIDAY: 12:00AM-6:00AM, 9:00AM-2:00PM,
6:00PM-12:00AM
SATURDAY-SUNDAY: 12:00AM-10:00AM, 4:00PM-12:00AM

WORK IN BOTH LANES -FOUR LANE UNDIVIDED HIGHWAY
MAINTENANCE TRAFFIC CONTROL PLAN 12

LEGEND

- CL&P SELECTED ROUTE — BRIDGEPORT
- CL&P SELECTED ROUTE — FAIRFIELD (SEE SEPARATE REPORT)

ONE LANE - ONE DIRECTION

WORK HOURS:

WORK IN RIGHT/LEFT LANE -FOUR LANE UNDIVIDED HIGHWAY
MAINTENANCE TRAFFIC CONTROL PLAN 10 & 11

- RTE. 130 FROM TOWN LINE TO ALBION ST.:
MONDAY-FRIDAY: 12:00AM-6:00AM, 9:00AM-3:00AM,
6:00PM-12:00AM
SATURDAY-SUNDAY: 12:00AM-10:00AM, 4:00PM-12:00AM
- RTE. 130 FROM ALBION ST. TO RAILROAD AVE.:
MONDAY-FRIDAY: 12:00AM-6:00AM, 9:00AM-2:00PM,
6:00PM-12:00AM
SATURDAY-SUNDAY: 12:00AM-10:00AM, 4:00PM-12:00AM



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ALLOWABLE WORK HOURS MAP

MIDDLETOWN TO NORWALK
345-kV TRANSMISSION PROJECT
BRIDGEPORT, CONNECTICUT

Scale 1"=600'
Project No. 05C1314
Date 4/27/06
CAD File 015_TRPT05C1314 FIG B5

FIGURE B.5

XREF(s): NONE

C. Railroad Avenue

1. When the Contractor is not actively working the Contractor shall maintain and protect one lane of traffic on a paved travel path not less than 11 feet in width.
2. Nighttime work (Monday – Friday from 12:00 a.m. to 7:00 a.m. and from 6:00 p.m. to 12:00 a.m. and Saturday - Sunday from 12:00 a.m. to 9:00 a.m. and 6:00 p.m. to 12:00 a.m.) shall be prohibited between South Avenue and Park Avenue due to abutting residential developments. At least 48 hours notification for prohibition of on-street parking shall be posted.
3. When actively working, during the following periods, the Contractor will be allowed to close Railroad Avenue to through traffic, one block at a time, and detour traffic as shown on Detour Plans to be developed:
 - Monday – Sunday: No Restrictions
4. Where two-way traffic on a segment of Railroad Avenue is required for access to abutting properties, the Contractor shall provide uniformed Trafficpersons to direct traffic.
5. Temporary signs, markings, barricades, etc. shall be inspected by the Contractor on a daily basis.
6. At the following locations, traffic control signal equipment will be impacted by construction activities. Disturbed “local” loop detectors shall be restored or temporary detection must be provided within 24 hours.
 - Railroad Avenue at Bostwick Avenue – Shoring of the mast arm located within the southwest corner of the intersection is required during the construction of the duct bank connections to the vaults. This mast arm is directly adjacent to the duct banks. Potentially replace multiple conduit and wiring (including electrical service) at the southwest corner of the intersection during the construction of the duct bank connections to the vaults.
 - Railroad Avenue at Hancock Avenue – Replace the local detector (D2) and the system detector (SD1) located within the eastbound Railroad Avenue approach to the intersection during both the duct bank installation and mill and overlay phases. Potentially replace the conduit and wiring located across the westerly Railroad Avenue leg of the intersection during the duct bank installation process.
 - Railroad Avenue at Howard Avenue – Replace the pedestal with pedestrian push button and pedestrian signal face and adjacent handhole located within the southwest corner of the intersection during the duct bank installation process. Shore the mast arm also located within the southwest corner of the intersection

during the duct bank installation phase. Replace local loop detector (D2) and system loop detector (SD1) located within the eastbound approach to the intersection and local loop detector (D8 and D8A) located within the Howard Avenue northbound approach to the intersection during the duct bank installation, construction of the duct bank connections to the vaults, and during the mill and overlay process. Potentially replace multiple conduit and wiring near the southwest corner of the intersection and across the Howard Avenue southerly leg of the intersection.

- Railroad Avenue at Wordin Avenue – Replace local detector (D2) and system loop detector (SD1) located within the Railroad Avenue eastbound approach to the intersection during the mill and overlay process. Protect or potentially replace the handholes located along the southerly side of Railroad Avenue during the duct bank installation phase. Potentially replace the conduit and wiring from detectors D2 and SD1 to the adjacent handholes.
- Railroad Avenue at Iranistan Avenue – Replace local detector (D2) and system detector (SD1) located within the Railroad Avenue eastbound approach to the intersection during the mill and overlay process. Protect or potentially replace the handholes located along the southerly side of Railroad Avenue during the duct bank installation phase. Potentially replace the conduit and wiring from detectors D2 and SD1 to the adjacent handholes. Replace the pedestal with pedestrian push button and pedestrian signal face during the duct bank installation phase. Potentially replace the multiple conduit and wiring located across the easterly Railroad Avenue leg of the intersection.
- Railroad Avenue at South Avenue – Replace system detector (SD1) located within the eastbound Railroad Avenue approach to the intersection during the mill and overlay process and the duct bank installation process. Replace the handhole adjacent to detector (SD1) during the bank installation process. Replace local loop detector D2 also located within the eastbound Railroad Avenue approach to the intersection during the mill and overlay process. Potentially replace the conduit and wiring located across South Avenue southerly leg of the intersection during the construction of the duct bank connections to the vaults. Replace local loop detector D8 and D8A located at the South Avenue northbound approach to the intersection during the construction of the duct bank connections to the vaults and during the full depth reconstruction process. Replace the mast arm, handhole, conduit and wiring located within the southeasterly corner of the intersection during the vault construction process. Potentially replace the conduit and wiring located across Columbia Street during construction of the duct bank connections to the vaults.
- Railroad Avenue at Park Avenue – Potentially replace the multiple conduits and wiring located across the easterly Railroad Avenue leg of the intersection as well as the conduit and wiring located across the westerly Railroad Avenue leg of the intersection during the duct bank installation process. Replace local loop detector

(D2) and system loop detector (SD1) during both the duct bank installation and mill and overlay process.

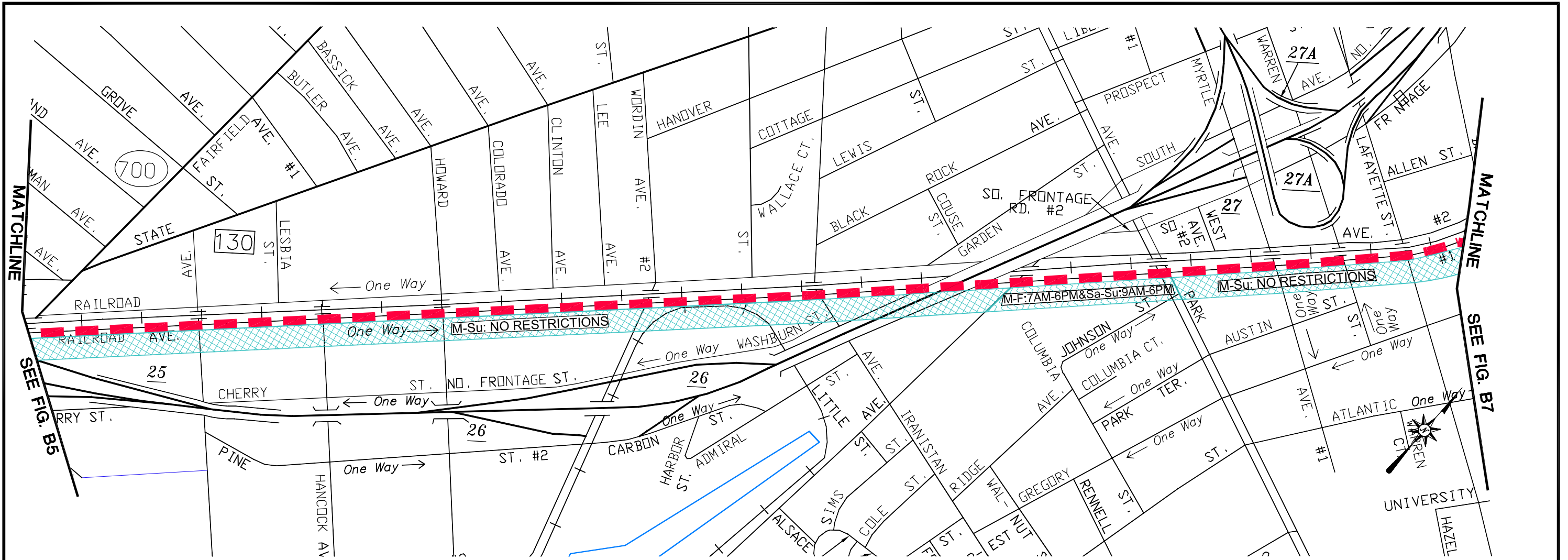
- Railroad Avenue at Myrtle Avenue – Replace local loop detector (D2) and system loop detector (SD1) during both the duct bank installation and mill and overlay process. Potentially replace the conduits and wiring located across the easterly Railroad Avenue leg of the intersection during the duct bank installation process.
- Railroad Avenue at Warren Avenue – Replace local loop detector (D2) located within the Railroad Avenue eastbound approach to the intersection during the mill and overlay process. Protect or replace the handhole adjacent to detector D2 during construction of the duct bank connections to the vaults. Replace system loop detector (SD1) also located within the Railroad Avenue eastbound approach to the intersection during both the duct bank installation and mill and overlay processes. Replace the conduit and wiring between the handholes for detectors D2 and SD1 during the vault construction phase. Potentially replace the conduits and wiring located across the westerly Railroad Avenue leg of the intersection.
- Railroad Avenue at Lafayette Street – Replace local loop detector D2 located within the eastbound Railroad Avenue approach to the intersection during the mill and overlay process. Replace system loop detector (SD1) also located within the eastbound Railroad Avenue approach to the intersection during both the duct bank installation and mill and overlay phases. Replace local detectors D8 and D8A located on the northbound Lafayette Street approach to the intersection during both the duct bank installation and mill and overlay processes. Potentially replace the conduit and wiring between detectors SD1 and D2 during the duct bank installation process.
- Railroad Avenue at Broad Street – Replace system detector (SD1) located on the Railroad Avenue eastbound approach to the intersection during the duct bank installation and mill and overlay process. Replace the handhole located adjacent to the detector SD1 and potentially replace the conduit and wiring located between the handholes for detectors SD7 and D2/D2A during the duct bank installation process. Replace local loop detector D8A located on the Broad Street northbound approach to the intersection during the mill and overlay process.

See Figures B.6 and B.7 for Allowable Work Hours Maps.

See Section E for vault recommendations.

D. Broad Street, Main Street

1. The Contractor will not be allowed to perform any nighttime work (Monday – Friday from 12:00 a.m. to 7:00 a.m. and from 6:00 p.m. to 12:00 a.m. and Saturday - Sunday from 12:00 a.m. to 9:00 a.m. and 6:00 p.m. to 12:00 a.m.) due to abutting residential land uses.
2. When the Contractor is not actively working s/he shall maintain and protect one lane of traffic in each direction on a paved travel path not less than 22 feet in total width.
3. When actively working, during the following periods, the Contractor will be allowed to close roadways to through traffic, one block at a time, and detour traffic as shown on Detour Plans to be developed:
 - Monday – Friday: 7:00 a.m. to 6:00 p.m.
4. When actively working, during the following periods, the Contractor will be allowed to maintain and protect at least an alternating one-way traffic operation on a paved travel path not less than 11 feet in width. The length of the alternating one-way traffic operation shall not exceed 300 feet, and shall require Trafficperson(s). One-way traffic operation shall be in accordance with a ConnDOT Maintenance Traffic Control Plan (see Appendix XIII for ConnDOT Maintenance Traffic Control Plans). On-street parking shall be prohibited during construction. At least 48 hours notification for prohibition of on-street parking shall be posted.
 - Monday – Friday: 7:00 a.m. to 6:00 p.m.
5. The residents in this neighborhood rely on off-street parking due to limited driveway parking, therefore it is important to find temporary parking for displaced vehicles. There are several potential areas for temporary parking in the vicinity that CL&P should investigate leasing for this purpose. These areas include two areas on the corners of the Broad Street at University Avenue intersection: a parking lot at a restaurant building on the northeast corner and a University of Bridgeport parking lot on the northwest corner. There is also a parking lot between Railroad Avenue and Whiting Avenue, as well as the parking area where the vaults 7549 and 6449 are proposed.



LEGEND

— CL&P SELECTED ROUTE

RESTRICTIONS:

- 1) WHEN THE CONTRACTOR IS NOT ACTIVELY WORKING, S/HE MUST MAINTAIN AND PROTECT ONE LANE OF TRAFFIC ON A 11' WIDE (MINIMUM) PAVED TRAVEL PATH.
- 2) WHEN ACTIVELY WORKING, THE CONTRACTOR WILL BE ALLOWED TO CLOSE RAILROAD AVENUE TO THROUGH TRAFFIC, ONE BLOCK AT A TIME, AND DETOUR TRAFFIC AS SHOWN ON DETOUR PLANS INCLUDED IN THE MAINTENANCE AND PROTECTION OF TRAFFIC SPECIAL PROVISIONS.



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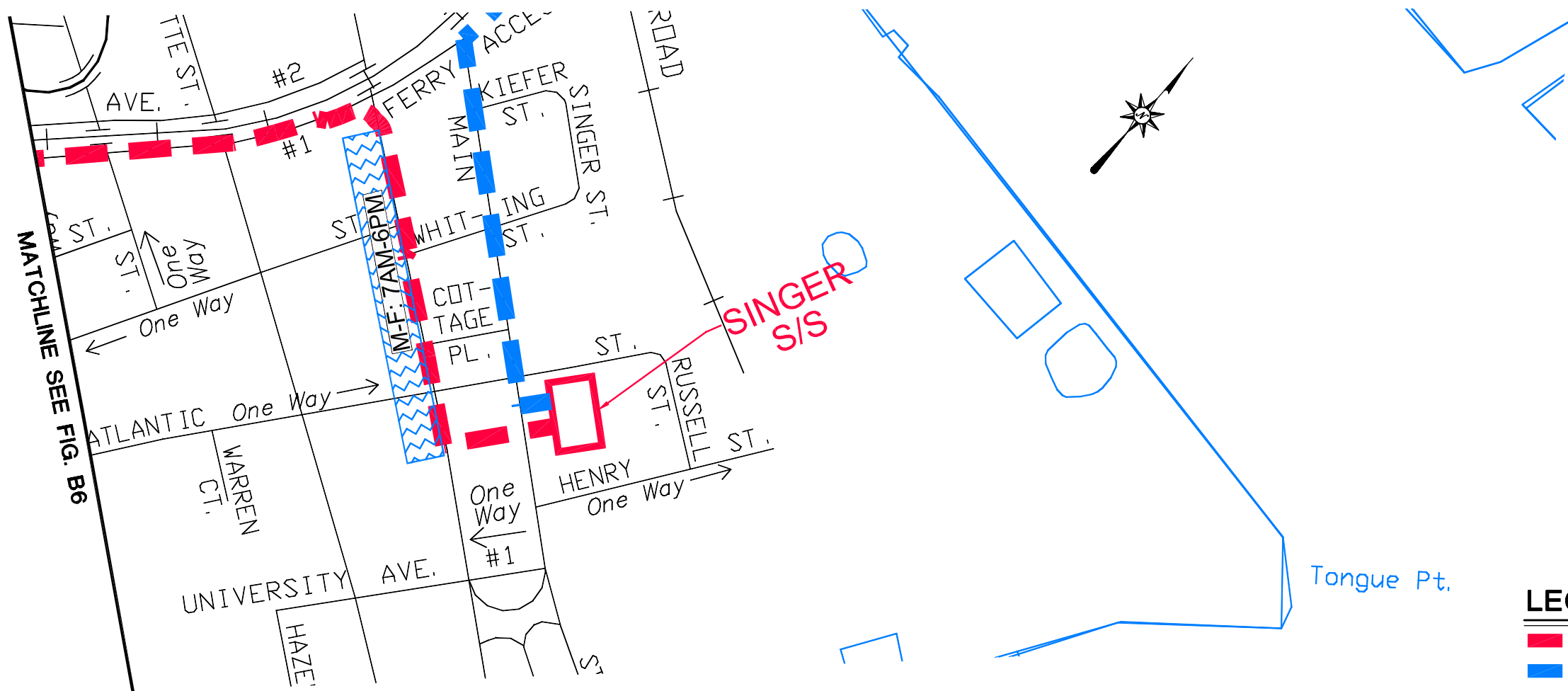
ALLOWABLE WORK HOURS MAP

MIDDLETOWN TO NORWALK
345-kV TRANSMISSION PROJECT
BRIDGEPORT, CONNECTICUT

Scale 1"=600'
Project No. 05C1314
Date 4/17/06
CAD File 015_TRPT05C1314 FIG B6

FIGURE B.6

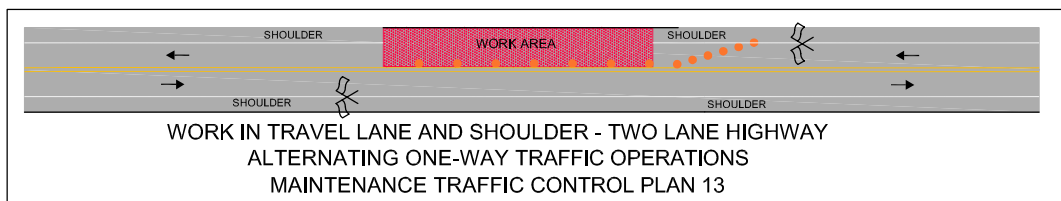
XREF(s): NONE



LEGEND

- CL&P SELECTED ROUTE
- UI SELECTED ROUTE

ONE LANE - ALTERNATING TRAFFIC



- WORK HOURS:**
- BROAD STREET:
MONDAY-FRIDAY: 7:00AM-6:00PM
 - UNIVERSITY AVENUE:
MONDAY-FRIDAY: 7:00AM-6:00PM
 - MAIN STREET:
MONDAY-FRIDAY: 7:00AM-6:00PM

- RESTRICTIONS:**
- 1) WHEN THE CONTRACTOR IS NOT ACTIVELY WORKING, S/HE MUST MAINTAIN AND PROTECT ONE LANE OF TRAFFIC IN EACH DIRECTION ON A PAVED TRAVEL PATH NOT LESS THAN 22 FEET IN TOTAL WIDTH.
 - 2) WHEN ACTIVELY WORKING, THE CONTRACTOR WILL BE ALLOWED TO CLOSE ROADWAYS TO THROUGH TRAFFIC, ONE BLOCK AT A TIME, AND DETOUR TRAFFIC AS SHOWN ON THE DETOUR PLANS INCLUDED IN THE MAINTENANCE AND PROTECTION OF TRAFFIC SPECIAL PROVISIONS.



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ALLOWABLE WORK HOURS MAP

MIDDLETOWN TO NORWALK
345-kV TRANSMISSION PROJECT
BRIDGEPORT, CONNECTICUT

Scale 1"=500'
Project No. 05C1314
Date 4/27/06
CAD File 015_TRPT05C1314 FIG B7

FIGURE B.7

XREF(s): NONE

6. Temporary signs, markings, barricades, etc. shall be inspected by the Contractor on a daily basis.

See Figure B.7 for Allowable Work Hours Map.

See Section E for vault recommendations.

E. Vault Installation/Construction

The following are recommendations specific to vault installation and construction and are in addition to the previously listed recommendations. Aerial photographs of each vault location are provided in Appendix VI, and ConnDOT Maintenance Traffic Control Plans can be found in Appendix XIII. In general, duct-bank connections to off-street vaults will require special provisions to be addressed in Maintenance and Protection of Traffic Plans. From west to east along the selected route the following provides specific recommendations for each vault location:

Vaults 7539 and 6439 are located within a vacant lot at the northeasterly corner of the Route 130 (Fairfield Avenue) at Pacanow Street intersection. Although the vaults do not encroach on either Pacanow Street or Route 130 (Fairfield Avenue), the duct bank connection construction will encroach upon Pacanow Street. There are no adjacent residential properties to restrict nighttime work, however, a variance for the City's Noise Control Regulations would be required. The following are specific recommendations for the vault and duct bank connection construction:

- The Contractor will close the sidewalk within the construction zone and provide a temporary bituminous sidewalk for safe pedestrian passage according to the Maintenance and Protection of Traffic Plans.
- The Contractor will be allowed to close the curbside (outside) westbound Route 130 (Fairfield Avenue) shoulder during construction in accordance with the Maintenance and Protection of Traffic Plans. The Contractor is permitted to work during the allowable hours determined in Section A of the Specific Recommendations.
- The Contractor will install temporary concrete barrier curb around the vault construction area according to the Maintenance and Protection of Traffic Plans.
- The Contractor shall install fencing around the vault construction area.
- A Trafficperson will be required during vault and duct bank connection construction to maintain access to/from Pacanow Street.

Vaults 7540 and 6440 are located in the middle of Route 130 (Fairfield Avenue) at the intersection of Route 130, Fox Street and Gilman Street. The following are specific recommendations for the vault and duct bank connection construction:

- The Contractor will be allowed to close the median-side (inside) eastbound travel lane and median-side (inside) westbound travel lane on Route 130 (Fairfield

Avenue) during construction in accordance with the Maintenance and Protection of Traffic Plans. The Contractor shall be permitted to work during the allowable hours determined in Section A of the Specific Recommendations.

- A Trafficperson will be required during vault and duct bank connection construction to maintain access to/from Fox Street and Gilman Street.
- The Contractor shall install steel support system over the excavation(s) to accommodate traffic during non-construction periods.
- During vault and duct bank connection construction, the following traffic movements will be restricted:
 - The southbound through movement from Fox Street to Gilman Street.
 - Left turns from Route 130 to both Fox Street and Gilman Street.

Vaults 7541 and 6441 are located west of the intersection of Route 130 and Melrose Street. Vault 7541 is located in the median area and roadway of Route 130 (Fairfield Avenue) and Vault 6441 is located on City property along the southerly side of Route 130 (Fairfield Avenue) on the former/future site of the Bridgeport Public Library – Black Rock Branch. Vault 6441 is located outside of the Route 130 roadway, but the vault and duct bank connection construction zone will impact Melrose Street. The following are specific recommendations for the vault and duct bank connection construction:

- The Contractor will be allowed to close the median-side (inside) eastbound travel lane and median-side (inside) westbound travel lane on Route 130 (Fairfield Avenue) during construction in accordance with the ConnDOT Maintenance Traffic Control Plans (See Appendix XIII) to be modified for a median. The Contractor shall be permitted to work during the allowable hours determined in Section A of the Specific Recommendations.
- The Contractor shall install steel support system over the excavation(s) to accommodate traffic during non-construction periods.
- During the following times the Contractor will be allowed to maintain one travel lane of alternating one-way traffic on Melrose Street during construction in accordance with the ConnDOT Maintenance Traffic Control Plans (See Appendix XIII) to be modified for a median:
 - Monday – Friday: 7:00 a.m. to 6:00 p.m.
 - Saturday - Sunday: 9:00 a.m. to 6:00 p.m.
- For Vault 7541, the Contractor shall install steel support system over excavation to accommodate traffic during non-construction periods.
- The Contractor shall close the sidewalk within the construction zone and provide a pedestrian detour for safe pedestrian passage according to the Maintenance and Protection of Traffic Plans.
- Remove on-street parking along the south side of Route 130 (Fairfield Avenue). There is no parking allowed on the north side of Route 130 between Alfred Street and Princeton Street. (Although it is not ideal to prohibit on-street parking on the south side of Route 130 between Rowley Street and Melrose Street, the two

buildings that occupy this block, an apartment complex and the former/future library building, both have available parking in parking lots or on side streets.)

- Relocate the bus stops located at the southwesterly corner of Route 130 (Fairfield Avenue) intersection and Melrose Street and at the northeasterly corner of the Route 130 (Fairfield Avenue) and Princeton Street intersection.
- Temporarily relocate mid-block crosswalk. Trafficpersons will be required at the temporary crosswalk during construction work hours because of the potential for inadequate sight distances around the construction zone.

Vaults 7542 and 6442 are located south of Route 130 (Fairfield Avenue) at the intersection with Martin Terrace. Vault 6442 is located on the Martin Terrace roadway and although Vault 7542 is located outside of the Route 130 (Fairfield Avenue) and Martin Terrace roadways, the construction zone will impact both roadways. The following are specific recommendations for the vault and duct bank connection construction:

- During the following times the Contractor will be allowed to close Martin Terrace at Route 130 (Fairfield Avenue) to through traffic in accordance with a detour plan included in the Maintenance and Protection of Traffic Special Provisions:
 - Monday – Friday: 7:00 a.m. to 6:00 p.m.
 - Saturday - Sunday: 9:00 a.m. to 6:00 p.m.
- The Contractor will be allowed to close the curbside (outside) eastbound shoulder on Route 130 (Fairfield Avenue) during construction in accordance with the Maintenance and Protection of Traffic Plans. The Contractor shall be permitted to work as determined in Section A of the Specific Recommendations.
- The Contractor will close the sidewalk within the construction zone and provide a pedestrian detour for safe pedestrian passage in accordance with the Maintenance and Protection of Traffic Plans.

Vaults 7543 and 6443 are located along the westbound side of Route 130 (Fairfield Avenue) and within the median area between Albion Street and Andover Street. The following are specific recommendations for the vault and duct bank connection construction:

- During the following times the Contractor will be allowed to close both westbound Route 130 (Fairfield Avenue) travel lanes with bi-directional travel on the eastbound side of Route 130 (Fairfield Avenue) in accordance with the Maintenance and Protection of Traffic Plans. The Contractor shall be permitted to work as determined in Section A of the Specific Recommendations.
- Trafficpersons are recommended due to the proximity to the Route 130 (Fairfield Avenue) intersection with Albion Street/Burr Road/Orland Street.
- The Contractor shall install steel support system over the excavation(s) to accommodate traffic during non-construction periods.

- The Contractor must maintain access to the Andover School at all times. The use of a Trafficperson is recommended.

Vaults 7544 and 6444 are located along the southerly side of Railroad Avenue (Eastbound) just west of Bostwick Avenue. Although the vaults are outside of the Railroad Avenue roadway, the construction zone will impact Railroad Avenue (eastbound) and the duct bank connection construction will affect Bostwick Avenue. The following are specific recommendations for the vault and duct bank connection construction:

- The Contractor will be allowed to maintain one travel lane of alternating one-way traffic on Bostwick Avenue during construction in accordance with the ConnDOT Maintenance Traffic Control Plans (See Appendix XIII).
- The Contractor must maintain a 12' foot (minimum) travel lane on Railroad Avenue at all times.
- The Contractor will close the sidewalk within the vault and duct bank connection construction zone and provide for safe pedestrian passage.

Vaults 7545 and 6445 are located on private property along the southerly side of Railroad Avenue (Eastbound) just east of Howard Avenue. Although the vaults are outside of the Railroad Avenue roadway, the vault and duct bank connection construction area will impact Howard Avenue and may impact Railroad Avenue (eastbound). The following are specific recommendations for the vault and duct bank construction:

- The Contractor will be allowed to maintain one travel lane of alternating one-way traffic on Howard Avenue during construction in accordance with the ConnDOT Maintenance Traffic Control Plans (See Appendix XIII).
- The Contractor must maintain a 12' foot (minimum) travel lane on Railroad Avenue at all times.
- The Contractor will close the sidewalk along the southerly side of Railroad Avenue.

Vaults 7546 and 6446 is located within Railroad Avenue (Eastbound) between Wordin Avenue and Iranistan Avenue. The following are specific recommendations for the vault and duct bank connection construction:

- It is recommended that Railroad Avenue (Eastbound) between Wordin Avenue and Harbor Street (a paper street) be closed in accordance with detour plans included in the Maintenance and Protection of Traffic Special Provisions. The Contractor shall be permitted to work as determined in Section C of the Specific Recommendations.
- Coordinate access with abutting properties for access either along Railroad Avenue (Eastbound) from Iranistan Avenue or via Washburn Avenue.
- Provide two-way travel on Railroad Avenue (Eastbound) for local business access only, if needed, between Harbor Street and Iranistan Avenue.

- Modify the traffic signal of Railroad Avenue (Eastbound) with Iranistan Avenue if two-way travel on Railroad Avenue (Eastbound) is necessary for local business access.
- There are no adjacent residential land uses to limit night work, however, nighttime work exceeding the City's Noise Control Regulations would require a variance from the City.

Vaults 7547 and 6447 are located partially within private property and partially within the local street right-of-way. Specifically, the vaults are located within the southeasterly corner of the Railroad Avenue (Eastbound) and South Avenue intersection. Although the vaults do not impact the roadways, the vault and duct bank connection construction area will impact Railroad Avenue (Eastbound), Columbia Street and South Avenue. The following are specific recommendations for the vault and duct bank connection construction:

- Due to proximity to the #400 Iranistan Avenue Apartments, all construction work in this area shall be limited to daytime hours (Monday through Friday from 7:00 a.m. to 6:00 p.m. Saturday and Sunday from 9:00 a.m. to 6:00 p.m.)
- The Contractor will install temporary concrete barrier curb around the vault construction site in accordance with the Maintenance and Protection of Traffic Plans.
- The Contractor will install fencing around the vault construction site.
- The Contractor will close the sidewalk within the vault construction area and provide a pedestrian detour for safe pedestrian passage in accordance with the Maintenance and Protection of Traffic Plans.
- The construction area may extend into Railroad Avenue (Eastbound) and require road closure of Railroad Avenue (Eastbound) between South Avenue and Park Avenue in accordance with a detour plan included in the Maintenance and Protection of Traffic Special Provisions.
- The construction area will also require the closure of Columbia Street and South Avenue (separately) in accordance with detour plans included in the Maintenance and Protection of Traffic Special Provisions.
- The Contractor shall install steel support system over the excavation(s) during non-construction periods.

Vaults 7548 and 6448 are located within the Railroad Avenue (Eastbound) right-of-way, just west of Warren Street. The following are specific recommendations for the vault and duct bank connection construction:

- The Contractor will be allowed to close Railroad Avenue (eastbound) to through traffic in accordance with a detour plan included in the Maintenance and Protection of Traffic Special Provisions. The Contractor shall be permitted to work as determined in Section C of the Specific Recommendations.
- There are no adjacent residential land uses to limit night work, however, nighttime work exceeding the City's Noise Control Regulations would require a variance from the City.

Vaults 7549 and 6449 are located outside of the City right-of-way, within a parking lot owned by the City of Bridgeport Housing Authority. The vault location is on the easterly side of Broad Street, between Railroad Avenue (Eastbound) and Whiting Street. Construction work hours should be limited to daytime hours (Monday through Friday, between 7:00 a.m. and 6:00 p.m.) due to adjacent residential land uses.

- The Contractor will close the sidewalk within the vault construction area and provide a pedestrian detour for safe pedestrian passage

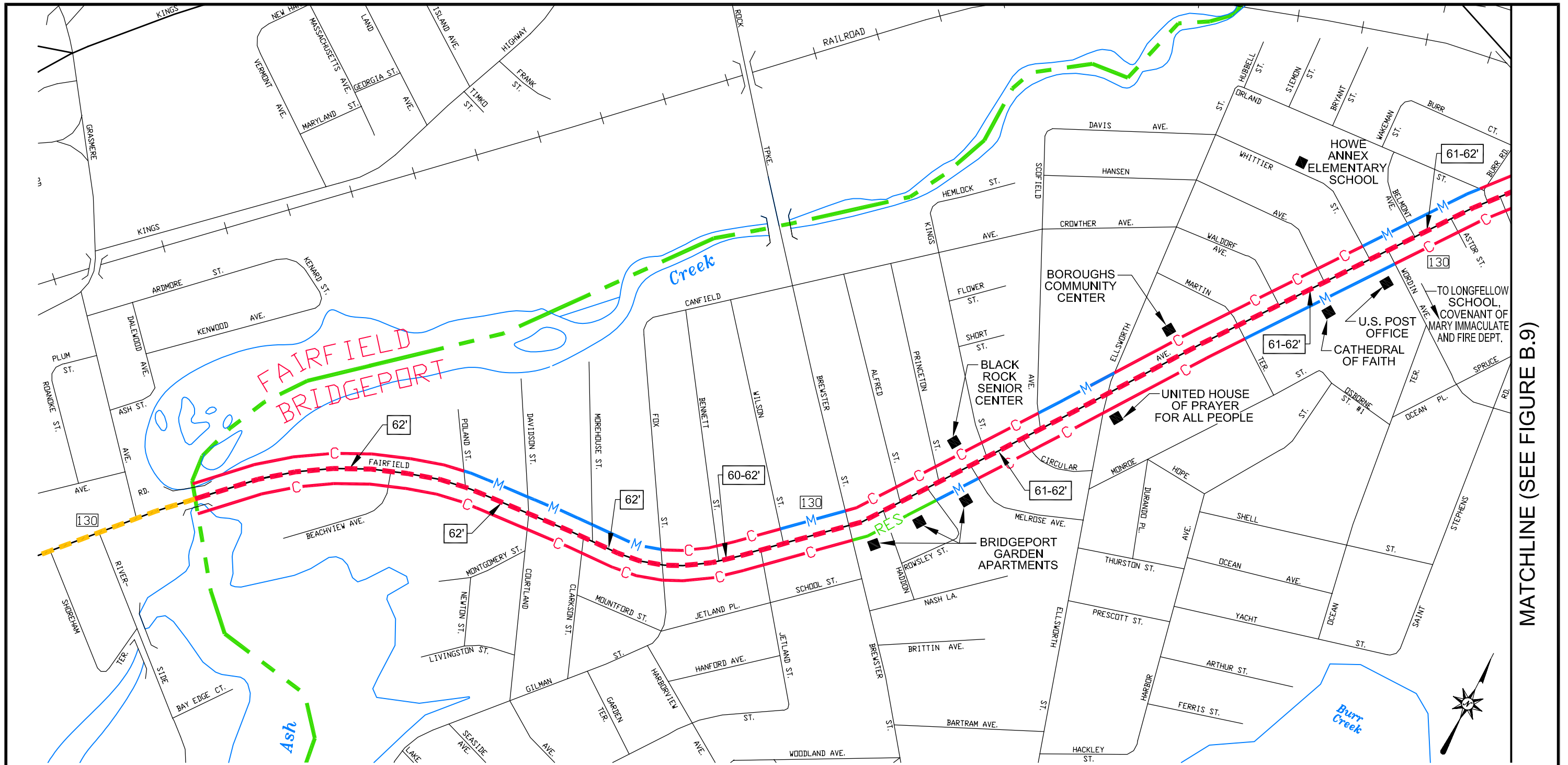
APPENDIX II

ROUTE INVENTORY

**CL&P - MIDDLETOWN TO NORWALK
ROUTE INVENTORY**

Town/City: Bridgeport

Roadway Name	From	To	Distance (feet)	# Travel Lanes	Width (c-c)	Speed Limit	Sidewalks Location	Parking Location	Illum. Y/N	Bus Route	ADT	PK. Hr. Volume	Traffic Signals at:	Comments	Abutting Commercial	Abutting Residential	Land Uses Of Concern
CL&P Selected Route:																	
Route 130 (Fairfield Ave.)	Fairfield TL	Poland Street	1,375	4	62	30	N & S	NO PKNG	Y	Y	15,500	1,500 (7am)		Divided (28' c-c each side);Raised planted median (5'-6' wide)	Y	N	
	Poland Street	Davidson/Coutland	340	4	62	30	N & S	NO PKNG EB; PKNG WB	Y	Y				Divided (28' c-c each side);Raised planted median (5'-6' wide)	Y	Y	
	Davidson/Courtland	Fox/Gilman	715	4	62	30	N & S	EB & WB	Y	Y			Courtland	Divided (28' c-c each side);Raised planted median (5'-6' wide)	Y	Y	
	Fox/Gilman	Brewster	945	4	60-62	30	N & S	EB & WB	Y	Y	13,300	1,380 (7am)	Fox/Gilman	Divided (28' c-c each side);Raised planted median (5'-6' wide)	Y	Y	
													Brewster	Unsignalized crosswalks at Melrose			
	Brewster	Ellsworth	1,450	4	61-62	30	N & S	EB & WB	Y	Y	14,200	1,300 (4pm)		Divided (28' c-c each side);Raised planted median (5'-6' wide)	Y	Y	Senior Center
	Ellsworth	Wordin/Whittier	1,465	4	61-62	30	N & S	EB & WB	Y	Y	17,500	1,600 (4pm)	Ellsworth	Divided (28' c-c each side);Raised planted median (5'-6' wide)	Y	N	Churches; Community Center; P.O.
	Wordin/Whittier	Albion/Orland	720	4	61-62	25	N & S	EB & WB	Y	Y	19,400	1,690 (4pm)	Wordin/Whittier	Divided (28' c-c each side);Raised planted median (5'-6' wide)	Y	Y	
	Albion/Orland	Pine/I-95 NB on	515	4	62	25	N & S	EB & WB	Y	Y	23,800	2,000 (4pm)	Albion/Orland/Burr (5-legs)	Divided (28' c-c each side);Raised planted median (5'-6' wide)	Y	N	
	Pine/I-95 NB on	I-95 SB off	250	4	62	25	N & S	EB & WB	Y	Y	22,400	1,740 (4pm)	Pine/I-95 NB on	Divided (28' c-c each side);Raised planted median (5'-6' wide)	-	-	
	I-95 SB off	Railroad	130	2	38	25	N & S	-	Y	Y			I-95 SB off		-	-	
Railroad Ave (Eastbound)	Route 130 (Fairfield)	Bostwick	965	1	17-33	25	S	-	Y	N	250	30 (7am)	Bostwick	One-way EB	Y	N	
	Bostwick	Hancock	675	1	26-33	25	-	-	Y	N			Hancock	One-way EB	N	N	
	Hancock	Howard	650	1	27-30	25	-	-	Y	N			Howard	One-way EB	N	N	
	Howard	Wordin/Whittier	1,100	1	21-22	25	-	-	Y	N	400	40 (3pm)	Wordin	One-way EB	Y	N	
	Wordin/Whittier	Iranistan	935	1	19-22	25	-	-	Y	N			Iranistan	One-way EB	Y	N	
	Iranistan	South	1,070	1	18-28	25	-	-	Y	N			South	One-way EB	N	N	
	South	Park	830	1	24-30	25	S	EB	Y	N			Park	One-way EB	N	Y	
	Park	Myrtle	580	1	30	25	S	-	Y	N	650	60 (5pm)	Myrtle (one-way SB)	One-way EB	N	N	
	Myrtle	Warren	370	1	29-30	25	S	-	Y	N			Warren (one-way NB)	One-way EB	N	N	
	Warren	Lafayette	340	1	28-30	25	S	-	Y	N			Lafayette	One-way EB	N	N	
	Lafayette	Broad	465	1	29-33	25	S	-	Y	N	1,100	100 (2pm)	Broad	One-way EB	N	N	
Broad St	Railroad	Private Property	1,680	2	30	-	E & W	NB & SB	Y	Y	800	160 (8am)	Railroad		Y	Y	Church
Main St	Private Property	Singer Substation	36	2	36	-	E & W	NB & SB	Y	Y					Y	Y	



MATCHLINE (SEE FIGURE B.9)

LEGEND

- - - - CL&P SELECTED ROUTE - BRIDGEPORT
- - - - CL&P SELECTED ROUTE - FAIRFIELD
- - - - RES - RESIDENTIAL LAND USE
- - - - C - COMMERCIAL LAND USE
- - - - M - MIXED LAND USE (COMMERCIAL AND RESIDENTIAL)
- - LAND USE OF CONCERN
- XX' - TYPICAL CURB-TO-CURB WIDTH



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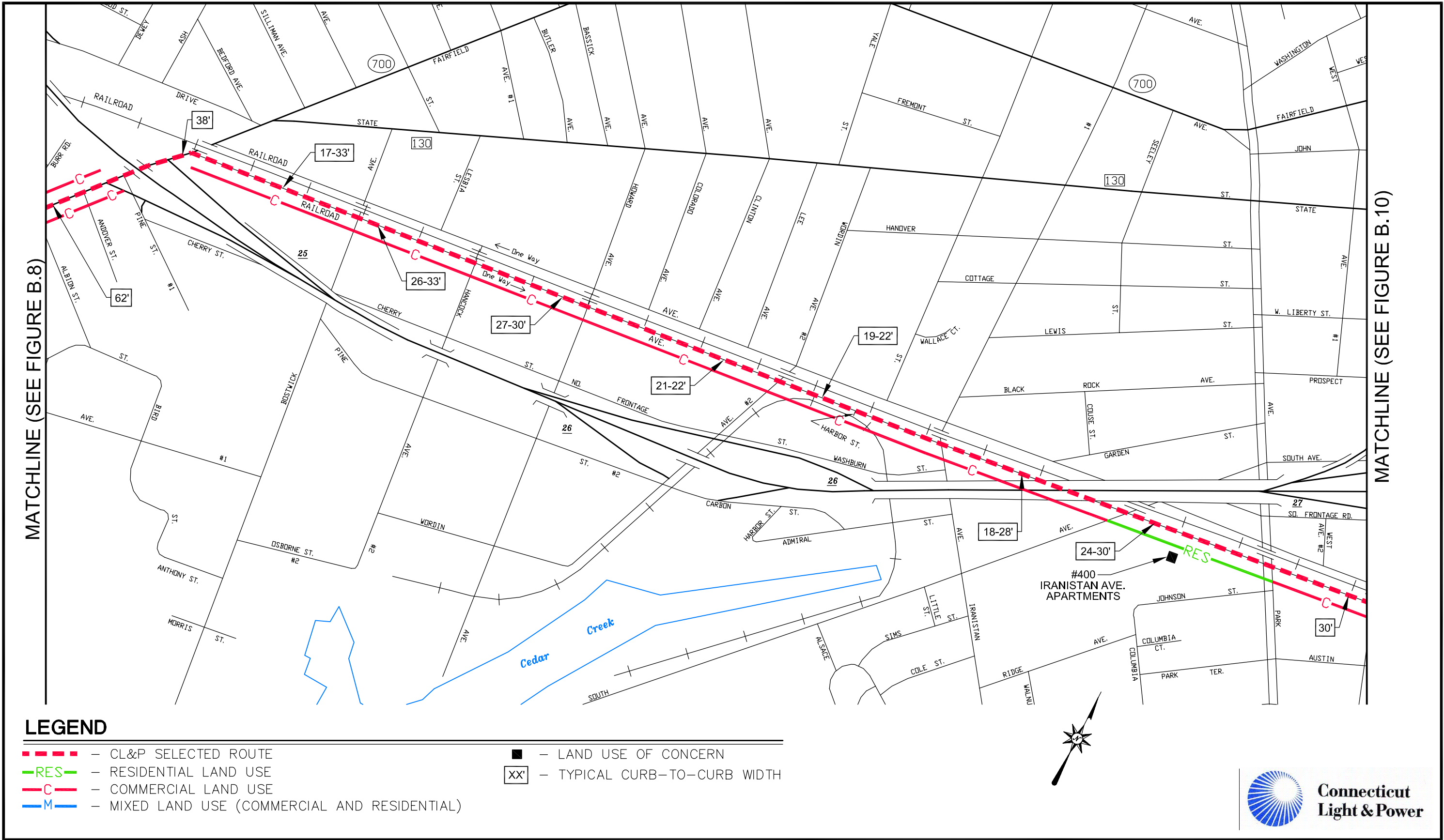
LAND USES AND ROADWAY WIDTHS

MIDDLETOWN TO NORWALK
345-kV TRANSMISSION PROJECT
BRIDGEPORT, CONNECTICUT

Scale 1"=500'
Project No. 05C1314
Date 8/17/05
Revised 4/17/06
CAD File 015_TRPT05C1314 FIG B8B9B10

FIGURE B.8

XREF(s): NONE



BL Companies

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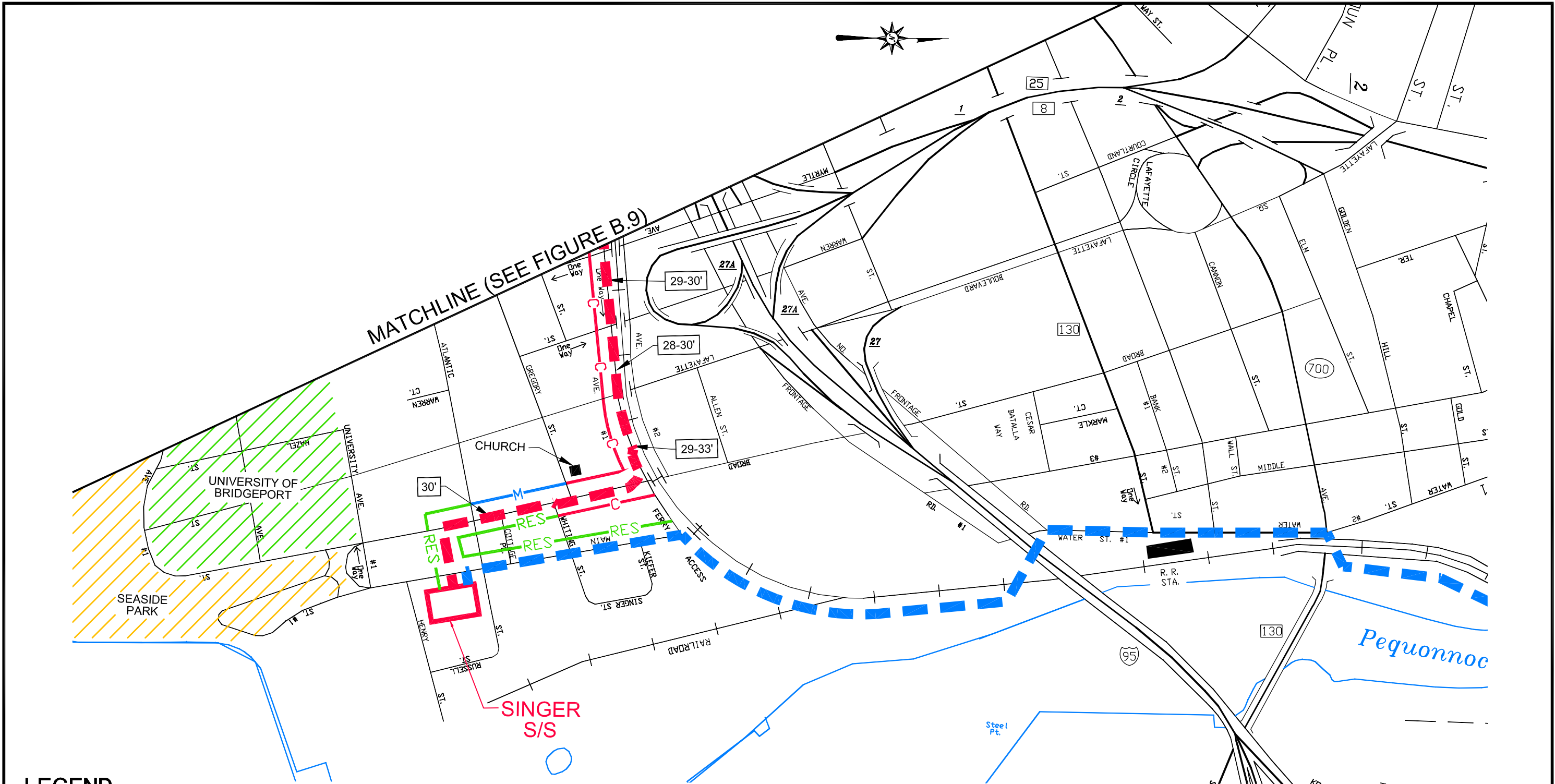
LAND USES AND ROADWAY WIDTHS

MIDDLETOWN TO NORWALK
345-kV TRANSMISSION PROJECT
BRIDGEPORT, CONNECTICUT

Scale 1"=500'
Project No. 05C1314
Date 8/17/05
Revised 4/17/06
CAD File 015_TRPT05C1314 FIG B8B9B10

FIGURE B.9

XREF(s): NONE



LEGEND

- - CL&P SELECTED ROUTE
- - THE UNITED ILLUMINATING COMPANY SELECTED ROUTE
- RES - RESIDENTIAL LAND USE
- C - COMMERCIAL LAND USE
- M - MIXED LAND USE (COMMERCIAL AND RESIDENTIAL)
- - LAND USE OF CONCERN
- xx' - TYPICAL CURB-TO-CURB WIDTH



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LAND USES AND ROADWAY WIDTHS

MIDDLETOWN TO NORWALK
345-kV TRANSMISSION PROJECT
BRIDGEPORT, CONNECTICUT

Scale 1"=500'
Project No. 05C1314
Date 8/17/05
Revised 4/27/06
CAD File 015_TRPT05C1314 FIG B889B10

FIGURE B.10

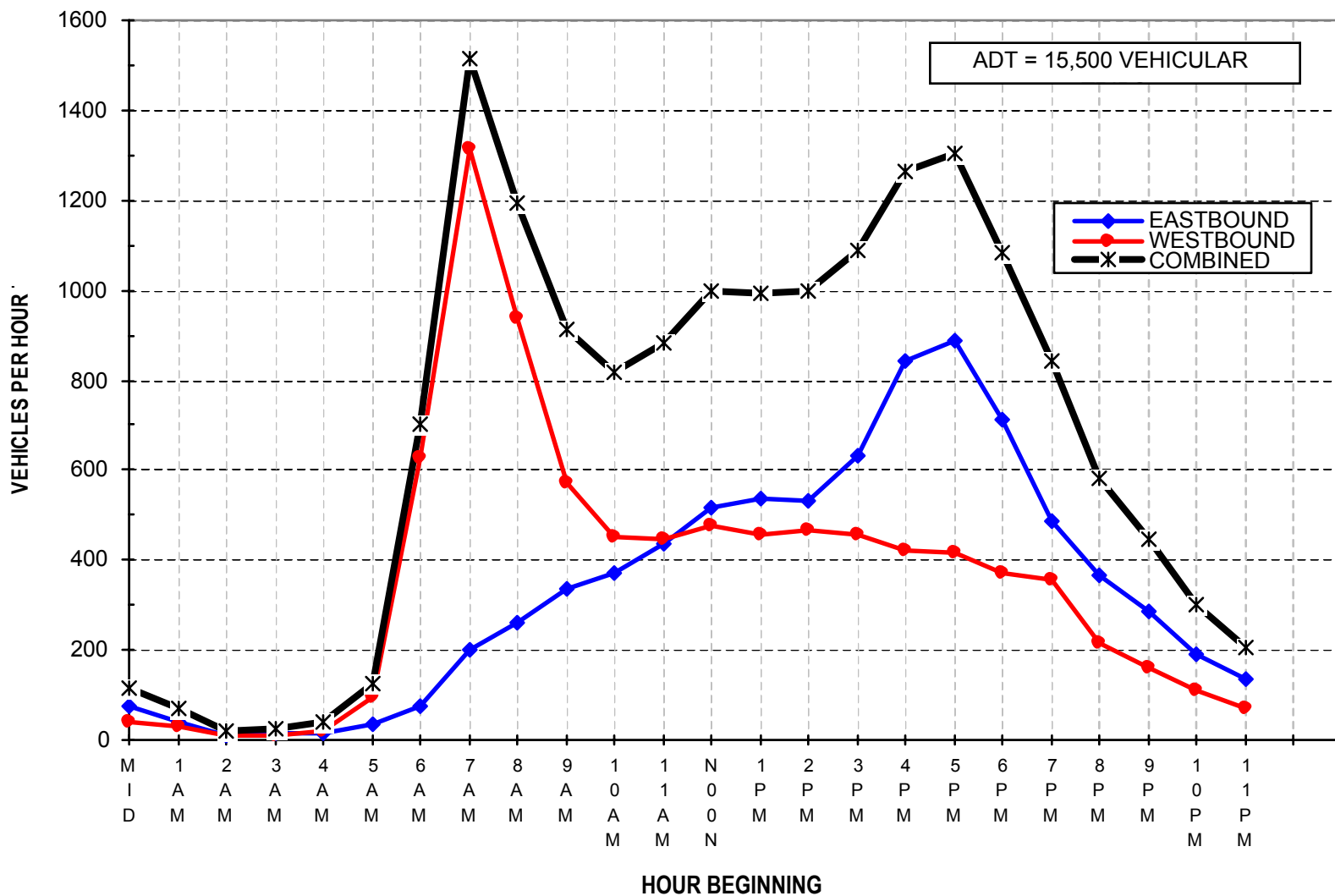
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APPENDIX III

SELECTED TRAFFIC COUNT GRAPHS

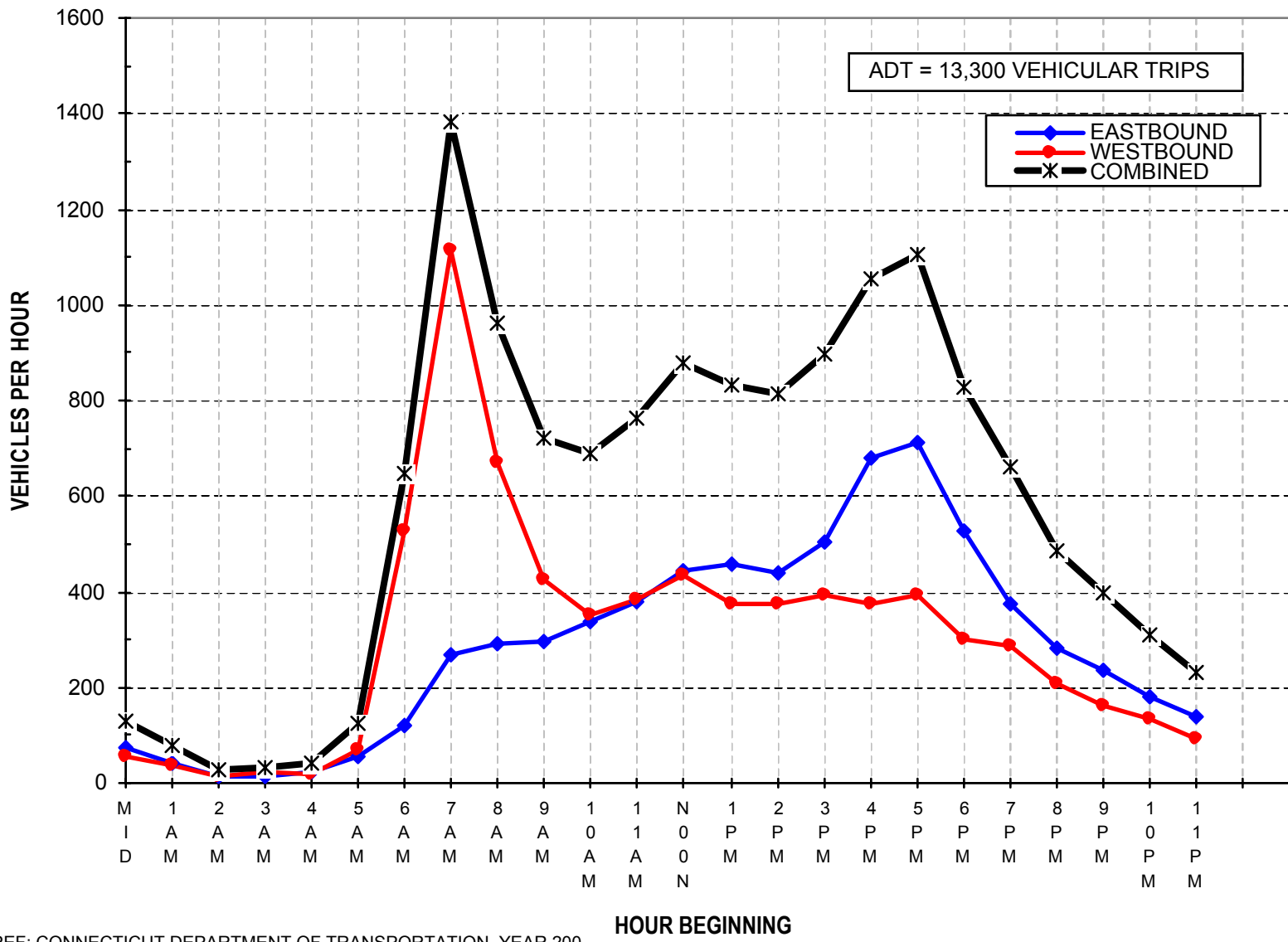
**Route 130 (Fairfield Avenue)
and side streets**

HOURLY TRAFFIC VOLUMES - ROUTE 130 (SW/O BEACHVIEW AVE) BRIDGEPORT, CT



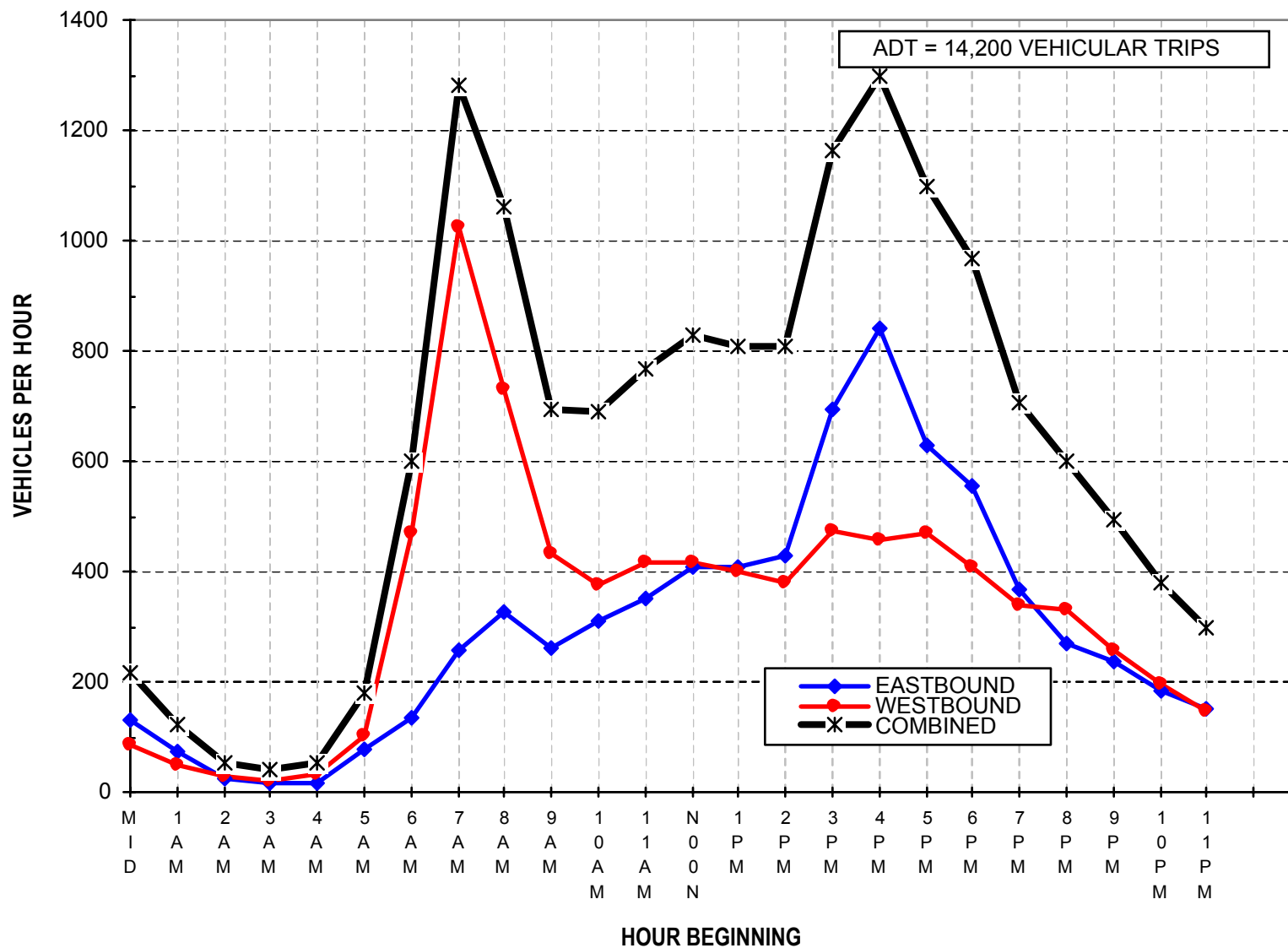
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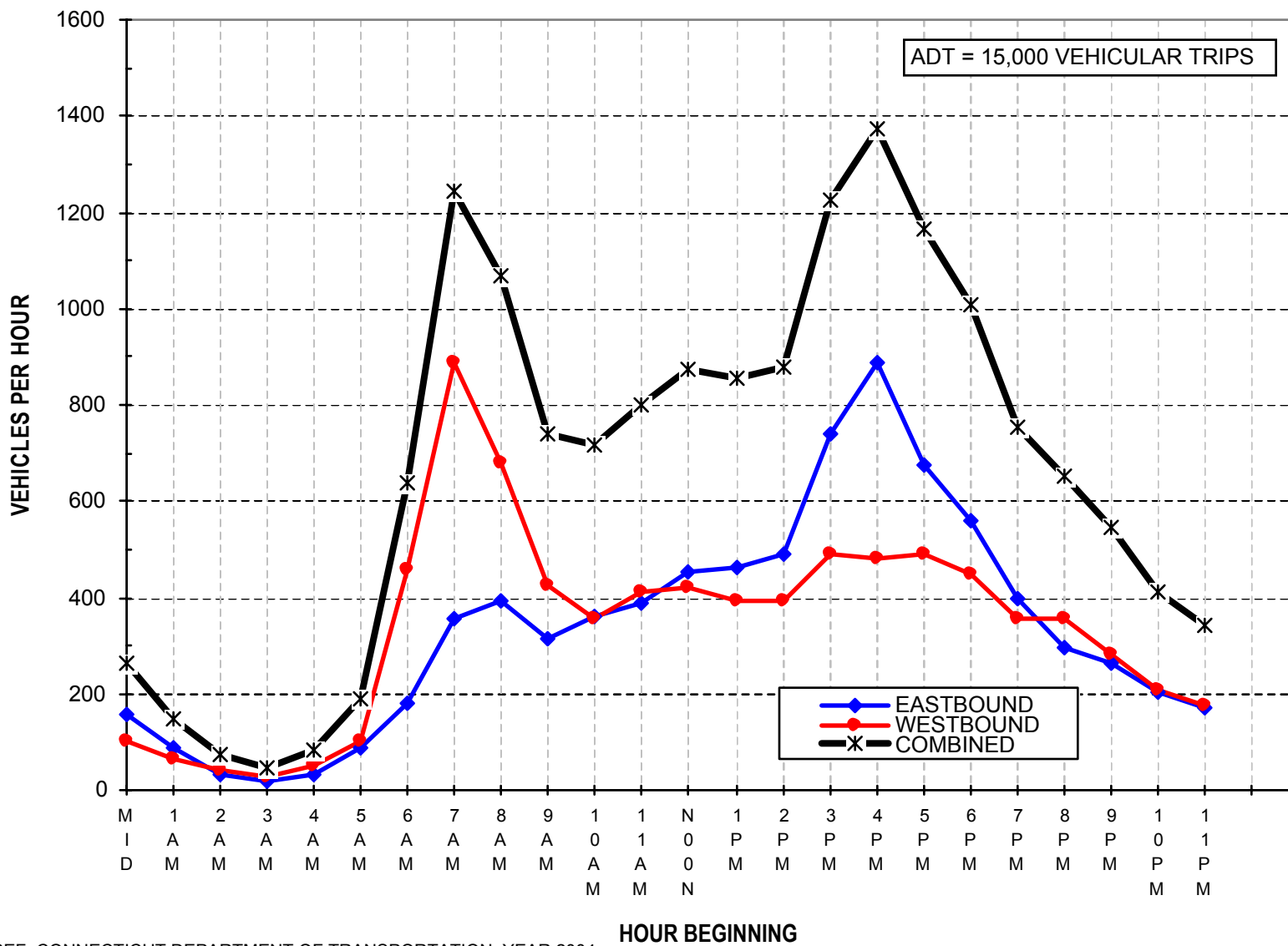
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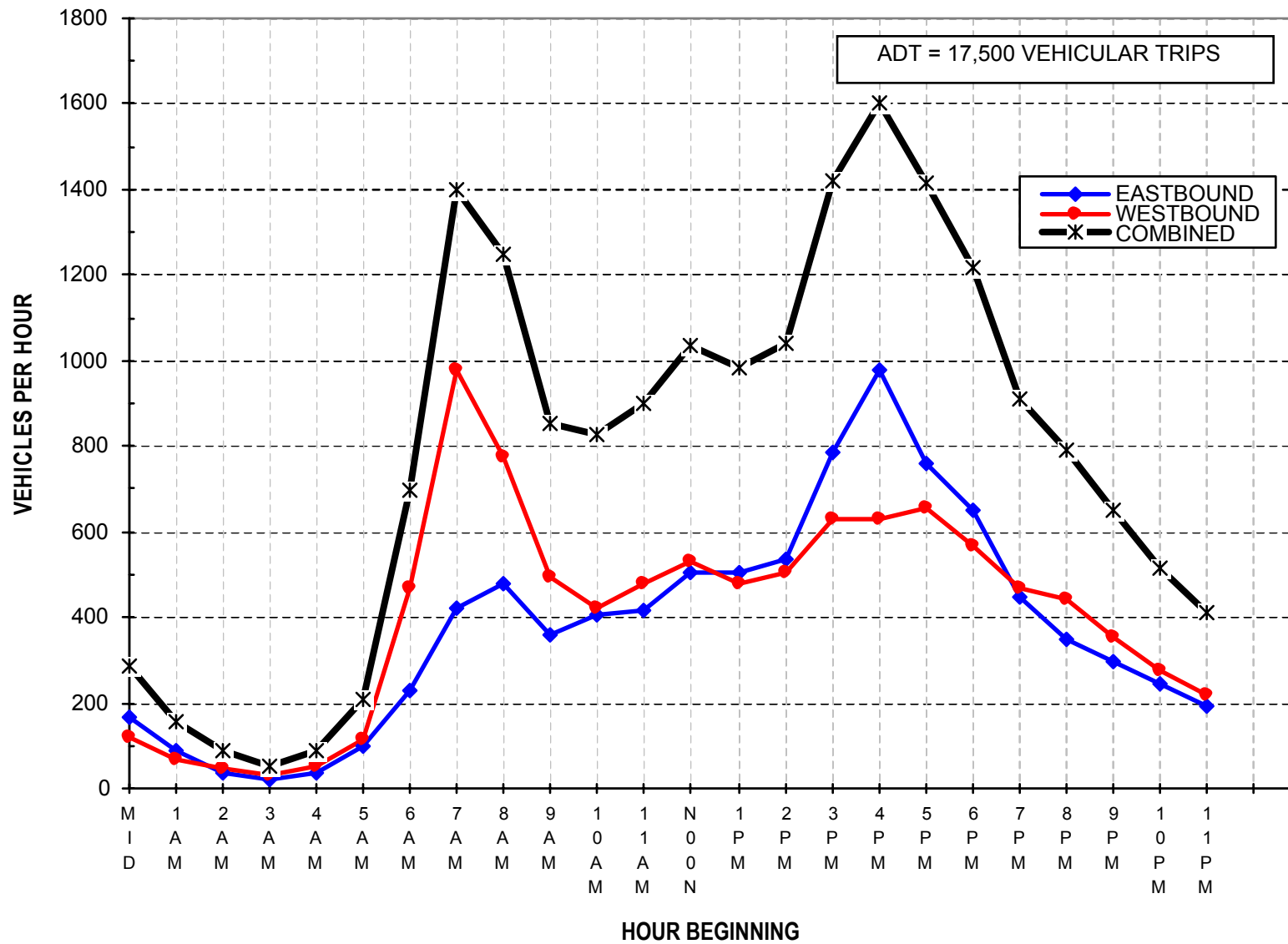
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HOURLY TRAFFIC VOLUMES - ROUTE 130 (NE/O ELLSWORTH STREET) BRIDGEPORT, CT



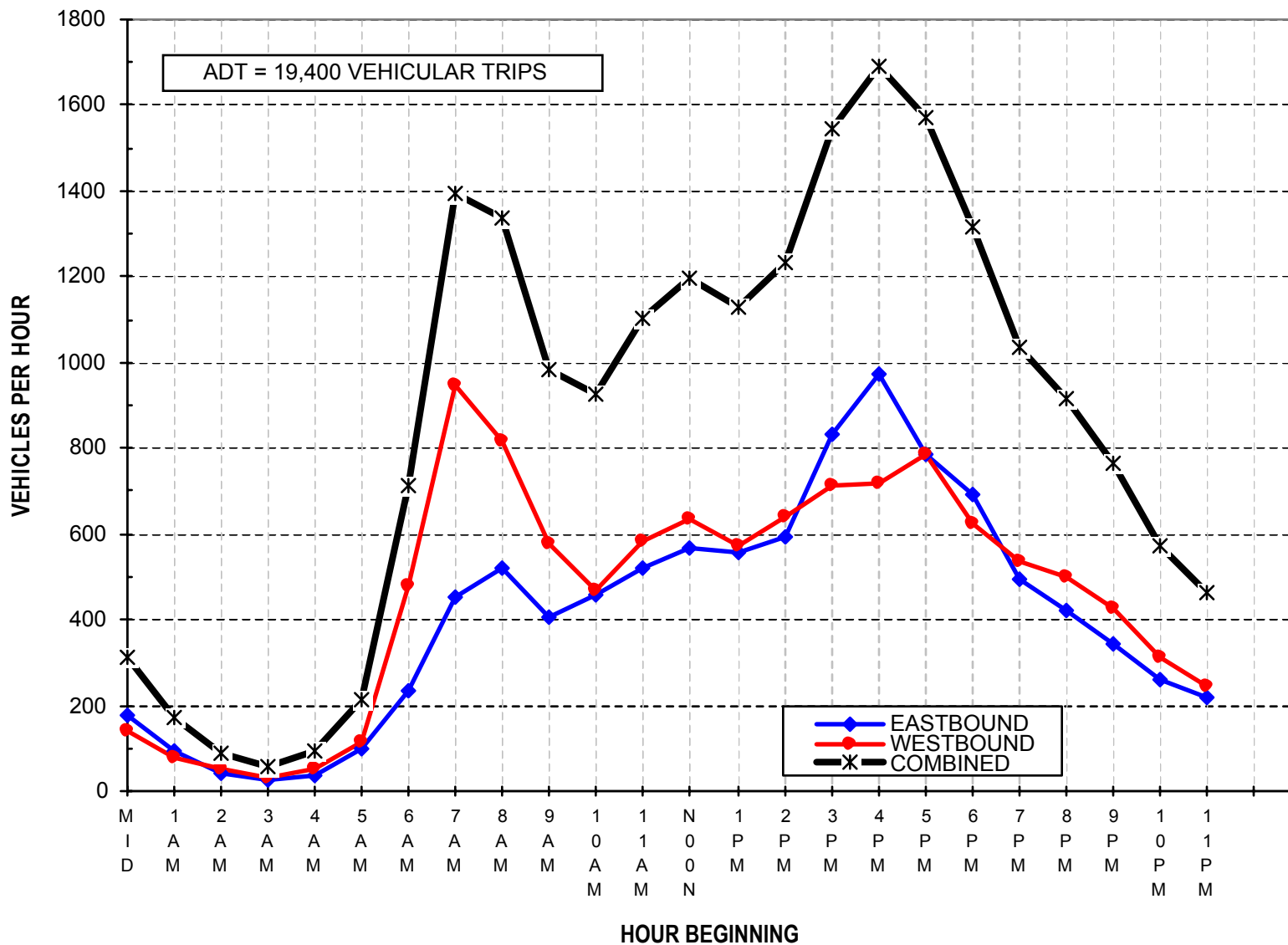
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HOURLY TRAFFIC VOLUMES - ROUTE 130 (SW/O WHITTIER STREET) BRIDGEPORT, CT



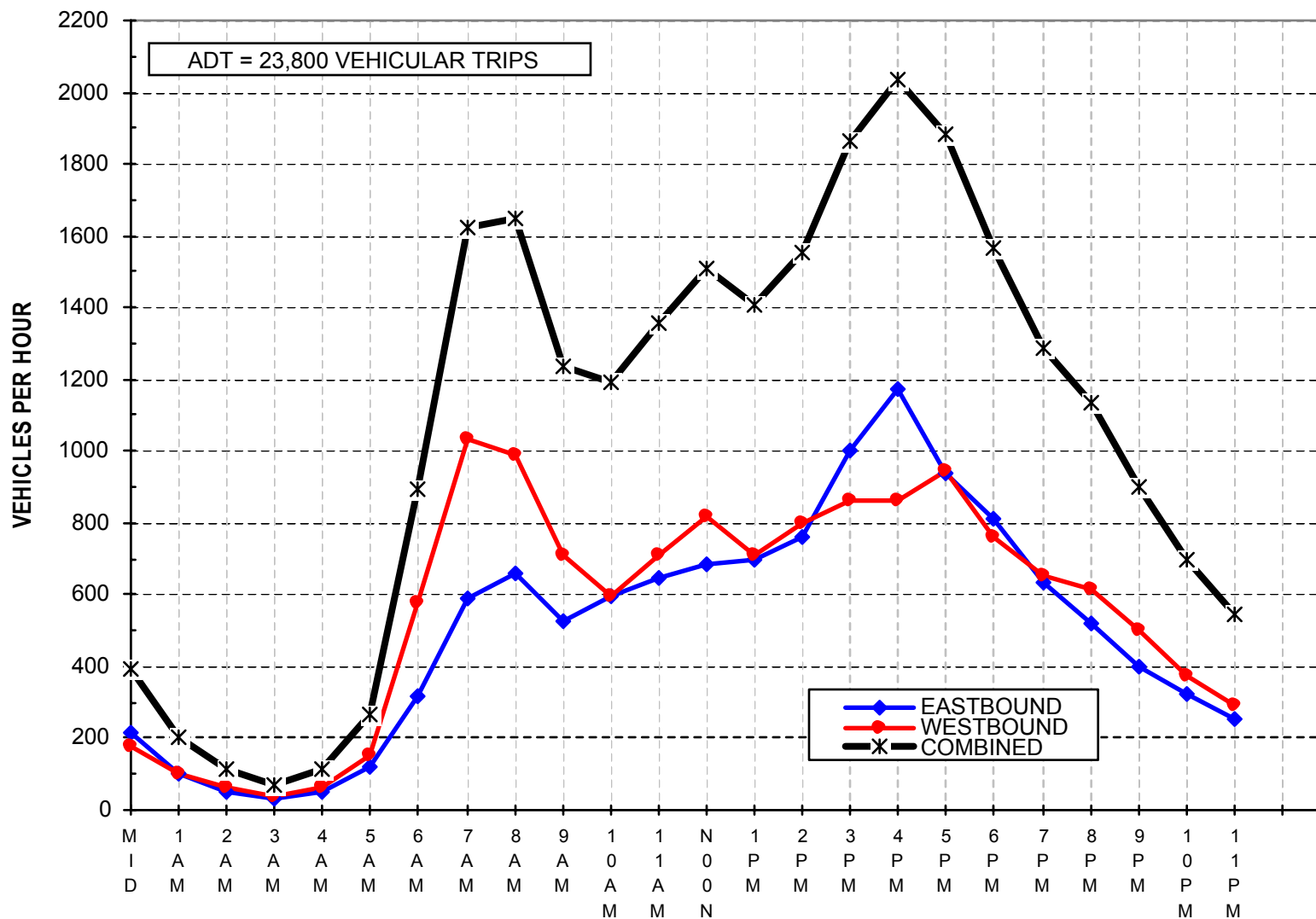
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HOURLY TRAFFIC VOLUMES - ROUTE 130 (NE/O BELMONT AVENUE) BRIDGEPORT, CT



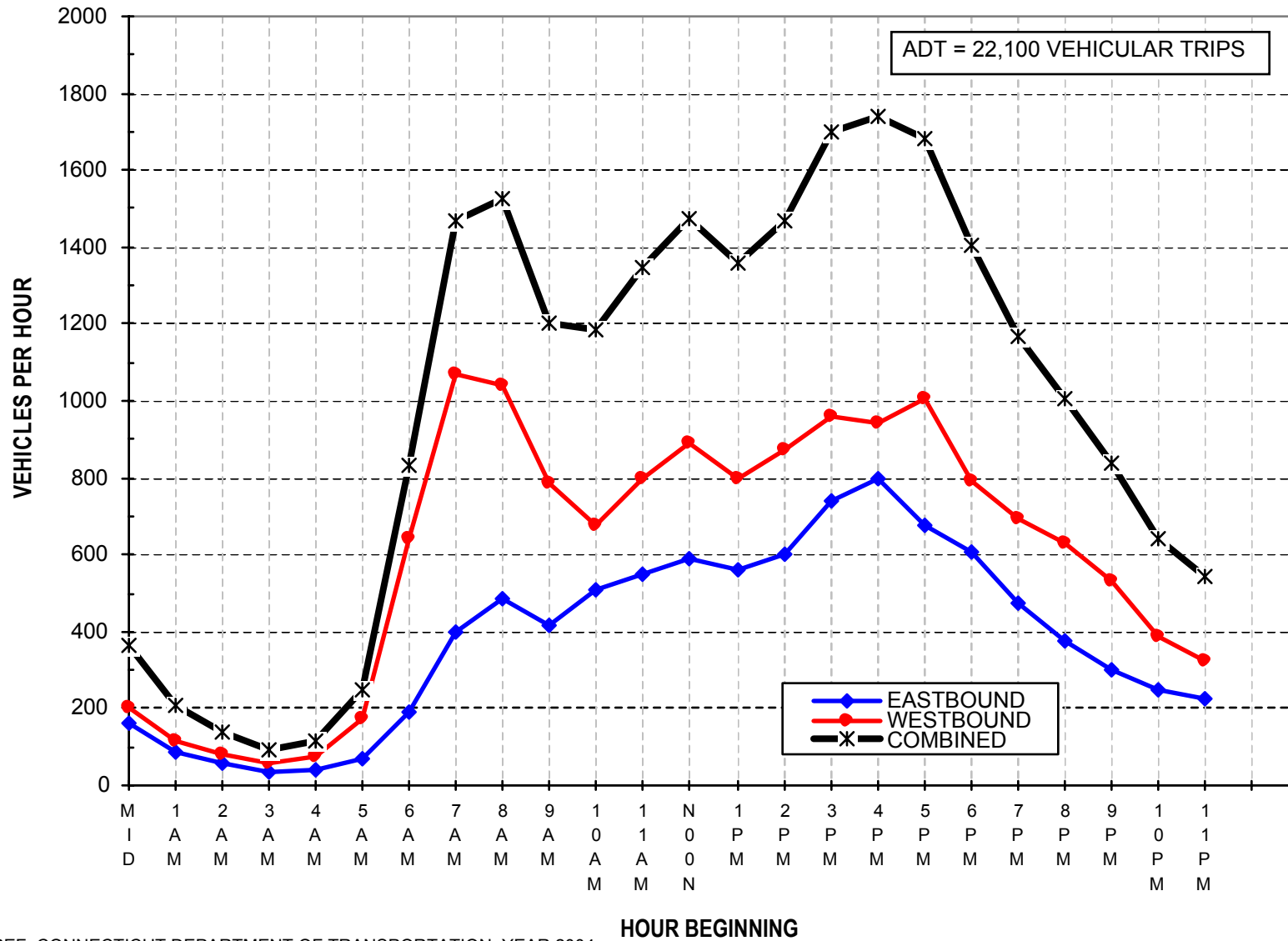
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HOURLY TRAFFIC VOLUMES - ROUTE 130 (NE/O ALBION STREET) BRIDGEPORT, CT



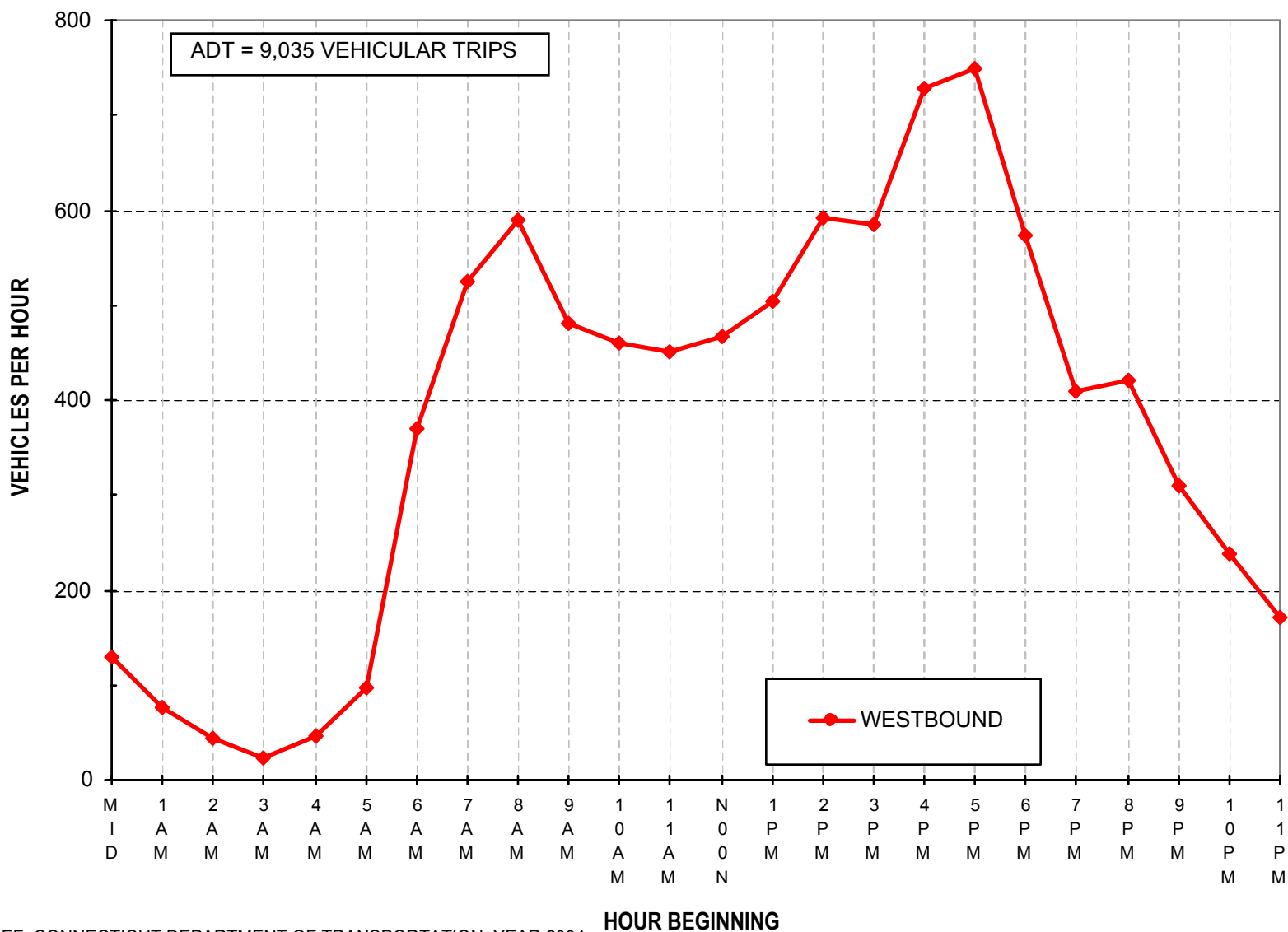
REF: CONNECTICUT DEPARTMENT OF TRANSPORTATION, YEAR 2004 **HOUR BEGINNING**

HOURLY TRAFFIC VOLUMES - ROUTE 130 (SW/O I-95 SB OFF RAMP-EXIT 25) BRIDGEPORT, CT



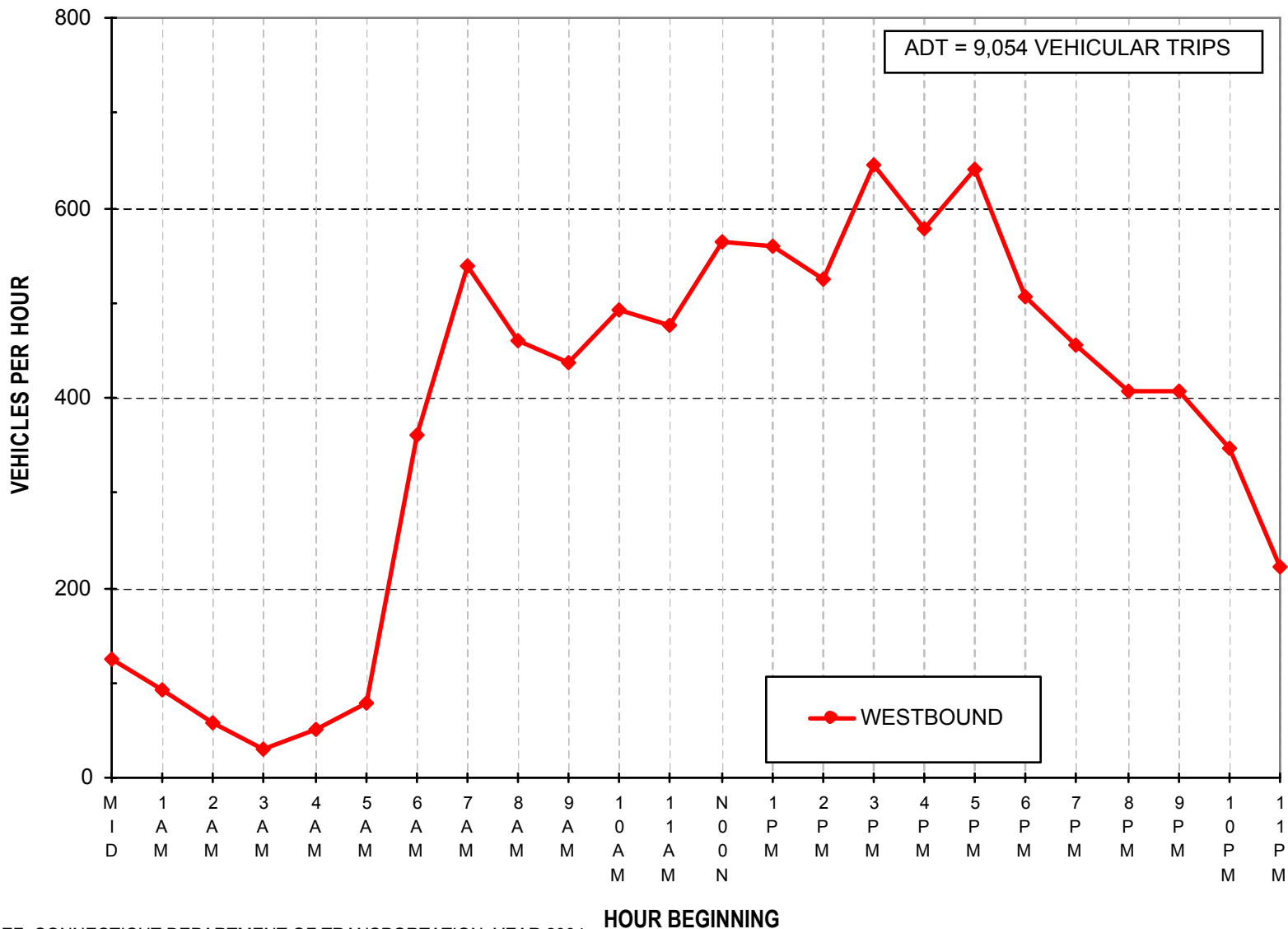
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BRIDGEPORT, CT**



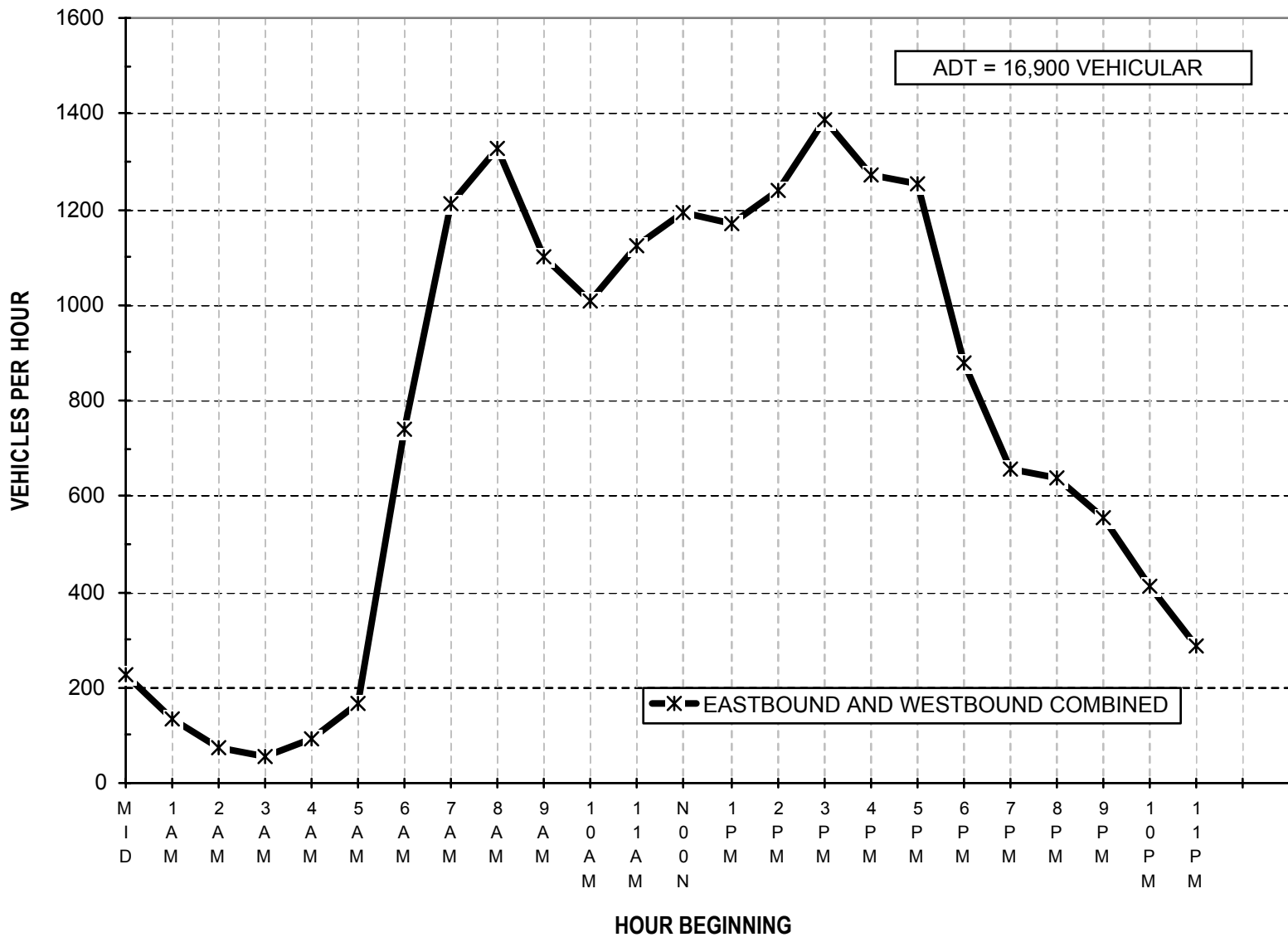
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BRIDGEPORT, CT**



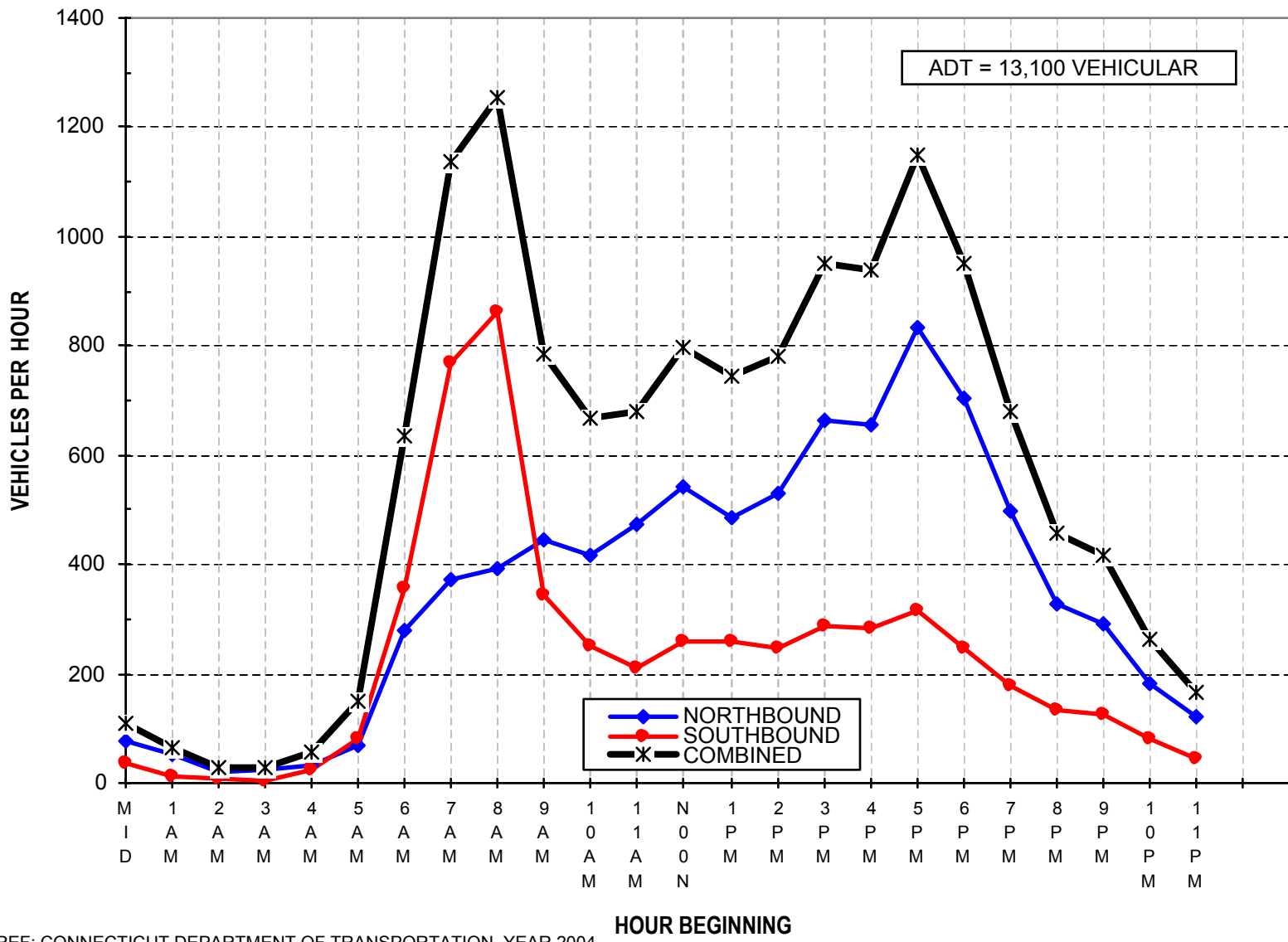
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**HOURLY TRAFFIC VOLUMES - ROUTE 130 (SW/O COMMERCE DRIVE)
BRIDGEPORT, CT**



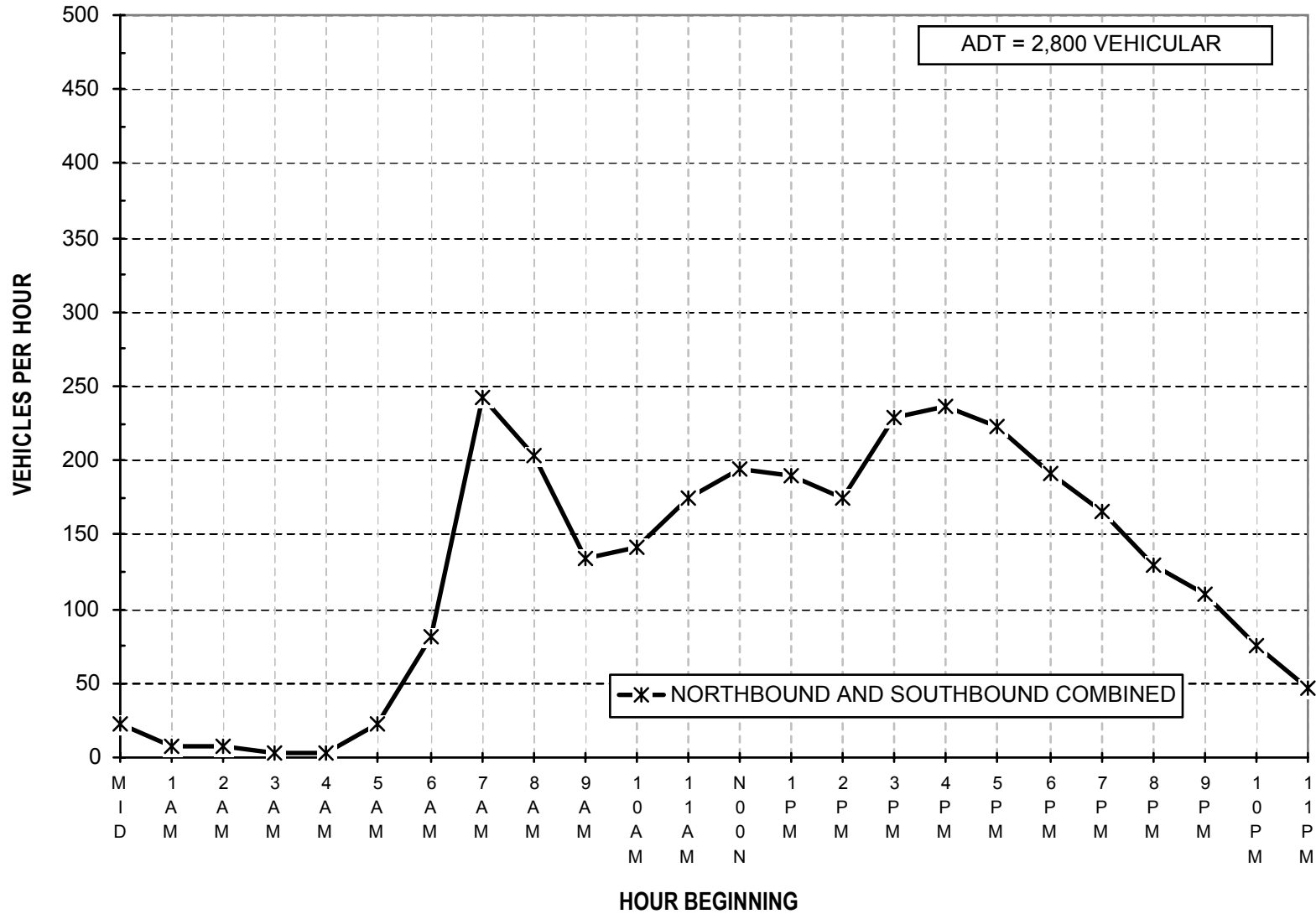
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HOURLY TRAFFIC VOLUMES -ROUTE 130 (S/O SR 700-FAIRFIELD AVE) BRIDGEPORT, CT



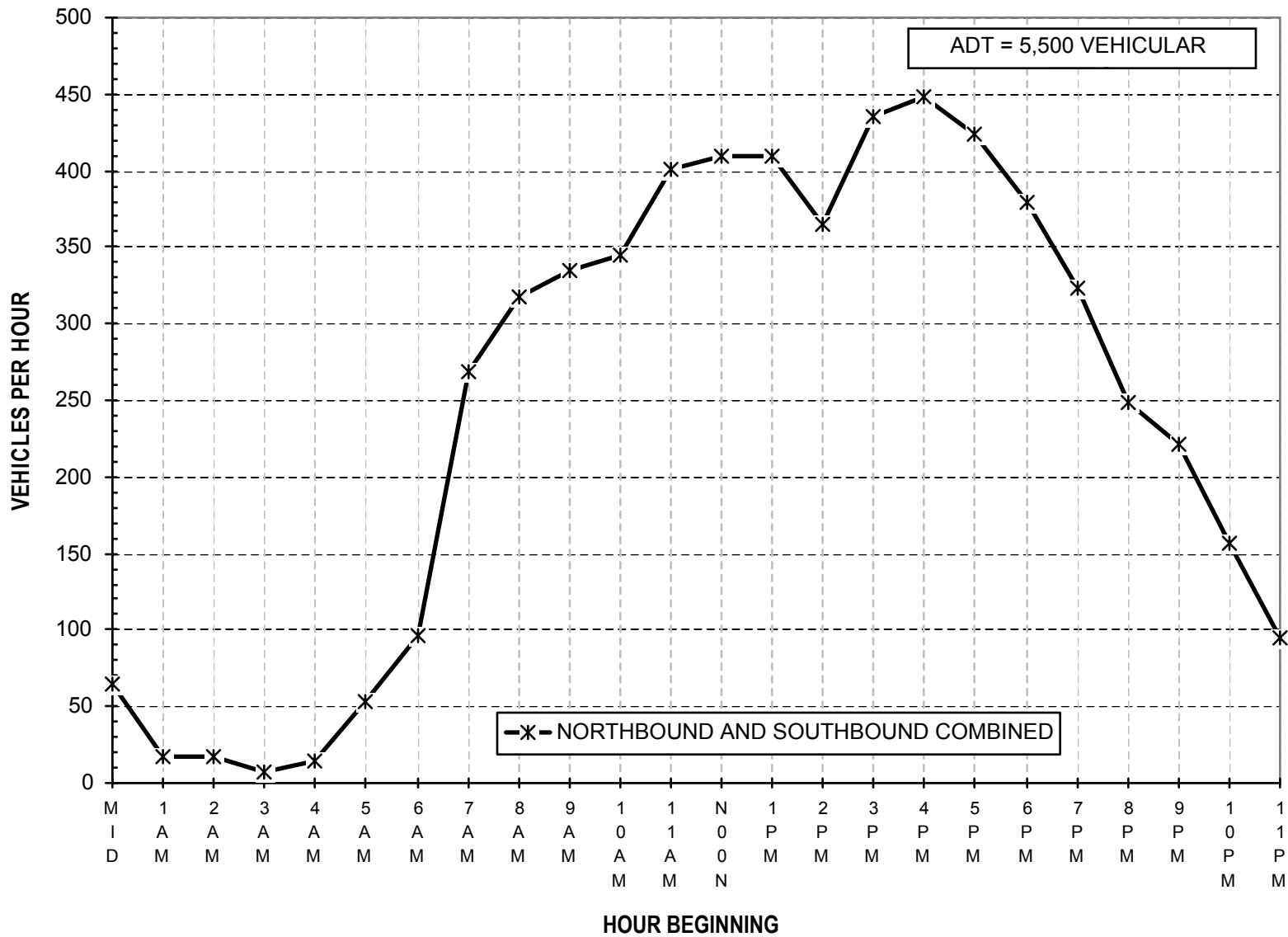
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**HOURLY TRAFFIC VOLUMES - BREWSTER ST. (NW/O BARTRAM AVE.)
BRIDGEPORT, CT**



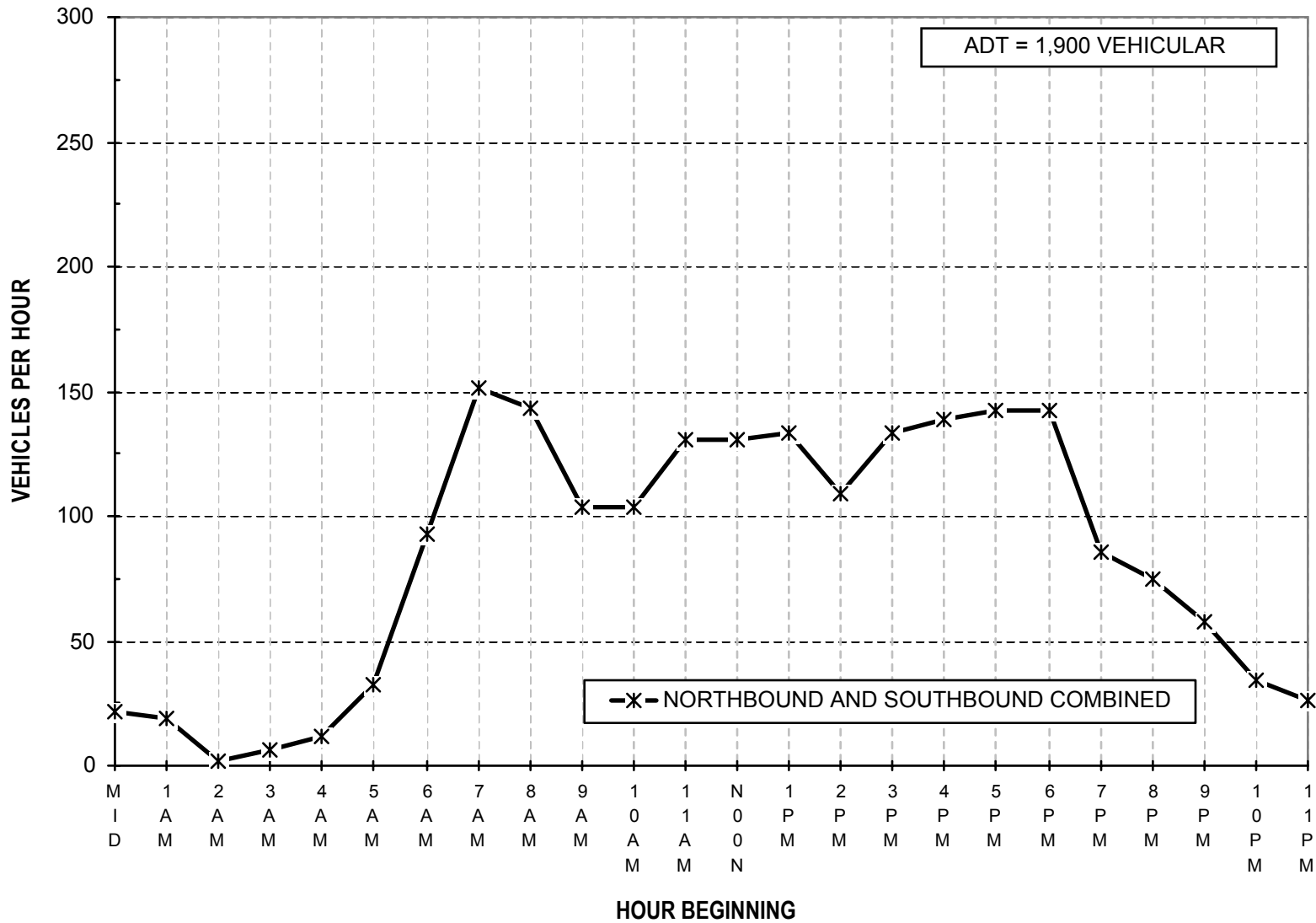
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**HOURLY TRAFFIC VOLUMES - BREWSTER ST. - (NW/O RTE. 130 (FAIRFIELD AVE.))
BRIDGEPORT, CT**



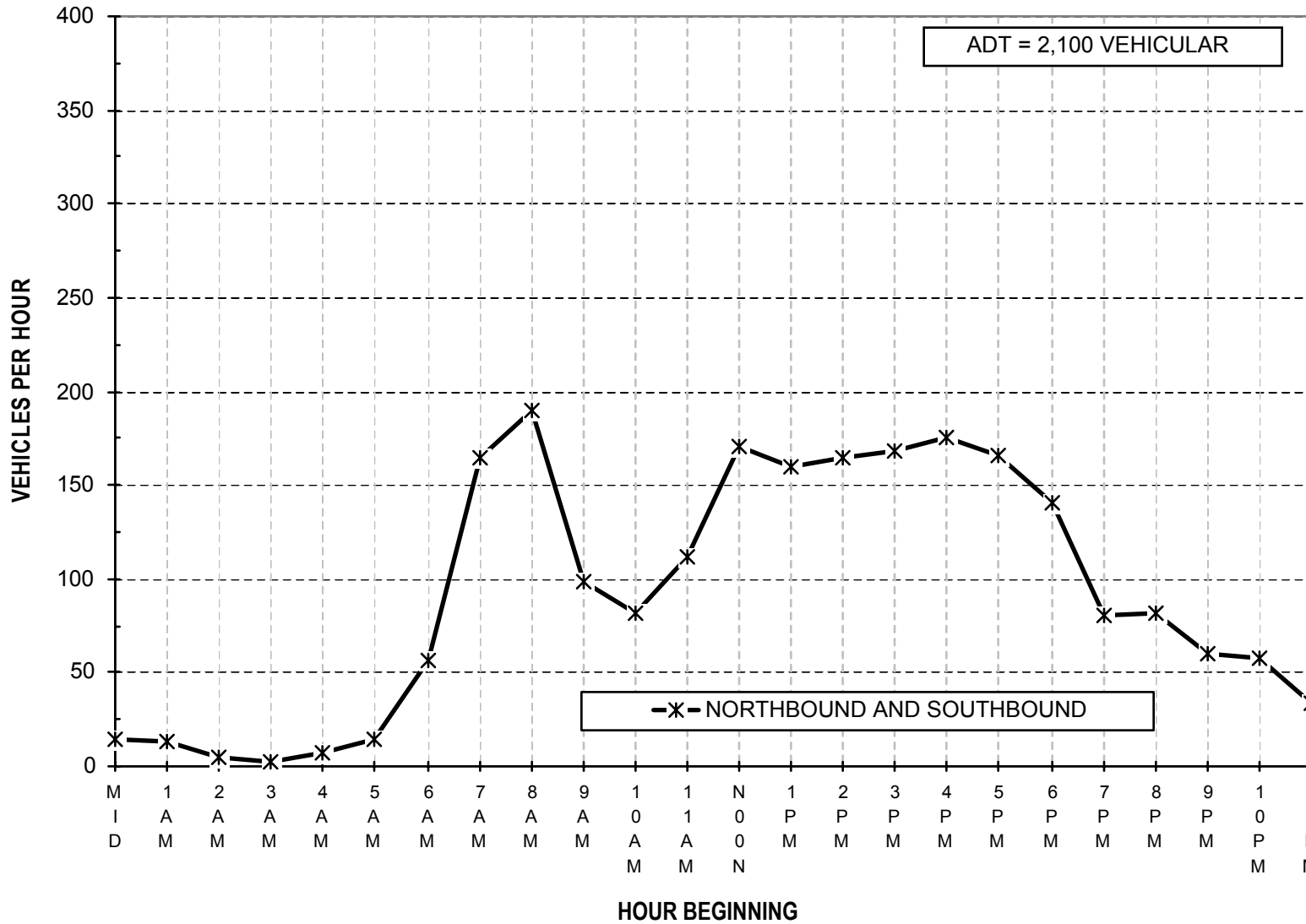
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BRIDGEPORT, CT**



REF: CONNECTICUT DEPARTMENT OF TRANSPORTATION, YEAR 200

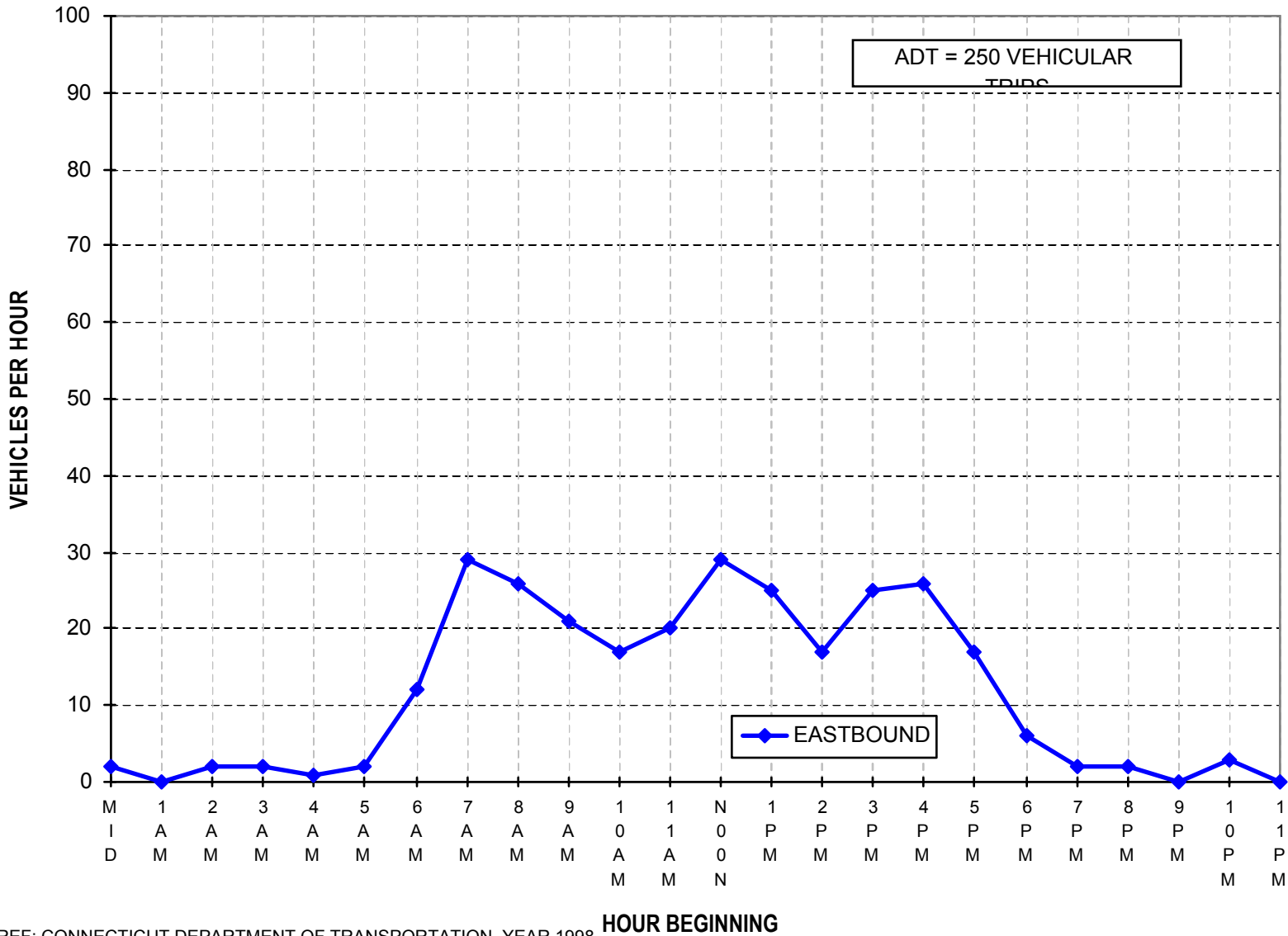
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BRIDGEPORT, CT**



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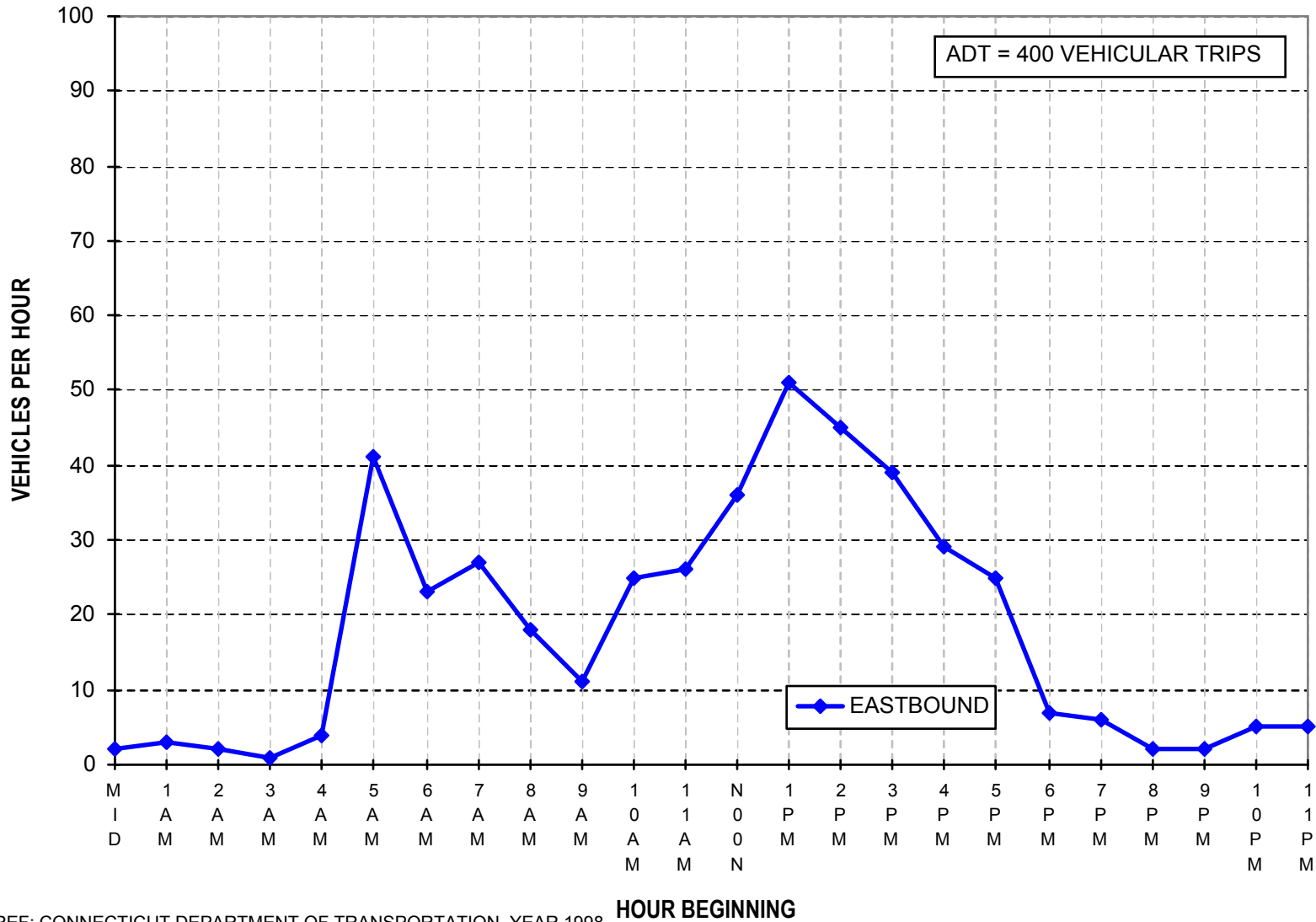
**Railroad Avenue (Eastbound)
and side streets**

**HOURLY TRAFFIC VOLUMES - RAILROAD AVE. (EB) - (E/O RTE. 130 (FAIRFIELD AVE.))
BRIDGEPORT, CT**



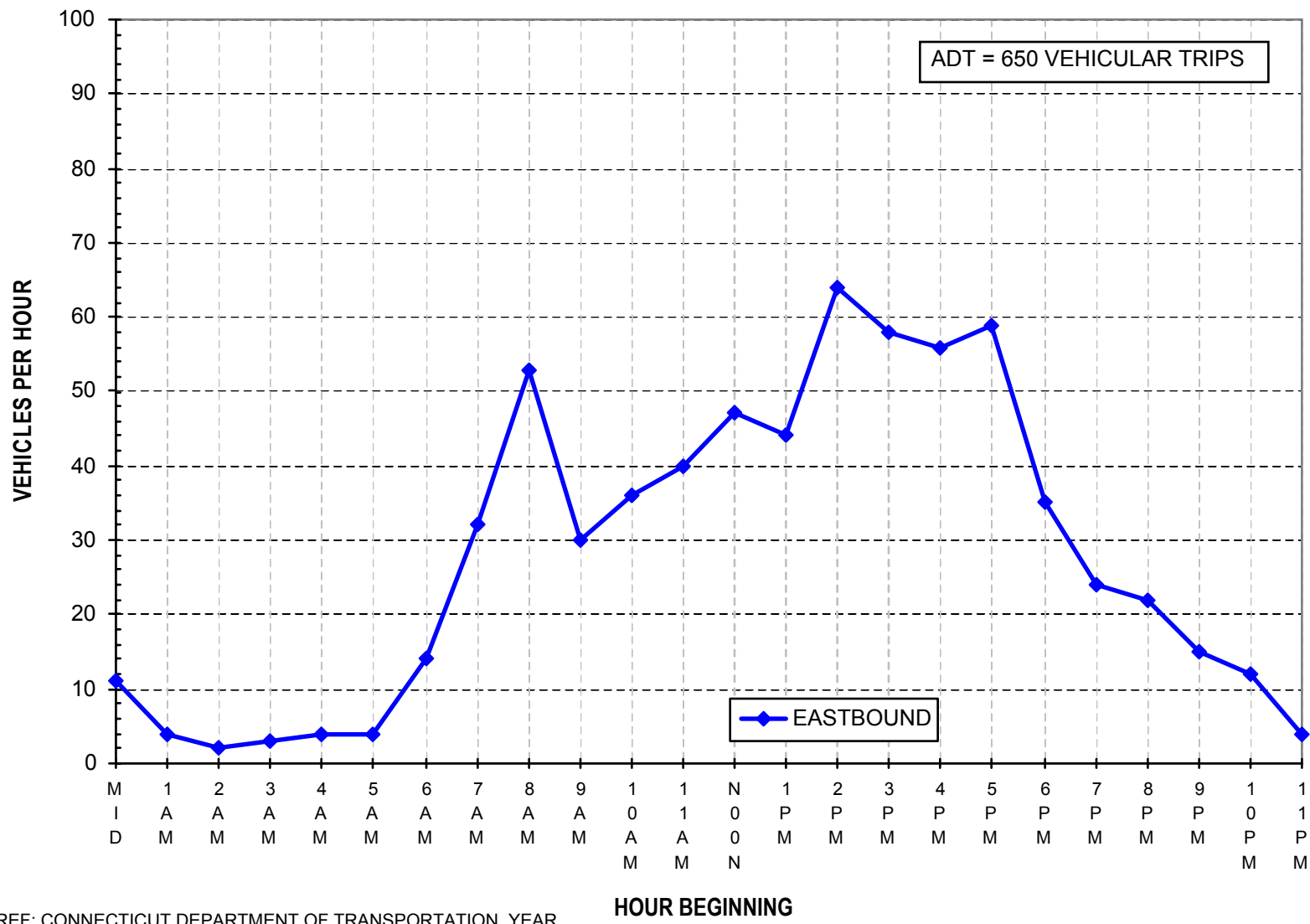
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**HOURLY TRAFFIC VOLUMES - RAILROAD AVE. (EB) - (E/O HOWARD AVE.)
BRIDGEPORT, CT**



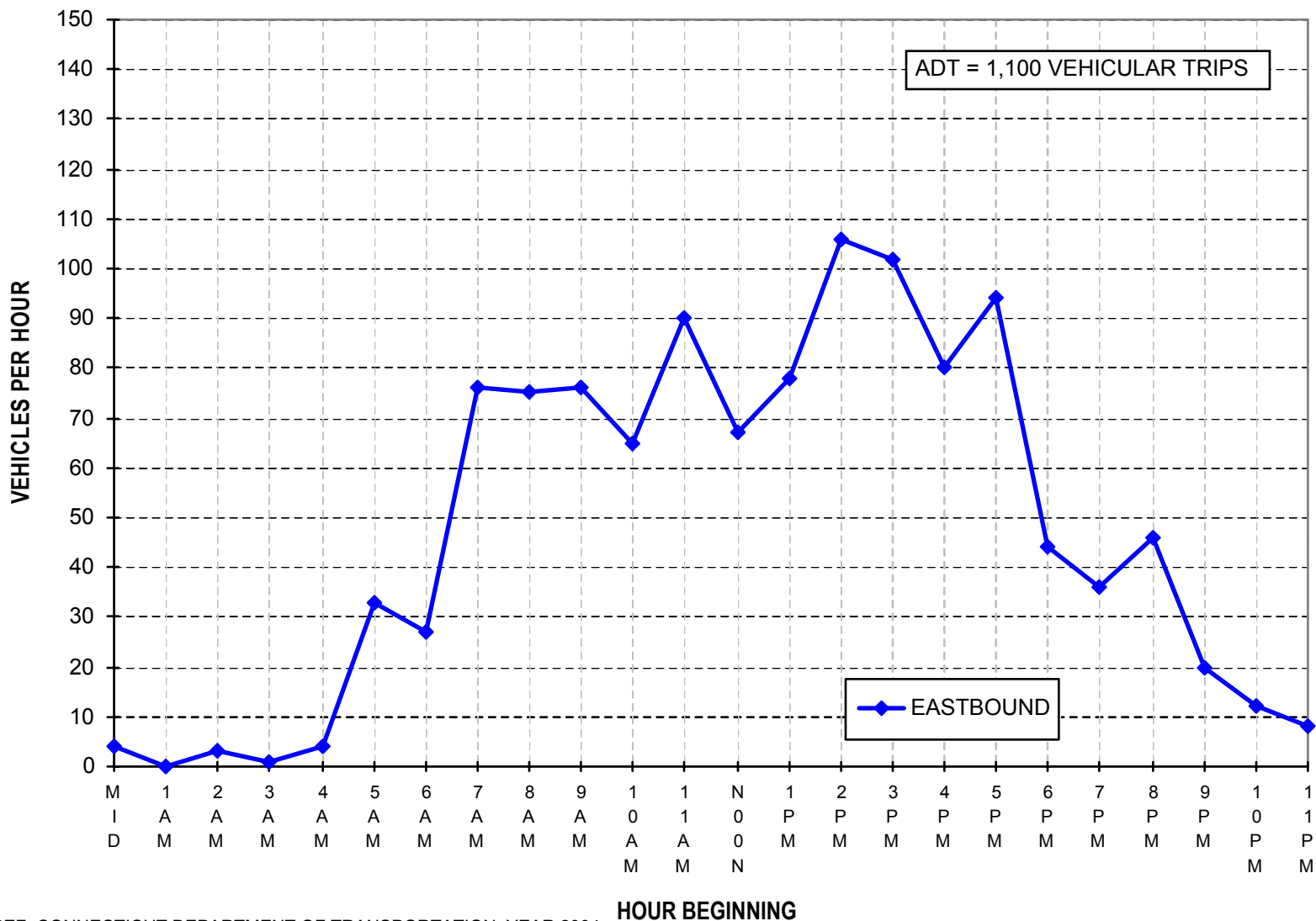
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HOURLY TRAFFIC VOLUMES - RAILROAD AVENUE (EB) (E/O PARK AVENUE) BRIDGEPORT, CT



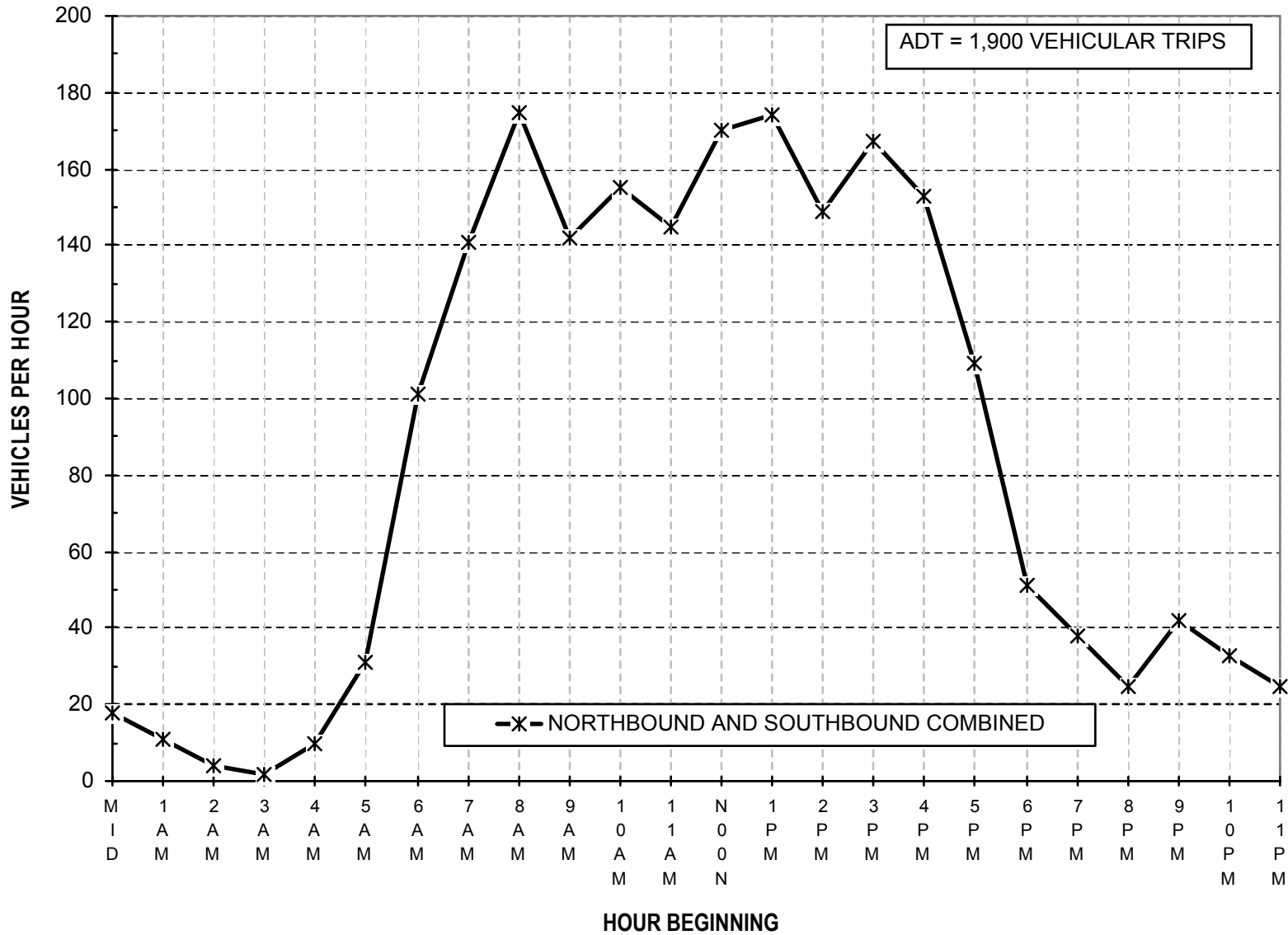
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**HOURLY TRAFFIC VOLUMES - RAILROAD AVE (EB) (E/O LAFAYETTE ST.)
BRIDGEPORT, CT**



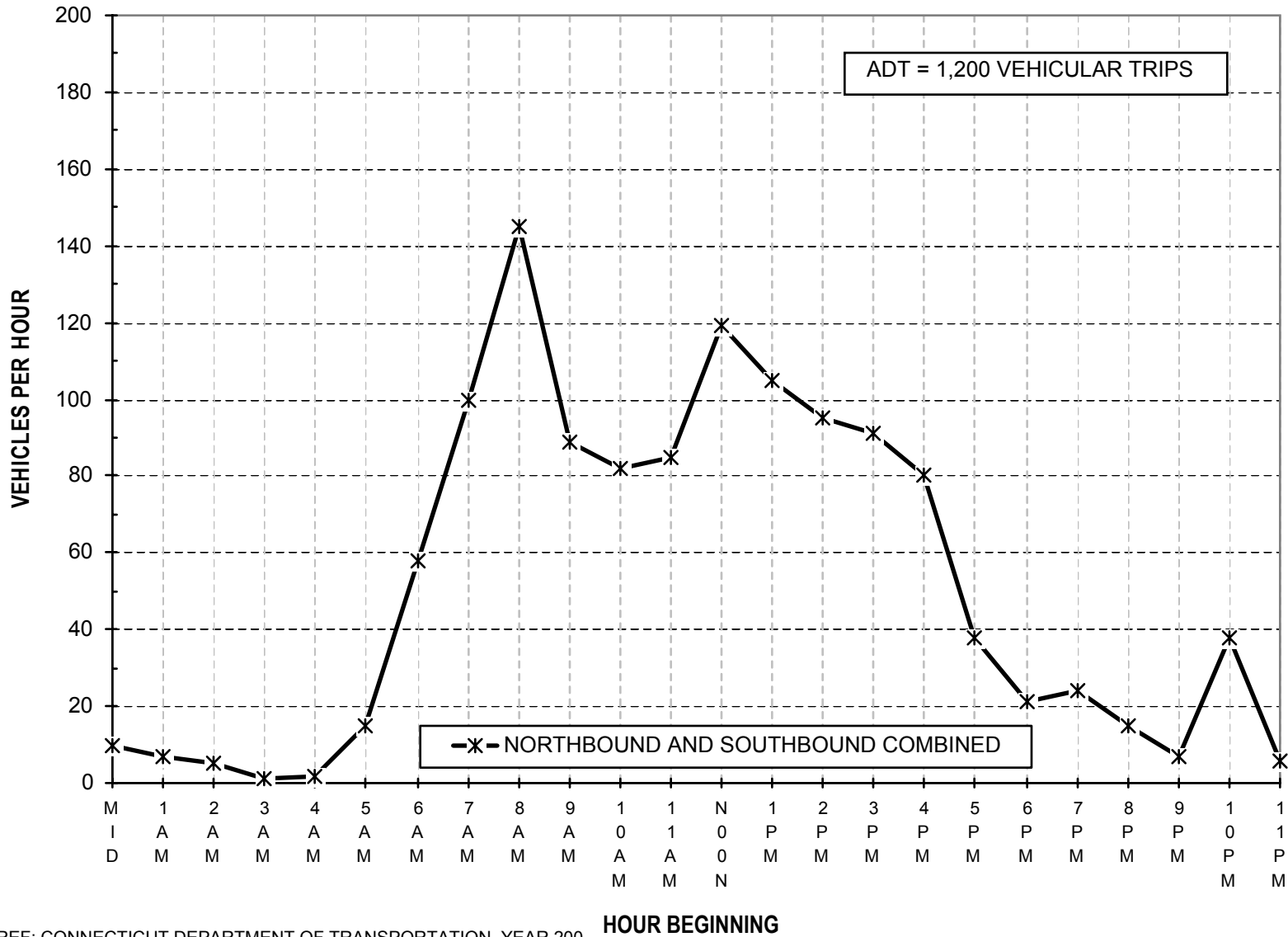
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**HOURLY TRAFFIC VOLUMES - BOSTWICK AVENUE (S/O WORDIN AVENUE)
BRIDGEPORT, CT**



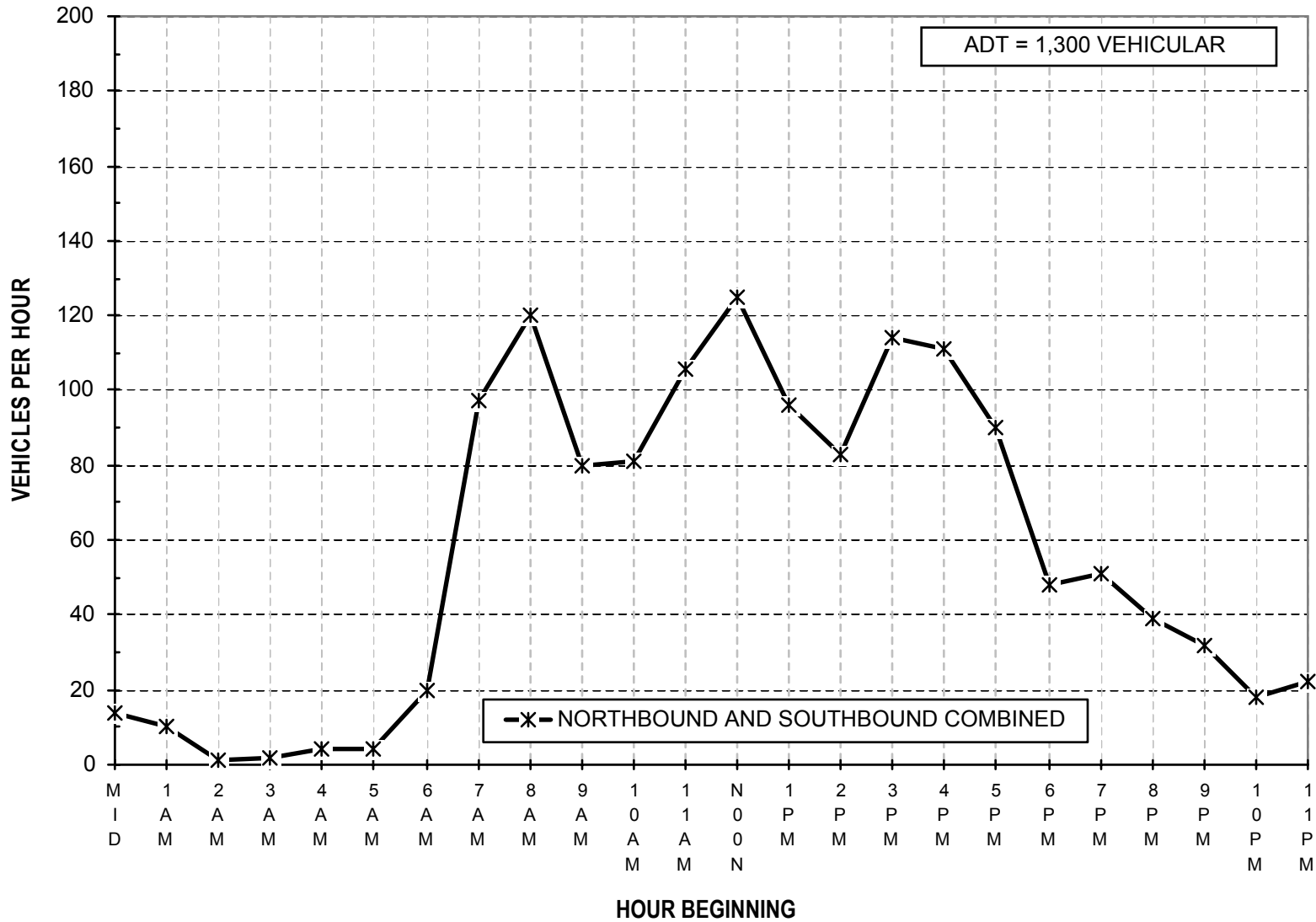
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**HOURLY TRAFFIC VOLUMES - HANCOCK AVENUE (N/O WORDIN AVENUE #2)
BRIDGEPORT, CT**



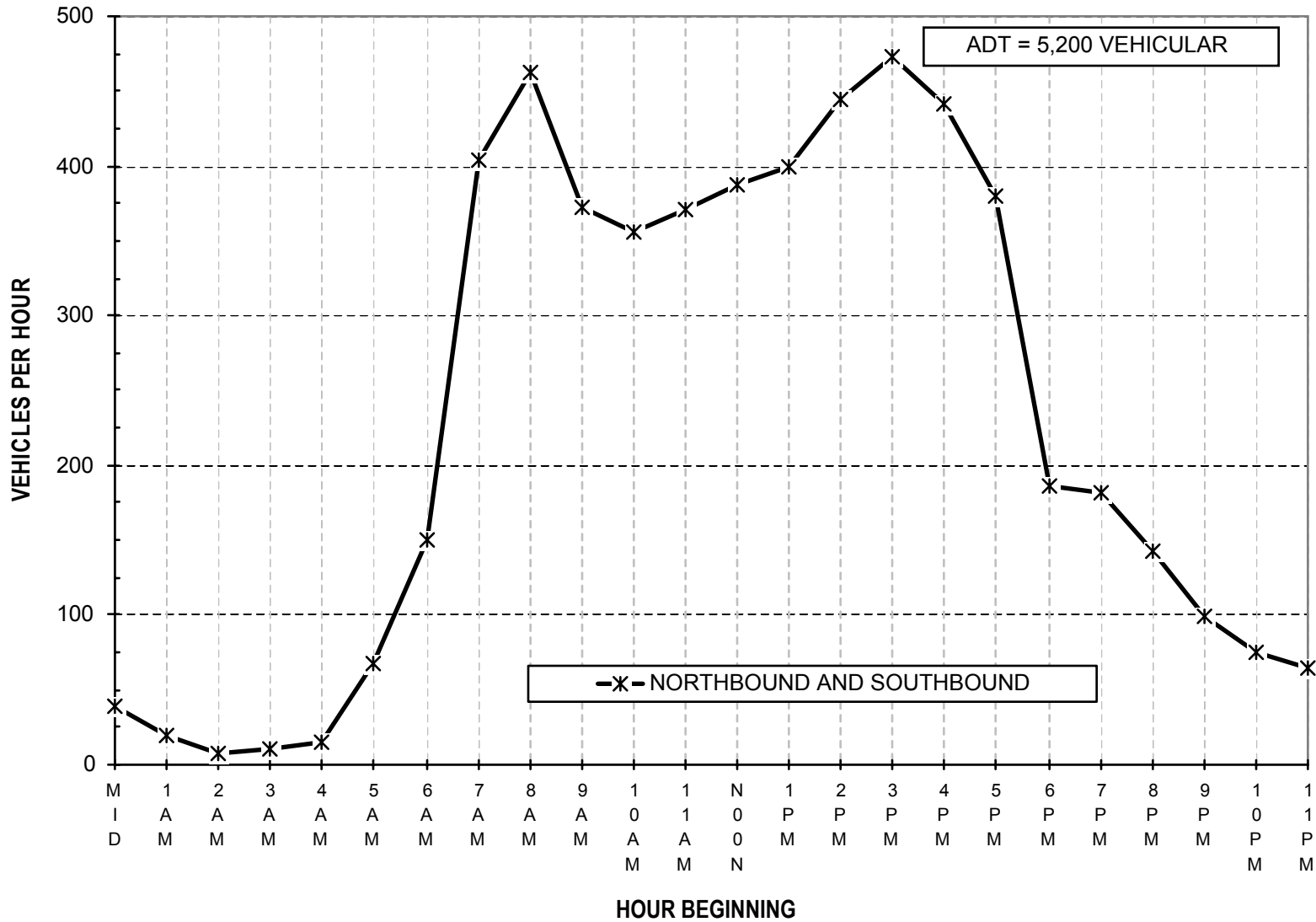
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**HOURLY TRAFFIC VOLUMES - LAFAYETTE ST. (S/O ATLANTIC ST.)
BRIDGEPORT, CT**



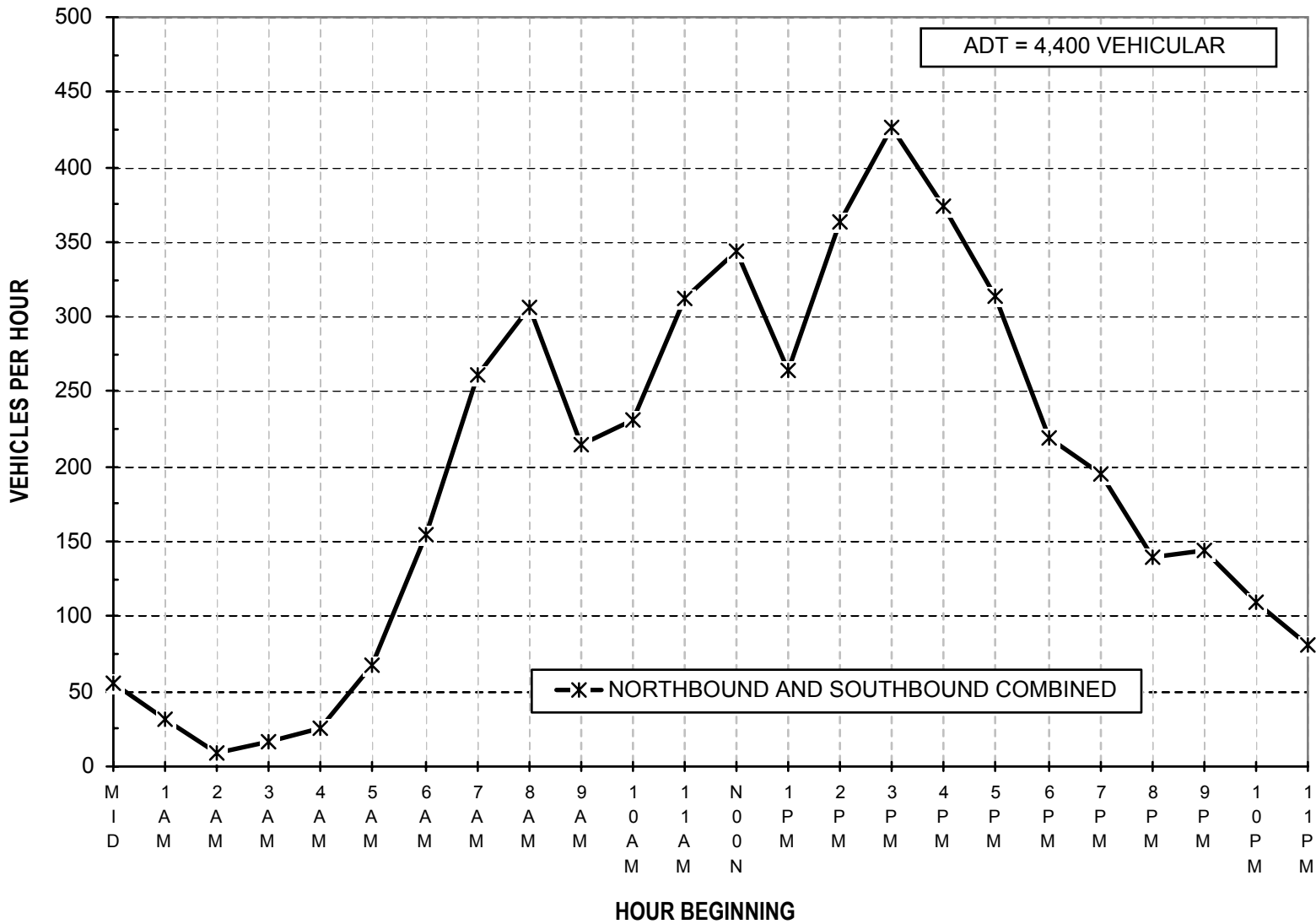
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BRIDGEPORT, CT**



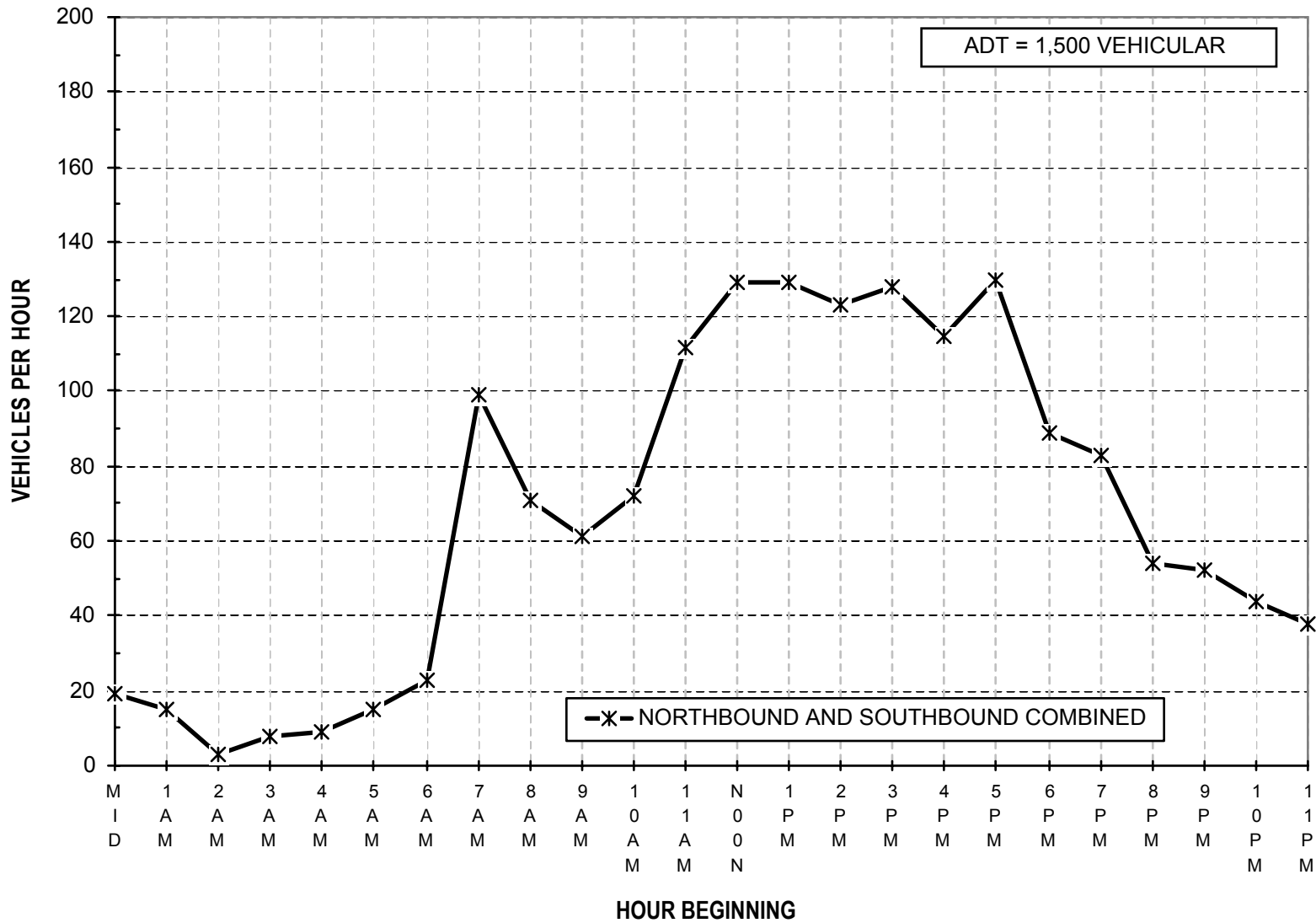
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BRIDGEPORT, CT**



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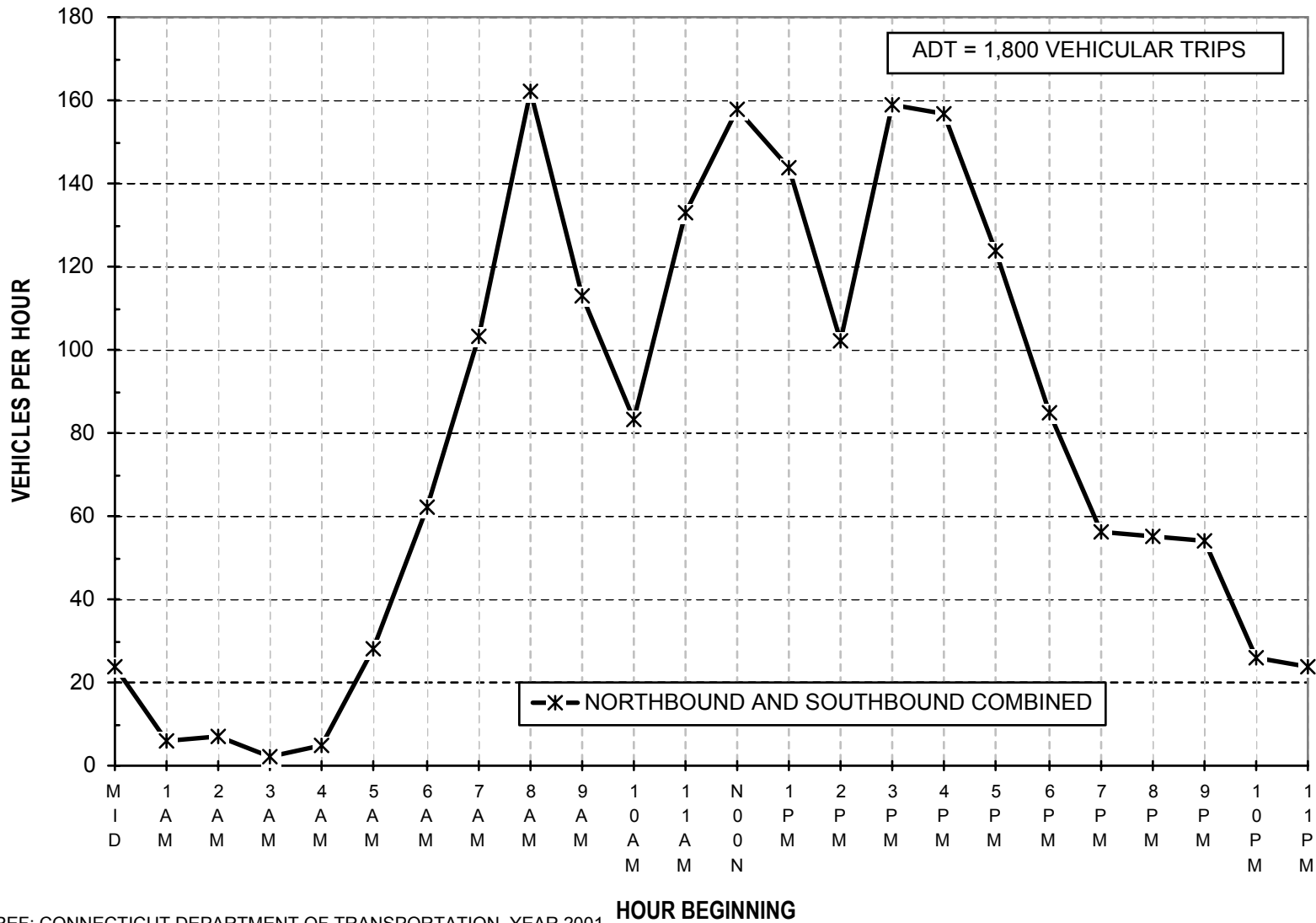
**HOURLY TRAFFIC VOLUMES - IRANISTAN AVE. (SE/O ATLANTIC ST.)
BRIDGEPORT, CT**



REF: CONNECTICUT DEPARTMENT OF TRANSPORTATION, YEAR 200

Broad Street

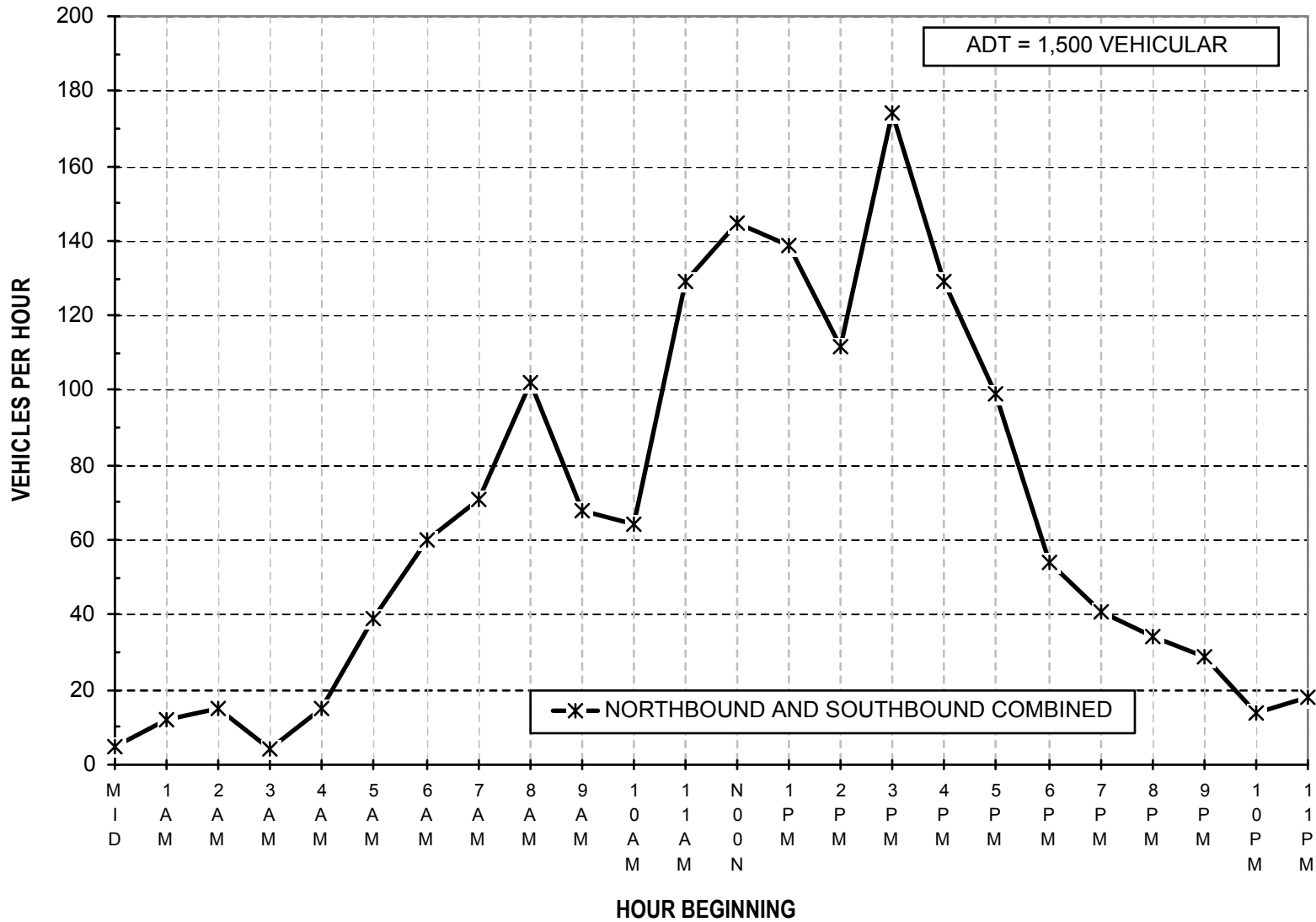
**HOURLY TRAFFIC VOLUMES - BROAD STREET (S/O RAILROAD AVE. (EB))
BRIDGEPORT, CT**



REF: CONNECTICUT DEPARTMENT OF TRANSPORTATION, YEAR 2001

Main Street

**HOURLY TRAFFIC VOLUMES - MAIN ST. (S/O WHITING ST.)
BRIDGEPORT, CT**



REF: CONNECTICUT DEPARTMENT OF TRANSPORTATION, YEAR 200

APPENDIX IV

SIGNALIZED INTERSECTIONS
AERIAL PHOTOGRAPHS



**SIGNALIZED INTERSECTION #15-341
RTE. 130 (FAIRFIELD AVE.) AT COURTLAND ST.
BRIDGEPORT, CONNECTICUT**

SCHMATIC, NOT TO SCALE

FIGURE BI-1



**SIGNALIZED INTERSECTION #15-240
 RTE. 130 (FAIRFIELD AVE.) AT FOX ST./GILMAN ST.
 BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BI-2



**SIGNALIZED INTERSECTION #15-241
 RTE. 130 (FAIRFIELD AVE.) AT BREWSTER ST.
 BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BI-3



**SIGNALIZED INTERSECTION #15-242
RTE. 130 (FAIRFIELD AVE.) AT ELLSWORTH ST.
BRIDGEPORT, CONNECTICUT**

SCHMATIC, NOT TO SCALE

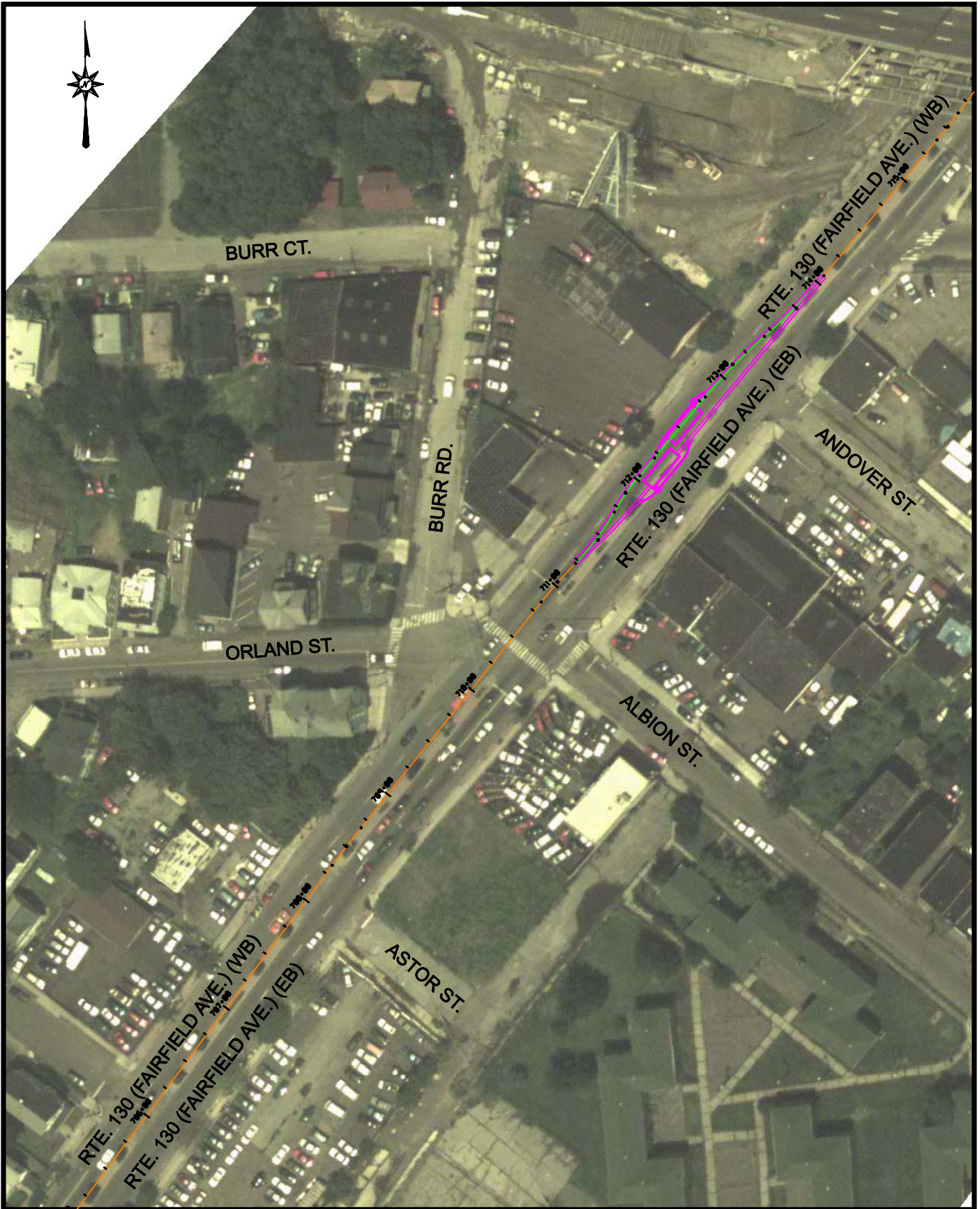
FIGURE BI-4



**SIGNALIZED INTERSECTION #15-349
RTE. 130 (FAIRFIELD AVE.) AT WORDIN AVE./WHITTIER ST.
BRIDGEPORT, CONNECTICUT**

SCHMATIC, NOT TO SCALE

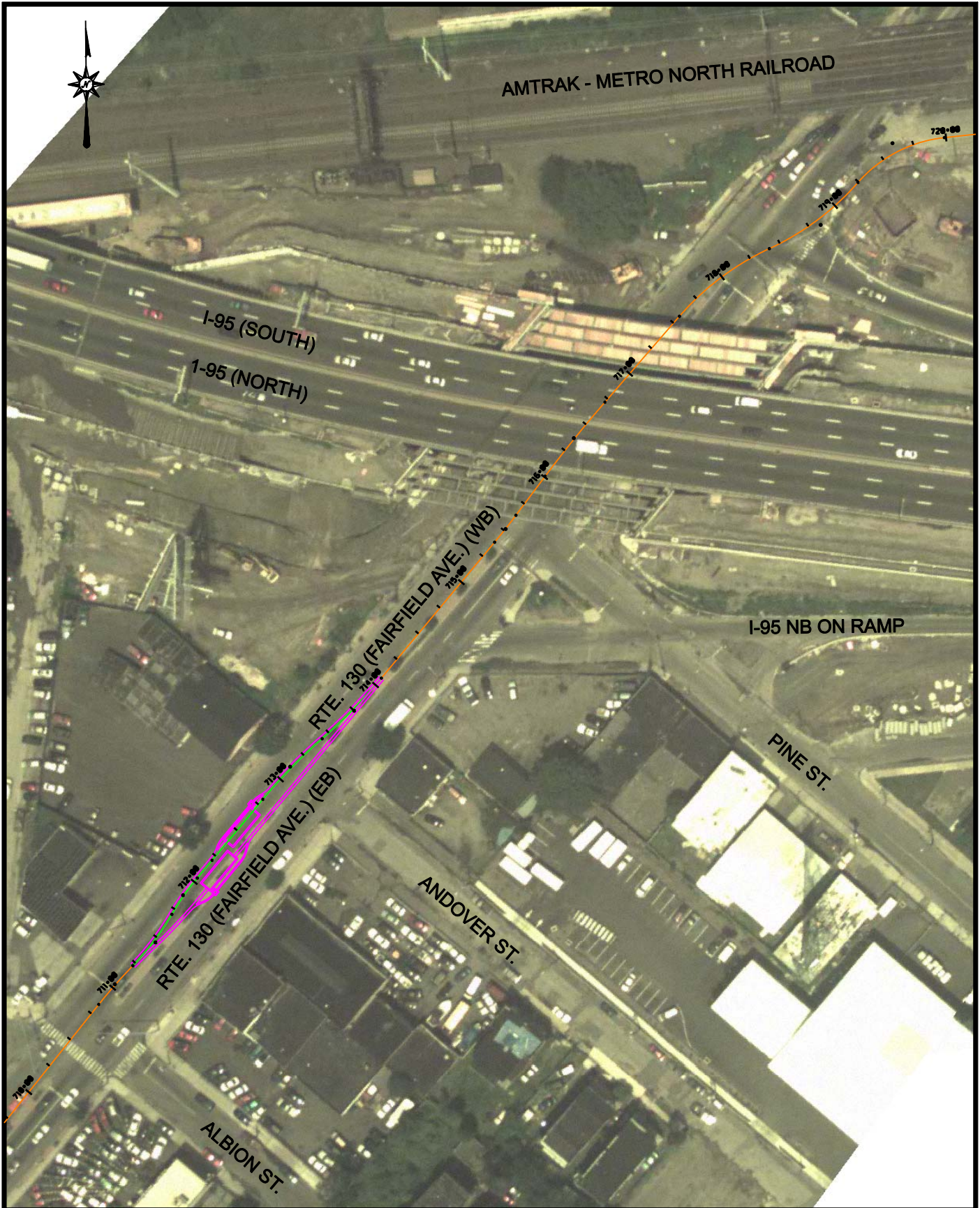
FIGURE BI-5



**SIGNALIZED INTERSECTION #15-244
 RTE. 130 (FAIRFIELD AVE.) AT ALBION ST./ORLAND ST.
 BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

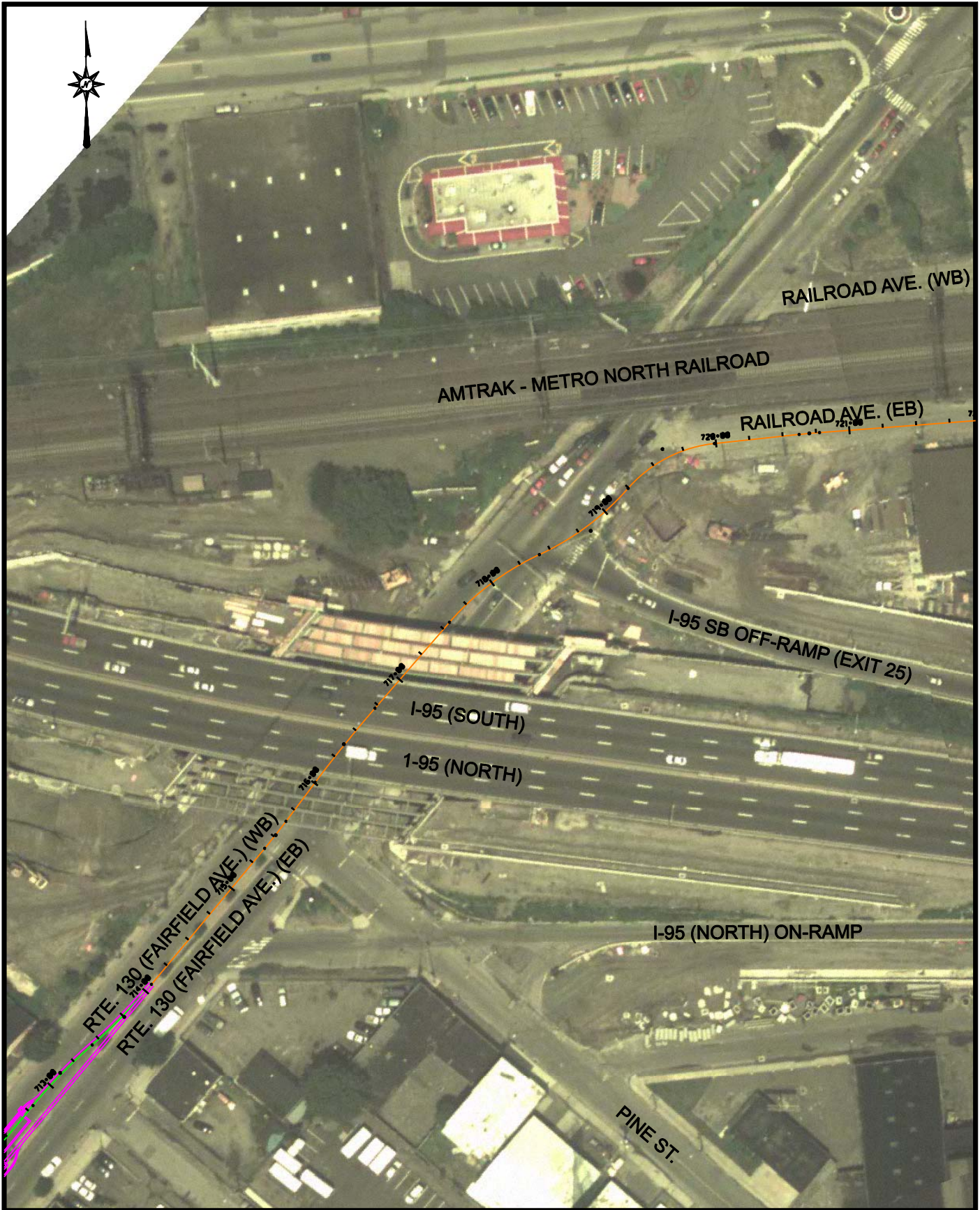
FIGURE BI-6



**SIGNALIZED INTERSECTION #15-245
 RTE. 130 (FAIRFIELD AVE.) AT I-95 NB ON RAMP
 BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BI-7



SIGNALIZED INTERSECTION #15-246
RTE. 130 (FAIRFIELD AVE.) AT I-95 SB OFF-RAMP (EXIT 25)
BRIDGEPORT, CONNECTICUT

SCHMATIC, NOT TO SCALE

FIGURE BI-8



**SIGNALIZED INTERSECTION OF
RAILROAD AVE. AT BOSTWICK AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BI-9



LESBIA ST.

HANCOCK AVE.

RAILROAD AVE. (WB)

AMTRAK - METRO NORTH RAILROAD

RAILROAD AVE. (EB)

HANCOCK AVE.

732+00

733+00

734+00

735+00

736+00

737+00

738+00

739+00



SIGNALIZED INTERSECTION OF RAILROAD AVE. AT HANCOCK AVE. BRIDGEPORT, CONNECTICUT

SCHEMATIC, NOT TO SCALE

FIGURE BI-10



**SIGNALIZED INTERSECTION OF
RAILROAD AVE. AT HOWARD AVE.
BRIDGEPORT, CONNECTICUT**

SCHMATIC, NOT TO SCALE

FIGURE BI-11



**SIGNALIZED INTERSECTION OF
RAILROAD AVE. AT WORDIN AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BI-12



**SIGNALIZED INTERSECTION OF
RAILROAD AVE. AT IRANISTAN AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BI-13



**SIGNALIZED INTERSECTION OF
RAILROAD AVE. AT SOUTH AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BI-14



**SIGNALIZED INTERSECTION OF
RAILROAD AVE. AT PARK AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

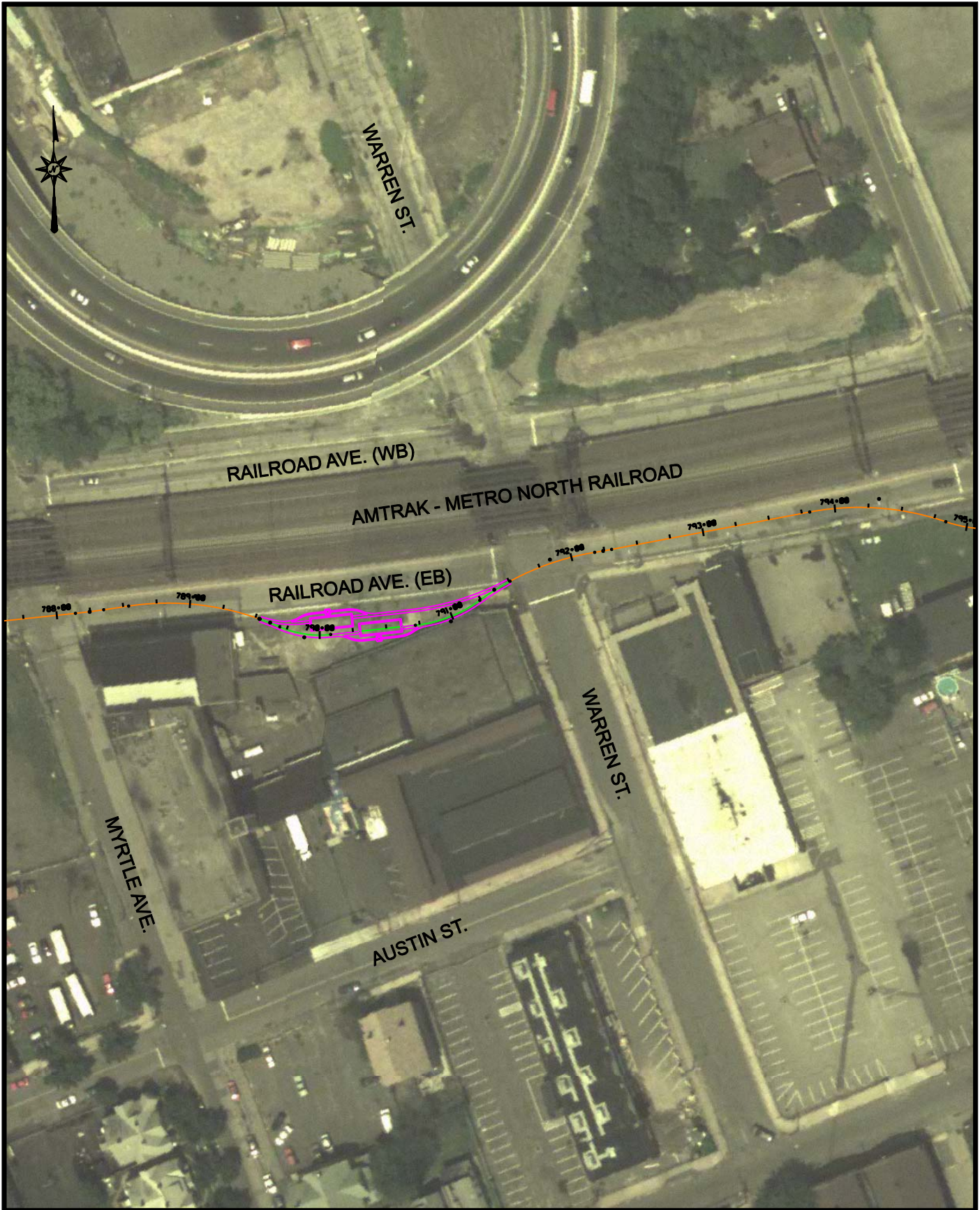
FIGURE BI-15



**SIGNALIZED INTERSECTION OF
RAILROAD AVE. AT MYRTLE AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BI-16



**SIGNALIZED INTERSECTION OF
RAILROAD AVE. AT WARREN ST.
BRIDGEPORT, CONNECTICUT**

SCHMATIC, NOT TO SCALE

FIGURE BI-17



**SIGNALIZED INTERSECTION OF
RAILROAD AVE. AT LAFAYETTE ST.
BRIDGEPORT, CONNECTICUT**

SCHMATIC, NOT TO SCALE

FIGURE BI-18



**SIGNALIZED INTERSECTION OF
RAILROAD AVE. AT BROAD ST.
BRIDGEPORT, CONNECTICUT**

SCHMATIC, NOT TO SCALE

FIGURE BI-19

APPENDIX V

**CITY OF BRIDGEPORT
NOISE CONTROL REGULATIONS**

CITY OF BRIDGEPORT MUNICIPAL CODE

Chapter 8.80 NOISE CONTROL REGULATIONS

Section 8.80.010 Purpose.

It is recognized that people have a right to and should be ensured an environment free from excessive sound and vibration that may jeopardize their health or safety or welfare or degrade the quality of life. This chapter is enacted to protect, preserve and promote the health, safety, welfare and quality of life for the citizens of Bridgeport through the reduction, control and prevention of noise. (Ord. dated 10/2/00: Ord. dated 5/1/00: prior code § 21-35)

Section 8.80.020 Definitions.

The following definitions shall apply in the interpretation and enforcement of this chapter:

"Ambient noise or background" means noise of a measurable intensity which exists at a point as a result of a combination of many distant sources individually indistinguishable.

"City council" means the city council of the city.

"Commercial zone" means any business zone including business zones Nos. 1, 2, 3 and 4 as defined in the zoning regulations of the city.

"Construction" means any site preparation, assembly, erection, substantial repair, alteration or similar action, but excluding demolition, for or of public or private rights-of-way, structures, utilities or similar property.

"Construction equipment" means any equipment or device operated by fuel or electric power used in construction or demolition work.

"Day-time hours" means the hours between seven a.m. and six p.m. Monday through Friday, and the hours between nine a.m. through six p.m. on Saturday and Sunday.

"Decibel" means a logarithmic unit of measure used in measuring magnitudes of sound. The symbol is DB.

"Demolition" means any dismantling, intentional destruction or removal of structures, utilities, public or private right-of-way surface or similar property.

"Domestic power equipment" means, but is not limited to, power saws, drills, grinders, lawn and garden tools and other domestic power equipment intended for use in residential areas by a homeowner.

"Emergency" means any occurrence or set of circumstances involving actual or imminent physical trauma or property damage which demands immediate action.

"Emergency vehicle" means any motor vehicle authorized by the city to have sound warning devices such as sirens and bells which can lawfully be used when responding to an emergency.

"Emergency work" means work made necessary to restore property to a safe condition following an emergency, or work required to protect persons or property from exposure to imminent danger.

"Impulse noise" means sound of short duration (generally less than one second) with an abrupt onset and rapid decay.

"Industrial zone" means any industrial zone including light industrial and heavy industrial as defined in the zoning regulations of the city.

"Legal holiday" means those days designated as legal holidays by Connecticut General Statutes Section 1-4 or its successor (amended July 21, 1999).

"Motor vehicle" shall be defined as per Section 14-1(26) of the Connecticut General Statutes (revision of 1958 as amended).

"Muffler" means a device for abating sounds such as escaping gases.

"Night-time hours" means the hours between six p.m. and seven a.m. Monday through Friday and six p.m. to nine a.m. Saturday and Sunday.

"Noise" means any sound, the intensity of which exceeds the standards set forth in Section 8.80.040 of this chapter.

"Noise level" means the sound pressure level as measured with a sound level meter using the A-weighting network. The level so read is designated DB(A) or dBA.

"Person" means any individual, firm, partnership, association, syndicate, company, trust, corporation, municipality, agency or political or administrative subdivision of the state or other legal entity of any kind.

"Premises" means any building, structure, land or portion thereof, including all appurtenances, and shall include yards, lots, courts, inner yards, and real properties without buildings or improvements, owned or controlled by a person. The emitter's premises includes contiguous publicly dedicated street and highway rights-of-way, all road rights-of-way and waters of the state.

"Property line" means that real or imaginary line along the ground surface and its vertical extension which: (1) separates real property owned or controlled by another person, and (2) separates real property from the public right-of-way.

"Public right-of-way" means any street, avenue, boulevard, highway, sidewalk, alley, park, waterway, railroad or similar place which is owned or controlled by a governmental entity.

"Recreational vehicle" means any nonregistered internal combustion engine powered vehicle which is being used for recreational purposes.

"Residential zone" means any residence zone including residence zones AA, A, B, BB, C, garden apartment zone or residence apartment zone, and any commercial zone when used for residential purposes, as defined in the zoning regulations of the city and all uses permitted therewith either as a right or as a special use.

"Sound" means a transmission of energy through solid, liquid or gaseous media in the form of vibrations which constitute alterations in pressure or position of the particles in the medium and which in the air evoke physiological sensations, including, but not limited to, an auditory response when impinging on the ear.

"Sound level meter" means an instrument used to take sound level measurements and which should conform, as a minimum, to the operational specifications of the American National Standards Institute for Sound Level Meters S1.4--1971 (Type S2A).

"Sound pressure level (SPL)" means twenty (20) times the logarithm to the base ten of the ratio of the pressure of a sound to the reference pressure of twenty micronewtons per square meter (20×10^{-6} Newton/meter²), and is expressed in decibels (dB).

"Vibration" means an oscillatory motion of sound bodies of deterministic or random nature described by displacement, velocity or acceleration with respect to a given reference point. (Ord. dated 10/2/00: Ord. dated 5/1/00: Ord. dated 6/21/99; Ord. dated 12/21/92 § 67; prior code § 21-36)

Section 8.80.030 Noise level measurement procedures.

For the purpose of determining noise levels as set forth in this chapter, the following guidelines shall be applicable:

- A. All personnel conducting sound measurements shall be trained in the current techniques and principles of sound measuring equipment and instrumentation.
- B. Instruments used to determine sound level measurements shall conform to the sound level meters as defined by this chapter.
- C. The general steps listed below shall be followed when preparing to take sound level measurements:
 1. The instrument manufacturer's specific instructions for the preparation and use of the instrument shall be followed.
 2. The sound level meter shall be calibrated before and after each set of measurements.
 3. The sound level meter shall be placed at an angle to the sound source as specified by the manufacturer's instructions and at least four feet above the ground. It shall be so placed as not to be interfered with by individuals conducting the measurements.
 4. Measurements shall be taken at a point that is located about one foot beyond the boundary of the emitter's premises within the noise receptor's premises. The emitter's premises includes his/her individual unit of land or group of contiguous parcels under the same ownership as indicated by public land records. (Ord. dated 10/2/00: Ord. dated 5/1/00: prior code § 21-37)

Section 8.80.040 Noise levels.

- A. It is unlawful for any person to emit or cause to be emitted any noise beyond the boundaries of his/her premises in excess of the noise levels established in these regulations.
- B. Noise level standards :

Receptor' s Zone				
Emitter' s Zone	Industrial	Commercial	Residential/Day	Residential/Night
Residential	62 dBA	55 dBA	55 dBA	45 dBA
Commercial	62 dBA	62 dBA	55 dBA	45 dBA
Industrial	70 dBA	66 dBA	61 dBA	51 dBA

- C. High background noise levels and impulse noise.
 - 1. In those individual cases where the background noise levels caused by sources not subject to these regulations exceed the standards contained in this chapter, a source shall be considered to cause excessive noise if the noise emitted by such source exceeds the background noise levels by five dBA, provided that no source subject to the provisions of this chapter shall emit noise in excess of eighty (80) dBA at any time, and provided that this section does not decrease the permissible levels of other sections of this chapter.
 - 2. No person shall cause or allow the emission of impulse noise in excess of eighty (80) dB peak sound pressure level during the nighttime to any residential noise zone.
 - 3. No person shall cause or allow the emission of impulse noise in excess of one hundred (100) dB peak sound pressure level at any time to any zone.
- D. Exclusions. These levels shall not apply to noise emitted by or related to:
 - 1. Natural phenomena;
 - 2. Any bell or chime from any building clock, schools or church;
 - 3. Any siren, whistle or bell lawfully used by emergency vehicles or any other alarm systems used in an emergency situation; provided, however, that burglar alarms not terminating within thirty (30) minutes after being activated shall be unlawful;
 - 4. Warning devices required by OSHA or other state or federal safety regulations;
 - 5. Farming equipment or farming activity operated within an area used for strictly farming.
- E. Exemptions. The following shall be exempt from these regulations subject to special conditions as spelled out:
 - 1. Noise created as a result of, or relating to an emergency;
 - 2. Noise from domestic power equipment such as, but not limited to, power saws, sanders, grinders, lawn and garden tools or similar devices operated during daytime hours;
 - 3. Noise from snow removal equipment operated by the city;
 - 4. Noise created by an aircraft flight operations which are specifically preempted by the Federal Aviation Administration;
 - 5. Noise created by any recreational activities which are permitted by law and for which a license or permit has been granted by the town, including, but not limited to, parades, sporting events, concerts and firework displays;
 - 6. Noise created by refuse and solid waste collection provided that the activity is conducted during daytime hours;
 - 7. Noise generated by any construction equipment which is operated during daytime hours, provided that operation of construction equipment during night-time hours shall not exceed the maximum

noise levels as specified in Section 8.80.040. (Ord. dated 10/2/00: Ord. dated 5/1/00: prior code § 21-38)

Section 8.80.050 Prohibited noise activities.

- A. General prohibition. It is unlawful for any person to make, continue or cause to be made or continued any loud, unnecessary or unusual noise or any noise which either annoys, disturbs, injures or endangers the comfort, repose, health, peace or safety of others.
- B. Officers of the police department shall issue a notice of violation for the following violations of this section of the noise ordinance without use of a sound level meter.
- C. Enumeration of prohibited acts. Each of the following acts, among others, is declared unlawful and is prohibited; but this enumeration shall not be deemed to be exclusive:
 - 1. Blowing horns, etc. The sounding of any horn or signal device on any automobile, motorcycle, bus, streetcar or other vehicle while not in motion, except as a danger signal if another vehicle is approaching apparently out of control, or if in motion only as a danger signal after or as brakes are being applied and deceleration of the vehicle is intended; the creation by means of any such signal device of any unreasonably loud or harsh sound; and the sounding of such device for an unnecessary and unreasonable period of time;
 - 2. Radios, phonographs, etc. The playing of any radio, phonograph or any musical instrument in such a manner or with such volume as to annoy or disturb the quiet, comfort or repose of persons in any office, or in any dwelling, hotel or other type of residence, or of any person in the vicinity;
 - 3. Shouting, singing, etc. Yelling, shouting, hooting, whistling or singing on the public street at any time or place so as to annoy or in any dwelling, hotel or other type of residence, or of any person in the vicinity;
 - 4. Animals. The keeping of any animal or bird which by causing frequent or long-continued noise shall disturb the comfort repose of any person in the vicinity;
 - 5. Steam whistles. The blowing of any steam whistle attached to any stationary boiler except to give notice of the time to begin or stop work or as a warning of fire or danger, or upon request of proper city authorities;
 - 6. Exhaust discharge. The discharge into the open air of the exhaust of any steam engine, stationary internal combustion engine, motor vehicle or motorboat engine except through a muffler or other device which will effectively prevent loud or explosive noises therefrom;
 - 7. Building operations. The erection (including excavating), demolition, alteration or repair of any residential building or the excavation of streets and highways, between the evening hours of six p.m. and seven a.m. Monday through Friday and the evening hours of six p.m. and nine a.m. on Saturday and Sunday and non-legal holiday weekdays, except in case of urgent necessity in the interest of public safety, and then only with a permit from the board of building commissioners or any other board or commission in charge, which permit may be granted for a period not to exceed three days while the emergency continues. At all other times the owner, tenant, or their agent, shall have the right to erect and repair their property (amended June 21, 1999);
 - 8. Loading and unloading operations. The creation of a loud and excessive noise in connection with loading or unloading any vehicle or the opening and destruction of bales, boxes, crates and containers;
 - 9. Truck idling. No person shall operate an engine or any standing motor vehicle with a weight in excess of ten thousand (10,000) pounds manufacturer' s gross vehicle weight (GVW) for a period in excess of ten minutes when such vehicle is parked on a residential premises or on a city road next to a residential premises.
 - 10. The creation of any excessive noise on any street that is adjacent to any school, institution of learning, church or court while the same is in session, or is adjacent to any hospital, or medical

facility for treatment which interferes with the working or sessions thereof or the persons therein.
(Ord. dated 10/2/00: Ord. dated 5/1/00: Ord. dated 6/21/99: prior code § 21-38)

Section 8.80.060 Motor vehicle noise.

- A. All motor vehicles operated within the limits of the city shall be subject to the noise standards and decibel levels set forth in the regulations authorized in Section 14-80a of the General Statutes.
 - B. No sound amplifying devices on or within motor vehicles shall emit noise in excess of noise levels as specified in Section 8.80.040.
 - C. This section dealing with motor vehicle noise shall be enforced by the chief of police and/or his designated subordinates. (Ord. dated 10/2/00: Ord. dated 5/1/00: Ord. dated 12/21/92 § 75(a); prior code § 21-40)
-

Section 8.80.070 Recreational vehicle noise.

No person shall create or cause to be created any unreasonably loud or disturbing noise due to the operation of a recreational vehicle. A noise shall be deemed to be unreasonably loud and a violation of this chapter when the noise so generated exceeds the noise level standard set forth in Section 8.80.040. (Ord. dated 10/2/00: Ord. dated 5/1/00: prior code § 21-41)

Section 8.80.080 Inspections.

- A. For the purpose of determining compliance with the provisions of this chapter, the director of health or his designated representative, as well as officers of the police department, are authorized to make inspections of all noise sources and to take measurements and make tests whenever necessary to determine the quantity and character of noise. In the event that any person refuses or restricts entry and free access to any part of a premises or refuses inspection, testing or noise measurement of any activity, device, facility or process where inspection is sought, the director of health or his designated representative may seek from the appropriate court a warrant without interference, restriction or obstruction at a reasonable time, for the purpose of inspecting, testing or measuring noise.
 - B. It is unlawful for any person to refuse to allow or permit the director of health or his designated representative free access to any premises when the director of health or his designated representative is acting in compliance with a warrant for the inspection and order issued by the appropriate court.
 - C. It is unlawful for any person to violate the provisions of any warrant or court order requiring inspection, testing or measurement of noise sources.
 - D. No person shall hinder, obstruct, delay, resist, prevent in any way, interfere or attempt to interfere with any authorized person while in the performance of his/her duties under this chapter. (Ord. dated 10/2/00: Ord. dated 5/1/00: prior code § 21-42)
-

Section 8.80.090 Variance and contracts.

- A. Variances.

1. Any person living or doing business in Bridgeport may apply to the department of health for a variance from one or more of the provisions of this chapter, which are more stringent than the Connecticut Department of Environmental Protection regulations for the control of noise, provided that the applicant supplies all the following information to the director of health;
 - a. The location and nature of the activity;
 - b. The time period and hours of operation of said activity;
 - c. The nature and intensity of the noise that will be generated; and
 - d. Any other information required by the director of health.
 2. No variance from these regulations shall be issued unless it has been demonstrated that:
 - a. The proposed activity will not violate any provisions of the Connecticut Department of Environmental Protection regulations;
 - b. The noise levels generated by the proposed activity will not constitute a danger to the public health; and
 - c. Compliance with the regulations constitutes an unreasonable hardship on the applicant.
 3. The application for variance shall be reviewed and either approved or rejected within fifteen (15) days of receipt by the director of health. The approval or rejection shall be in writing and shall state the condition(s) of approval of the variance.
 4. Failure to rule on the application in the designated time shall constitute approval of the variance.
- B. Recourse. Any person aggrieved by the decision of the director of health with respect to any variances may appeal in accordance with the Charter to the environmental review board within a period of fourteen (14) days of receipt of the health director's decision.
- C. Contracts. Any written agreement, purchase order or contract whereby the city is committed to an expenditure of funds in return for work, labor, services, supplies, equipment, materials or any combination thereof, shall not be entered into unless such agreement purchase order or instrument contains provisions that any equipment or activities which are subject to the provisions of this chapter will be operated, constructed, conducted or manufactured without violating the provision of this chapter. (Ord. dated 10/2/00: Ord. dated 5/1/00: prior code § 21-44)
-

Section 8.80.100 Violation--Penalties.

Any person in violation of any of the provisions of this chapter shall be fined in an amount of one hundred dollars (\$100.00) made payable to the city of Bridgeport. Each day such violation continues after the time for correction of the violation has been given in an order shall constitute a continuing violation. The imposition of any punishment under this chapter shall not prevent the enforced abatement of any unlawful conditions by the city. (Ord. dated 10/2/00: Ord. dated 5/1/00: prior code § 21-43; amended June 21, 1999)

Section 8.80.110 Noise violation appeals.

- A. Administrative appeal.
1. Any person, operator or owner of a vehicle which has been cited under Chapter 8.80 of this article may submit a written request for an administrative appeal to the chief of police or his designee of the issuance of such citation within a designated appeal period of not more than fourteen (14) days from the date of the citation.
 2. The chief of police or his designee shall establish and publish notices indicating the procedures to request administrative appeal under subsection (A)(1) above and shall cause notice of appeal rights to be printed on each violation notice issued.
 3. Payment of the penalty/fines shall be stayed pending the administrative appeal. Payment of the penalty/fine shall be made within fourteen (14) days of mailing of finding of the administrative

appeal unless a finding was made in favor of the appellant or the appellant has elected to proceed under subsection (B) below.

4. Any person who has requested administrative appeal shall be notified in writing within forty-five (45) days of the issuance of the citation, of the findings relative to the appeal. If dissatisfied with such finding, a formal hearing may be requested by submitting a written request to the chief of police or his designee within fourteen (14) days of such finding of the administrative appeal in accordance with subsection (B) of this section.

B. Formal hearing procedure: noise ordinance violations hearing officer.

1. Pursuant to Connecticut General Statute Sec. 7-148, as amended, the mayor shall appoint, with the approval of the city council, one or more noise ordinance violations hearing officer(s) (the "officer"). Officer(s) shall not be employed by the police department. Officer(s) shall serve for a term of two years or part thereof, which term shall commence from date of approval by the city council and shall end on December 31 of every even year. Officer(s) may be compensated by the city with the funds appropriated for this purpose as recommended by the mayor and approved by the city council.
2. Officer(s) shall be empowered to hear appeals from the issuance of noise violation citations or as otherwise herein provided.
3. Any person, owner or operator of a vehicle cited pursuant to Chapter 8.80 may request a formal hearing before officer(s) within fourteen (14) days of any of the following events:
 - a. Issuance of noise violation citation.
 - b. Issuance of adverse findings in the administrative appeal.
 - c. First issuance of notice of delinquency of noise violation citation.
4. Hearing procedure shall comply with Connecticut General Statute, Sec. 7-148.
 - a. In scheduling formal appeal hearings, the appellant shall be notified by mail of the place and time of hearing. Such notice shall be provided at least fifteen (15) days but not more than thirty (30) days prior to the scheduled hearing date.
 - b. The procedure for the hearing shall be informal as to the rules of evidence, but testimony shall be taken under oath or affirmation.
 - c. In considering an appeal, the hearing office may consider all relevant facts and circumstances and may require personal appearance of the appellant and issuing officer.
 - d. Should the officer find in favor of the appellant, he shall so certify to the police department and the record of the citation shall be removed from the files of the city.
 - e. Should the officer find the issuance of the citation proper, he shall so certify to the police department and no further appeal under this section shall be considered, either administrative or formal.
 - f. If such penalty is not paid on the date of its entry, the city may proceed to enforce the penalty pursuant to C.G.S. Secs. 7-148 (10)(A) as amended. (Ord. dated 10/2/00)

APPENDIX VI

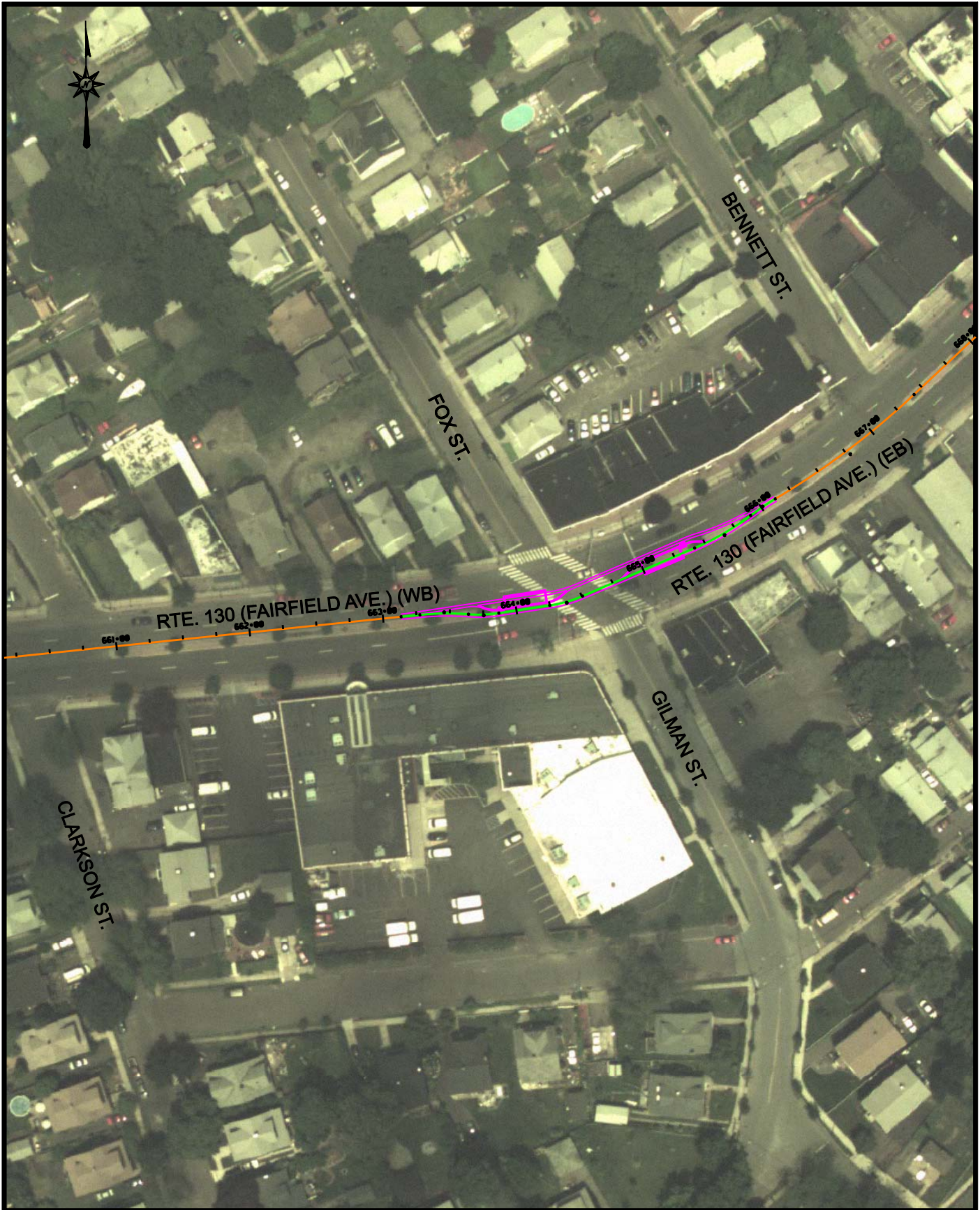
VAULT LOCATIONS AERIAL PHOTOGRAPHS



**VAULTS 7539 AND 6439
RTE. 130 (FAIRFIELD AVE.)
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BV-1



**VAULTS 7540 AND 6440
RTE. 130 (FAIRFIELD AVE.)
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BV-2



**VAULTS 7541 AND 6441
RTE. 130 (FAIRFIELD AVE.)
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

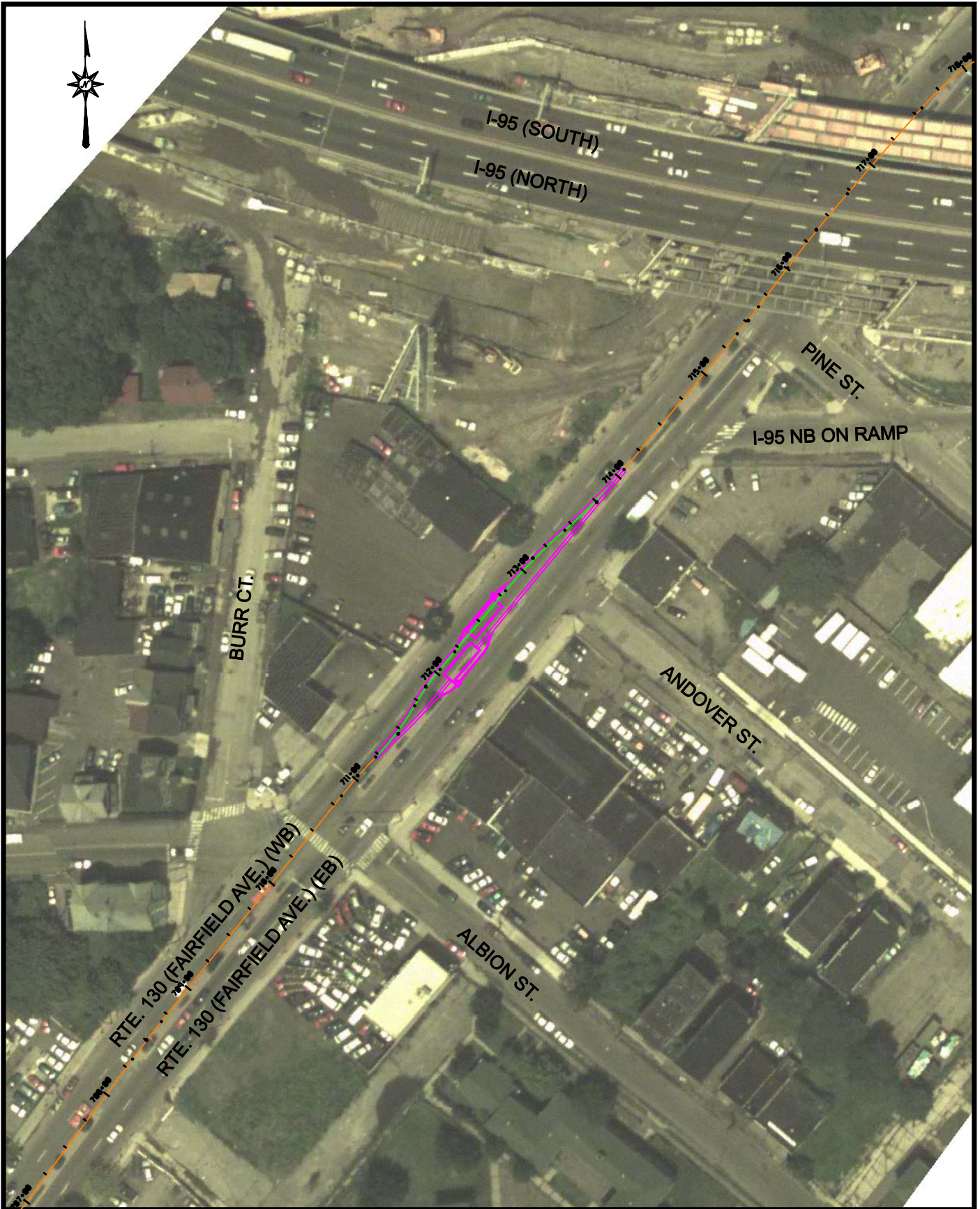
FIGURE BV-3



**VAULTS 7542 AND 6442
RTE. 130 (FAIRFIELD AVE.)
BRIDGEPORT, CONNECTICUT**

SCHMATIC, NOT TO SCALE

FIGURE BV-4



**VAULTS 7543 AND 6443
RTE. 130 (FAIRFIELD AVE.)
BRIDGEPORT, CONNECTICUT**

SCHMATIC, NOT TO SCALE

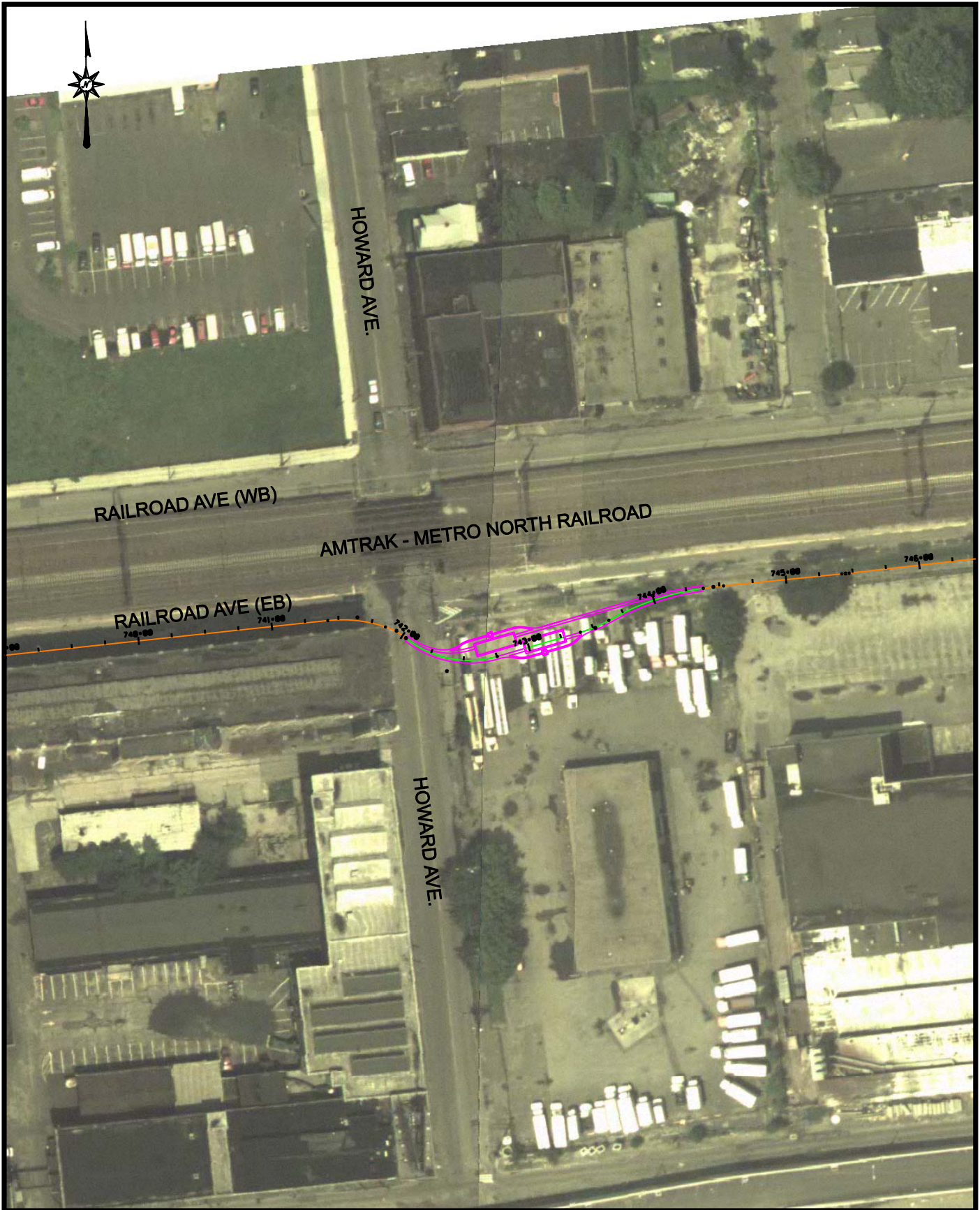
FIGURE BV-5



**VAULTS 7544 AND 6444
RAILROAD AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BV-6



**VAULTS 7545 AND 6445
RAILROAD AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

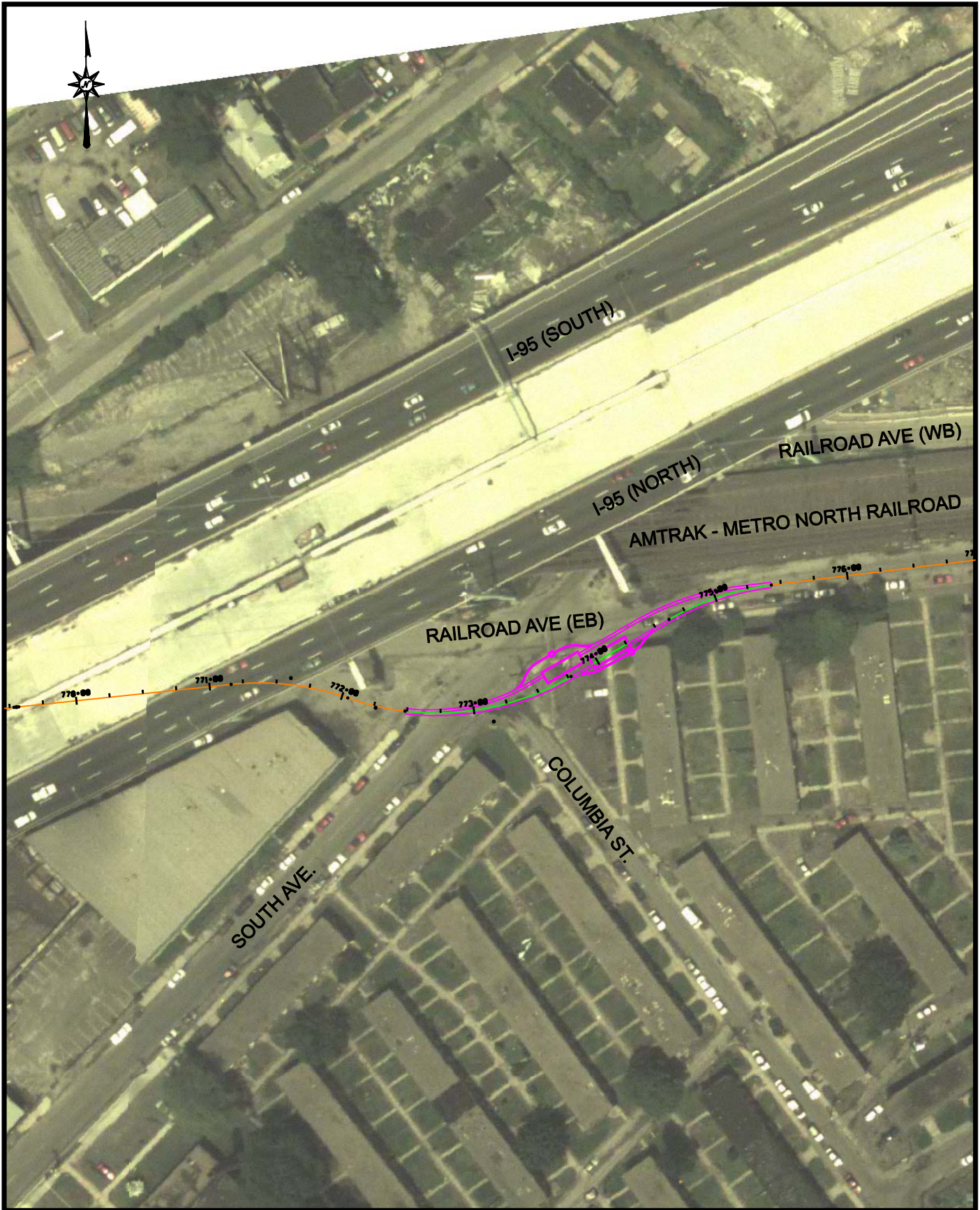
FIGURE BV-7



**VAULTS 7546 AND 6446
RAILROAD AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

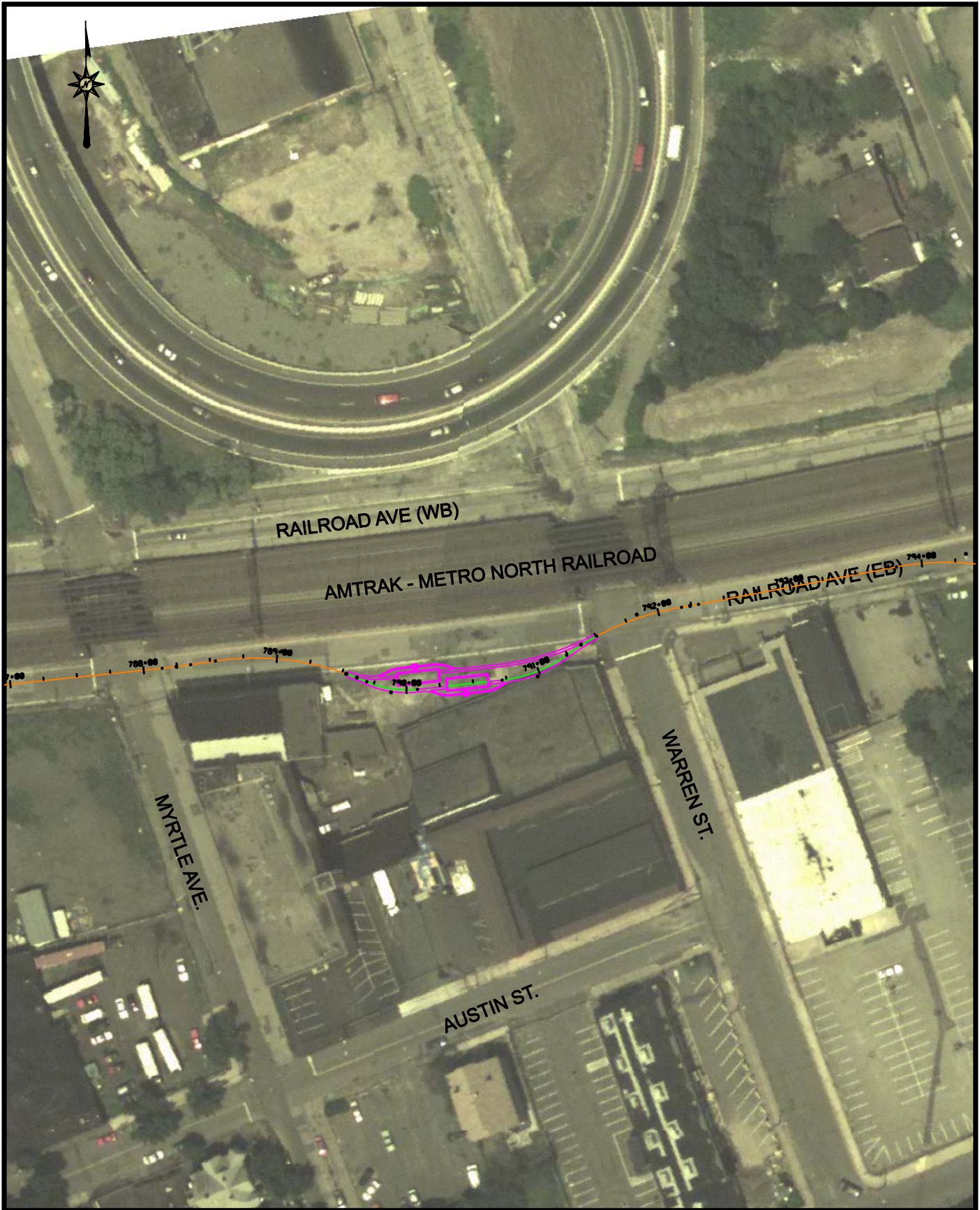
FIGURE BV-8



**VAULTS 7547 AND 6447
RAILROAD AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BV-9



**VAULTS 7548 AND 6448
RAILROAD AVE.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BV-10



**VAULTS 7549 AND 6449
BROAD ST.
BRIDGEPORT, CONNECTICUT**

SCHEMATIC, NOT TO SCALE

FIGURE BV-11