



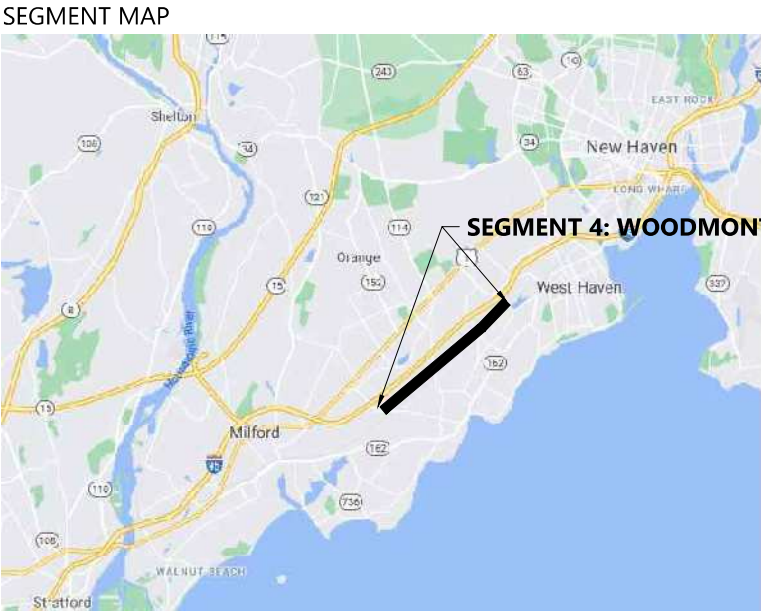
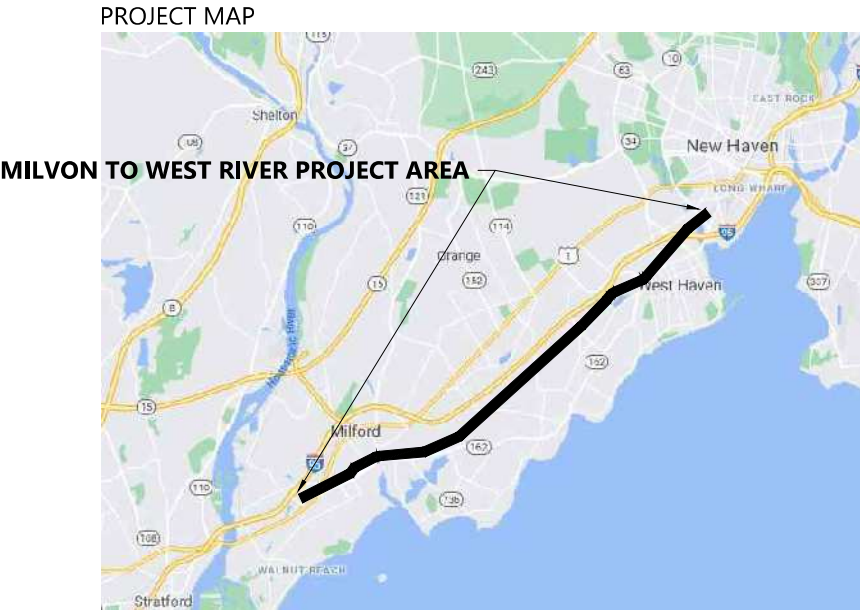
DEVELOPMENT & MANAGEMENT PLAN

MILVON TO WEST RIVER RAILROAD TRANSMISSION LINE 115-kV REBUILD PROJECT
(CONNECTICUT SITING COUNCIL DOCKET NO. 508)

SEGMENT 4: WOODMONT SUBSTATION TO ALLINGS CROSSING SUBSTATION
CITY OF MILFORD, TOWN OF ORANGE, CITY OF WEST HAVEN
NEW HAVEN COUNTY, CONNECTICUT

VOLUME 2: MAPS, DRAWINGS AND RESOURCE SPECIFICATIONS JUNE 2024

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STR 2 OF 2	TYPICAL FOUNDATION & GROUNDING DETAILS



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UI AVANGRID				UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN			
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REV.	DATE	BY	DESCRIPTION	APP.	DATE	FILE	REV.
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR.	SFB	SCALE AS SHOWN	0-0B
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW	CK	JRD	NO.	
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CROSS SECTION INDEX MAP



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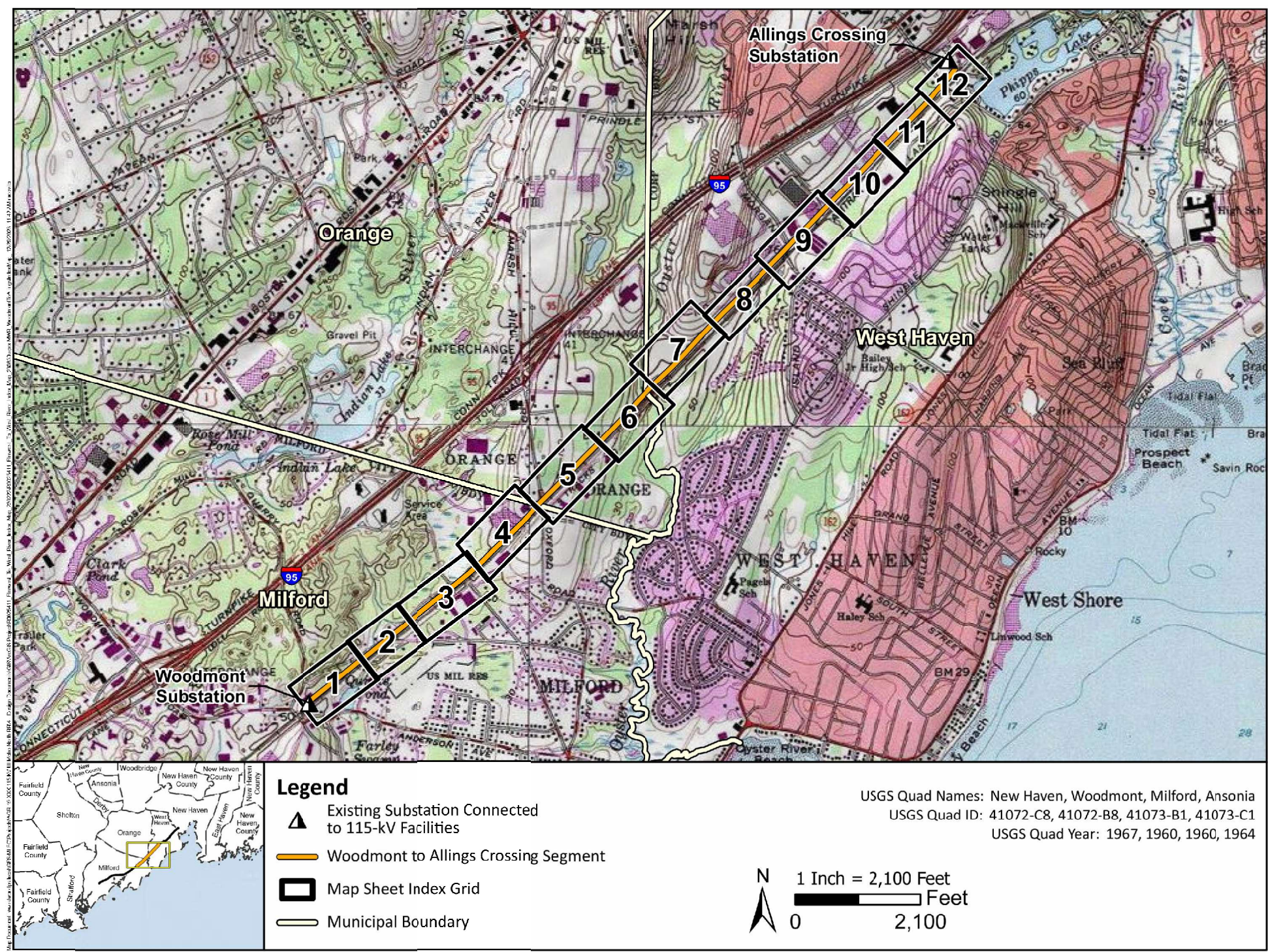
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REV.	DATE	BY	DESCRIPTION	APP.	APP.	MSP				
					DATE:	JUNE 2024				



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					MAPSHEET INDEX						
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION		DR.	SFB	SCALE AS SHOWN FILE:				
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW		CK	JRD	NO.		REV.		
REV.	DATE	BY	DESCRIPTION		APP.	MSP			0-0B		
						DATE: JUNE 2024					

ANSI D CADD Drawing. DO NOT REVISE MANUALLY.

STRUCTURE NO.	CROSS SECTION #	STRUCTURE HEIGHT (FT)	A (ft)	B (ft)	C (ft)	D (ft)
P959N	7	81.5	144	20	124	-
P960N	7	86	143	25	118	-
P961N	7	111	119	25	94	-
P962N	7	117	96	29	67	-
P963N	7	117	79	29	50	-
P964N	7	106	61	28	33	-
P965N	7	112	61	28	33	-
P966AN	7	116.5	96	30	66	-
P968N	7	117	96	32	64	-
P969N	7	112	96	33	63	-
P970N	7	101	56	23	33	-
P971N	7	106	56	23	33	-
P977N	7	112	96	22	74	-
P978N	7	91	96	30	66	-
P979N	7	81	96	30	66	-
P980N	7	81	96	25	71	-
P981N	7	86	76	25	51	-
P982N	7	96	77	26	51	-
P983N	7	106.5	76	42	34	-
P985N	7	106	76	42	34	-
P986N	7	91	76	34	42	-
P987AN/NN	7	96	76	28	48	-
P988N	7	96	76	35	41	-
P989N	7	91	76	40	36	-
P990N	7	86	76	40	36	-
P991N	7	91	76	28	48	-
P992N	7	96.5	76	24	52	-
P993N	7	106	76	25	51	-
P994N	7	121	76	25	51	-
P1008WN	7	101	71	39	32	-

EXISTING
(VIEW FACING EAST)

^A THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

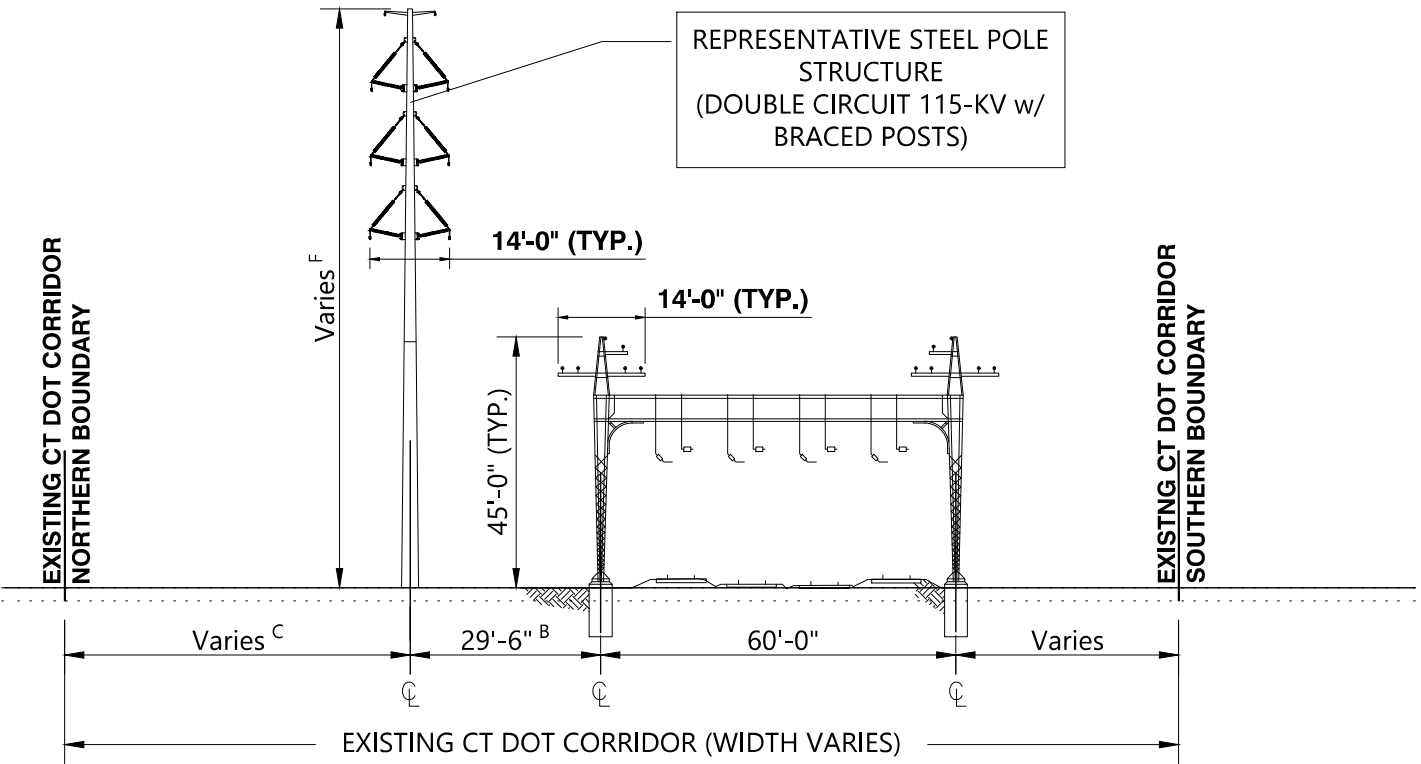
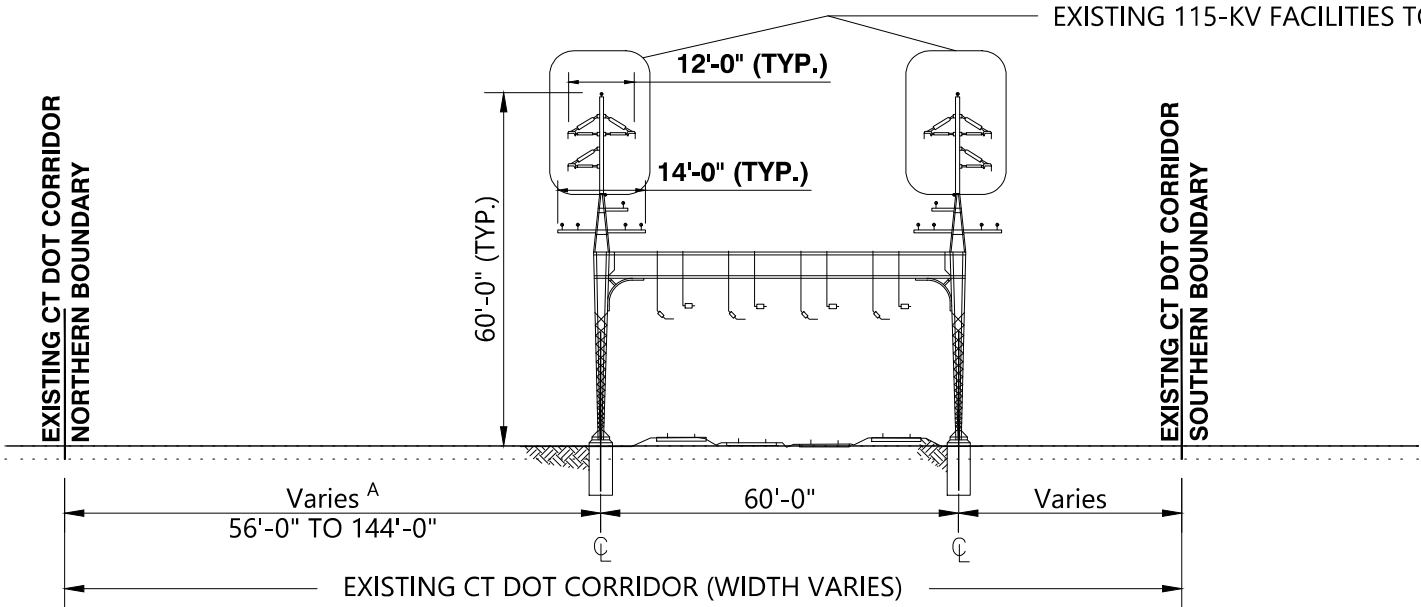
^B THIS DISTANCE AVERAGES TO BE 29'-6" BUT RANGES FROM 20'-0" TO 42'-0". REFER TO TABLE ABOVE FOR EXACT DISTANCES.

^C THE DISTANCE BETWEEN THE PROPOSED 115-KV DOUBLE-CIRCUIT STRUCTURES AND THE EXISTING CT DOT CORRIDOR NORTHERN BOUNDARY VARIES. REFER TO TABLE ABOVE FOR EXACT DISTANCES.

^F THE HEIGHT OF THE NEW 115-KV DOUBLE-CIRCUIT STRUCTURES VARIES. REFER TO TABLE ABOVE FOR EXACT STRUCTURE HEIGHTS.

PROPOSED
(VIEW FACING EAST)

CROSS SECTION 7
WOODMONT SUBSTATION TO ALLINGS CROSSING SUBSTATION:
STR P959N (MILFORD) TO STR P971N (MILFORD) - 0.74 MILES,
STR P977N (ORANGE) TO STR P994N (WEST HAVEN) - 1.14 MILES



- NOTES:
1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. DEAD END STRUCTURES WILL DIFFER.
 2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING OF TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE, BOTH ON THE NORTHERN AND SOUTHERN CATENARY SUPPORT COLUMNS.
 3. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
 4. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
 5. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
 6. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 32'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR NORTHERN BOUNDARY IS GREATER THAN 32'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI WILL NOT NEED ANY PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
 7. DISTANCE FROM SOUTHERN CATENARY STRUCTURE TO CT DOT CORRIDOR SOUTHERN BOUNDARY WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.

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						UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN			
						XS-7			
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR.	SFB	SCALE	AS SHOWN	FILE:	
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REV.	DATE	BY	DESCRIPTION	APP.	MSP	DATE:	JUNE 2024	XS 1 OF 3	REV. 0-0B

ANSI D CADD Drawing. DO NOT REVISE MANUALLY.

STRUCTURE NO.	CROSS SECTION #	STRUCTURE HEIGHT (FT)	A (ft)	B (ft)	C (ft)	D (ft)
P972N	8	101	45	27	18	14
P973AN/NN	8	96	43	23	20	13
P974N	8	106	56	25	31	1
P975EN	8	101	55	30	25	7
P996N	8	111	36	31	5	27
P997N	8	101	36	28	8	24
P998N	8	91	36	31	5	27
P999N	8	91	37	29	8	21
P1000N	8	91	37	29	8	20
P1001N	8	91	37	25	12	20
P1002N	8	91	36	26	10	22
P1003N	8	91	36	25	11	21
P1004N	8	81	36	32	4	28
P1005N	8	76	36	32	4	28
P1006N	8	86.5	37	26	11	21

EXISTING
(VIEW FACING EAST)

^A THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

^B THIS DISTANCE AVERAGES TO BE 28'-0" BUT RANGES FROM 23'-0" TO 32'-0". REFER TO TABLE ABOVE FOR EXACT DISTANCES.

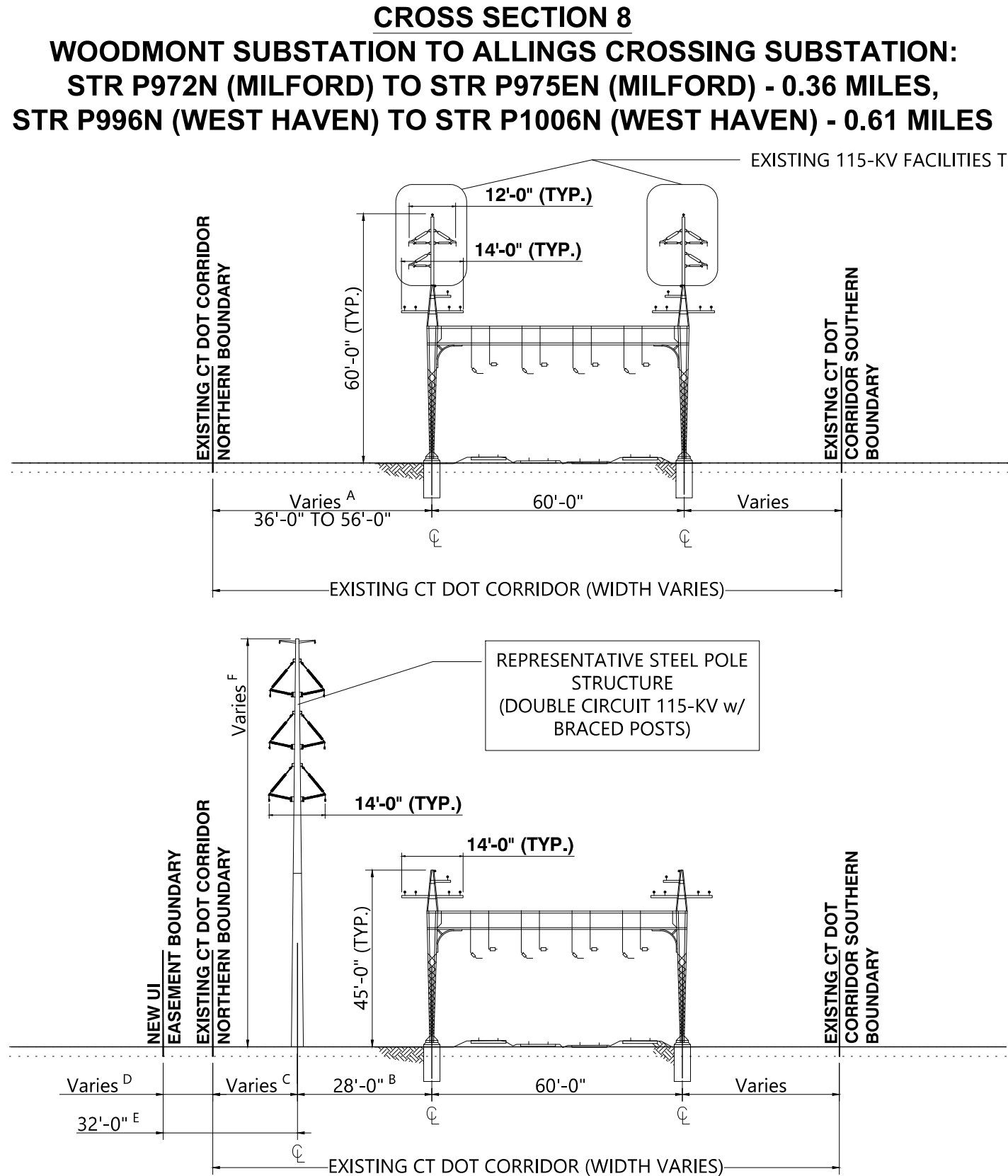
^C THE DISTANCE BETWEEN THE PROPOSED 115-KV DOUBLE-CIRCUIT STRUCTURES AND THE EXISTING CT DOT CORRIDOR NORTHERN BOUNDARY VARIES. REFER TO TABLE ABOVE FOR EXACT DISTANCES.

^D THE WIDTH OF UI'S REQUIRED NEW PERMANENT EASEMENT VARIES. REFER TO TABLE ABOVE.

^E REFER TO PROJECT MAPS AND DRAWINGS, FOR PROPOSED EASEMENT BOUNDARY LIMITS.

^F THE HEIGHT OF THE NEW 115-KV DOUBLE-CIRCUIT STRUCTURES VARIES. REFER TO TABLE ABOVE FOR EXACT STRUCTURE HEIGHTS.

PROPOSED
(VIEW FACING EAST)



- NOTES:
1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. DEAD END STRUCTURES WILL DIFFER.
 2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING OF TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE, BOTH ON THE NORTHERN AND SOUTHERN CATENARY SUPPORT COLUMNS.
 3. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
 4. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
 5. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
 6. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 32'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR NORTHERN BOUNDARY IS LESS THAN 32'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS. REFER TO PROJECT MAPS AND DRAWINGS, FOR PROPOSED ADDITIONAL PERMANENT EASEMENT LOCATIONS.
 7. DISTANCE FROM SOUTHERN CATENARY STRUCTURE TO CT DOT CORRIDOR SOUTHERN BOUNDARY WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.

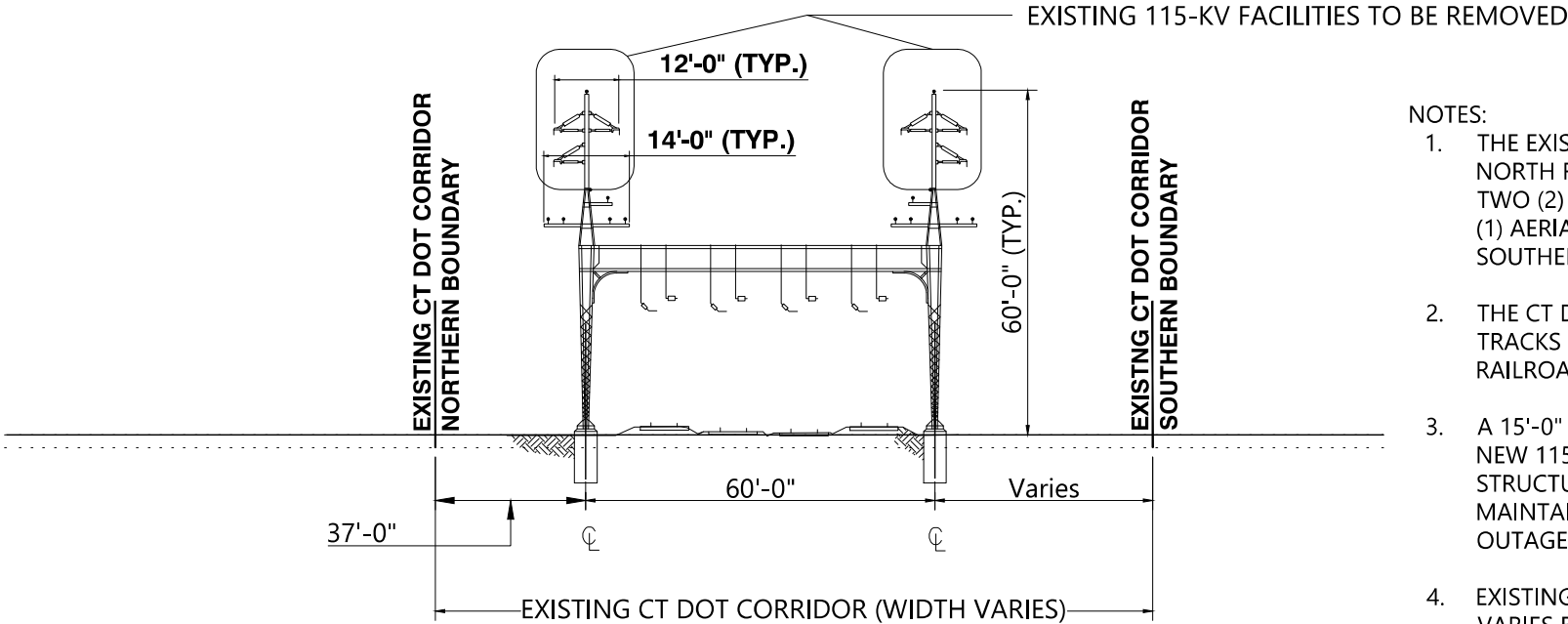
ANSI D CADD Drawing. DO NOT REVISE MANUALLY.

STRUCTURE NO.	CROSS SECTION #	STRUCTURE HEIGHT (FT)	A (ft)	B (ft)	C (ft)	D (ft)
P1007N	8a	101	37	51	-	46

CROSS SECTION 8a
WOODMONT SUBSTATION TO ALLINGS CROSSING SUBSTATION:
STR P1007N (WEST HAVEN)

EXISTING

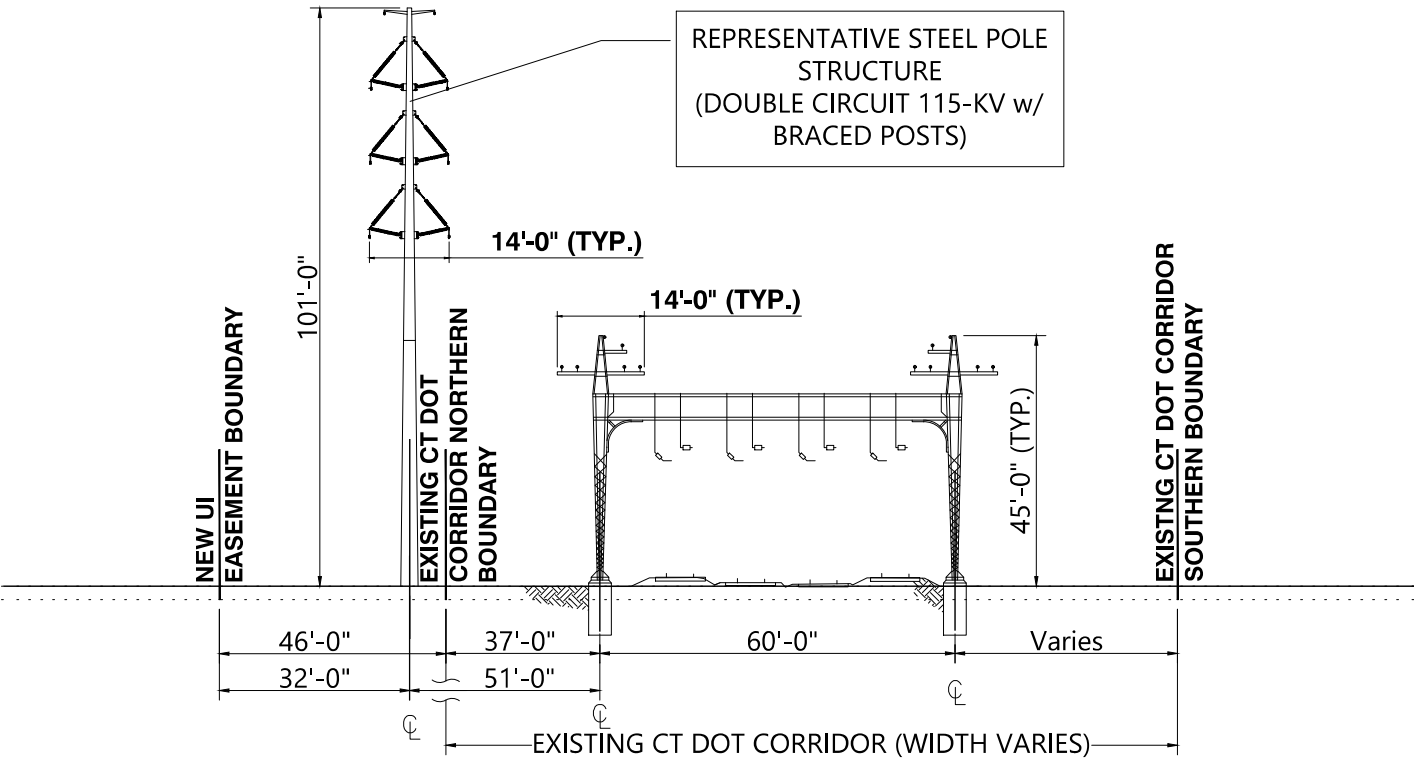
(VIEW FACING EAST)



- NOTES:
1. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING OF TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE, BOTH ON THE NORTHERN AND SOUTHERN CATENARY SUPPORT COLUMNS.
 2. THE CT DOT CORRIDOR CONTAINS THREE (3) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
 3. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
 4. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO PROJECT MAPS AND DRAWINGS FOR LOCATIONS OF PROPOSED TREE REMOVAL
 5. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 32'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE SURROUNDING ENVIRONMENT REQUIRES A PROPOSED STRUCTURE TO BE PLACED OUTSIDE OF THE CT DOT CORRIDOR, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS. REFER TO PROJECT MAPS AND DRAWINGS, FOR PROPOSED CLEARING AND ADDITIONAL PERMANENT EASEMENT LOCATIONS.
 6. DISTANCE FROM SOUTHERN CATENARY STRUCTURE TO CT DOT CORRIDOR SOUTHERN BOUNDARY WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.

PROPOSED

(VIEW FACING EAST)



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						UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN			
						XS-8a			
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR.	SFB	SCALE	AS SHOWN	FILE:	
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REV.	DATE	BY	DESCRIPTION	APP.	DATE:	JUNE 2024	XS 3 OF 3		REV. 0-0B

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P958ES, P959WS, P959N, P959S, P960N, P961N, AND P962N WILL BE INSTALLED.
- BONNETS B959S, B960N/S, B960AN/S, B961N/S, B961AN/S, AND B962AN/S WILL BE REMOVED.
- HARDWARE AND INSULATORS ON THE TERMINATION STRUCTURES ASSOCIATED WITH LINES 89005B-1, 8804A, AND 8904B AT WOODMONT SUBSTATION WILL BE REPLACED.
- EXISTING STEEL POLES P958N AND P958S OUTSIDE AND ADJACENT TO WOODMONT SUBSTATION WILL REMAIN; HARDWARE AND INSULATORS WILL BE REPLACED.
- EXISTING STEEL STRUCTURES ACROSS THE TRACKS FROM THE WOODMONT SUBSTATION WILL BE REMOVED.
- ONE EXISTING STEEL POLE (P959AN) TO BE REMOVED.
- EXISTING WOOD POLES FROM P962N TO P963N TO BE REMOVED.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.

CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

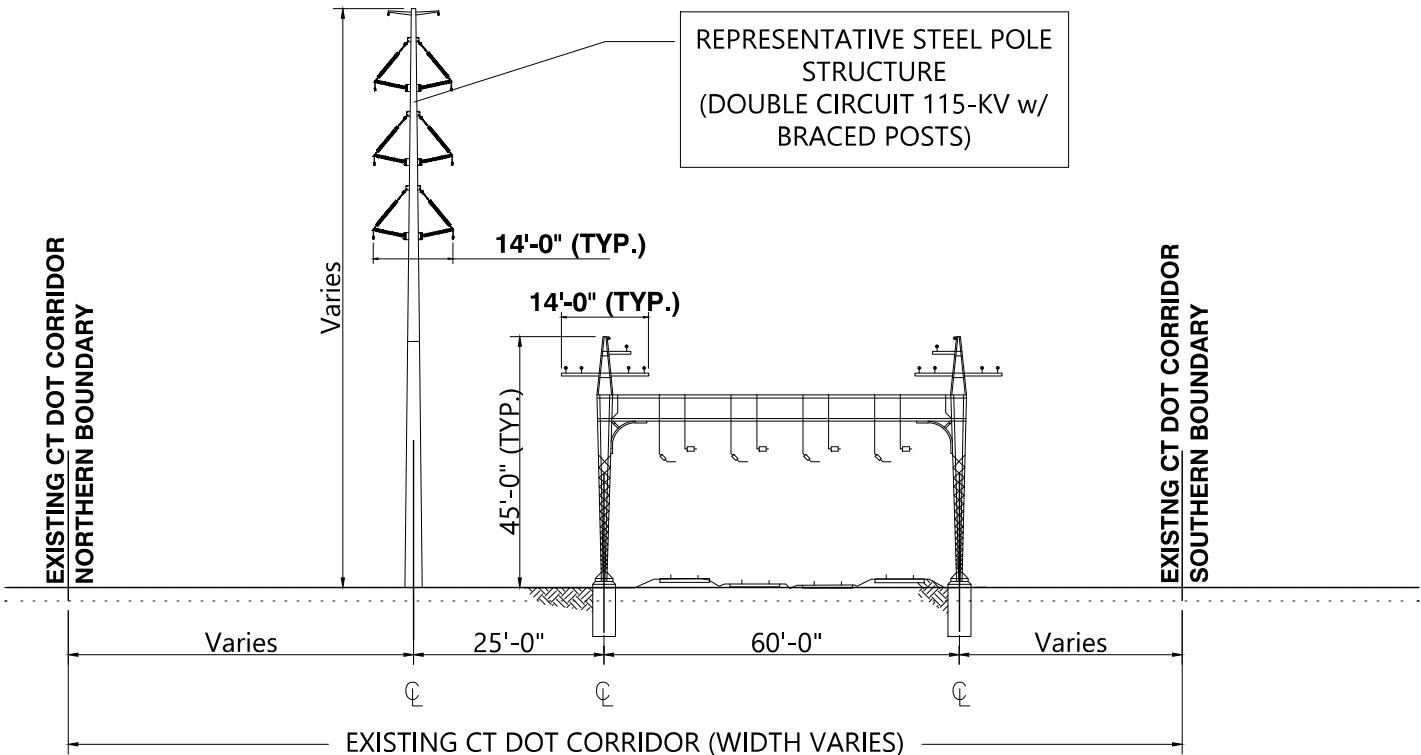
- WETLAND M-W10
- WETLAND M-W11
- WATERCOURSE M-WC6
- WATERCOURSE M-WC7
- QUIRKS POND
- PORTION OF SHEET WITHIN NDDB AREA. COORDINATE WITH ENVIRONMENTAL INSPECTOR/SPECIES PROTECTION SPECIALIST REGARDING WORK IN NDDB AREAS

ROAD CROSSINGS

- NONE

Drainage ID	Watercourse ID	Status	Crossing type/Culvert size	Closest Structure
09	M-WC6	Temporary	Mat bridge	P961N

DRAINAGE CROSSING TABLE



XS-7 (FACING EAST)

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P958ES	DEADEND	N/A*	81	CONCRETE DRILLED PIER
P959WS	DEADEND	N/A*	81	CONCRETE DRILLED PIER
P959S	DEADEND	N/A*	81.5	CONCRETE DRILLED PIER
P959N	DEADEND	XS-7	81.5	CONCRETE DRILLED PIER
P960N	TANGENT	XS-7	86	CONCRETE DRILLED PIER
P961N	TANGENT	XS-7	111	CONCRETE DRILLED PIER
P962N	TANGENT	XS-7	117	CONCRETE DRILLED PIER
*THESE STRUCTURE ARRANGEMENTS ARE NON-TYPICAL, SEE PROJECT PLAN AND PROFILE DRAWINGS FOR DETAILS				

STRUCTURE DETAIL TABLE

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P963N, P964N, AND P965N WILL BE INSTALLED.
- BONNETS B962AN/S, B963N/S, B963AN/S, B964N/S, B964AN/S, B965N/S, AND B965AN/S WILL BE REMOVED.
- EXISTING WOOD POLES FROM P962N TO P963N TO BE REMOVED, TWO WOOD POLES NEAR P964N TO BE REMOVED.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EXCLUSIONARY FENCING TO BE COORDINATED WITH UI IN ACCORDANCE WITH THE SPECIES PROTECTION PLAN.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.

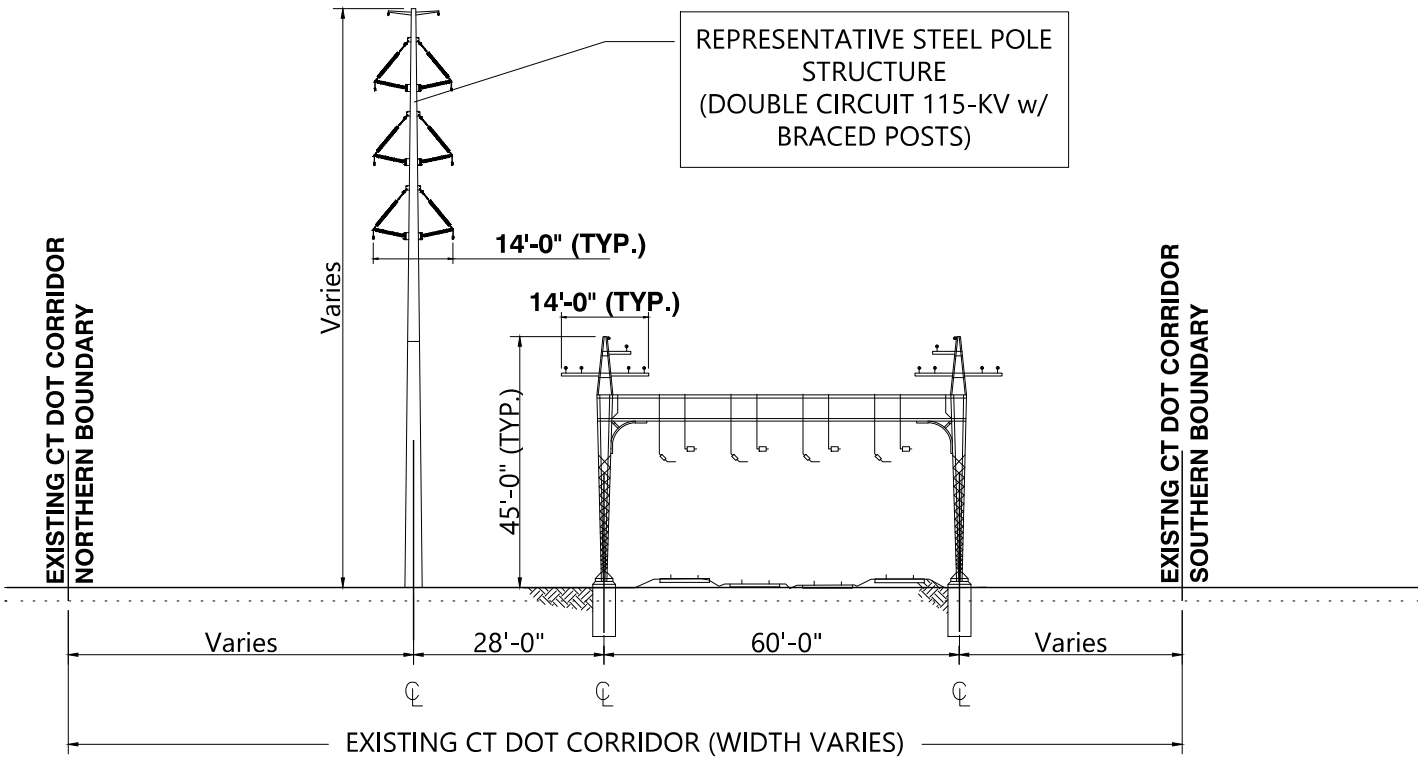
CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

- WETLAND M-W11
- WETLAND M-W12
- WETLAND M-W13
- WATERCOURSE M-WC7
- WATERCOURSE M-WC8
- MAJORITY OF SHEET WITHIN NDDB AREA. COORDINATE WITH ENVIRONMENTAL INSPECTOR/SPECIES PROTECTION SPECIALIST REGARDING WORK IN NDDB AREAS
- PORTION OF SHEET CONTAINS FEMA MAPPED 100-YEAR FLOODPLAIN
- OSPREY NEST SOUTH OF EXISTING CATENARY STRUCTURE B963A, SEE MAPSHEET 2.

ROAD CROSSINGS

- NONE



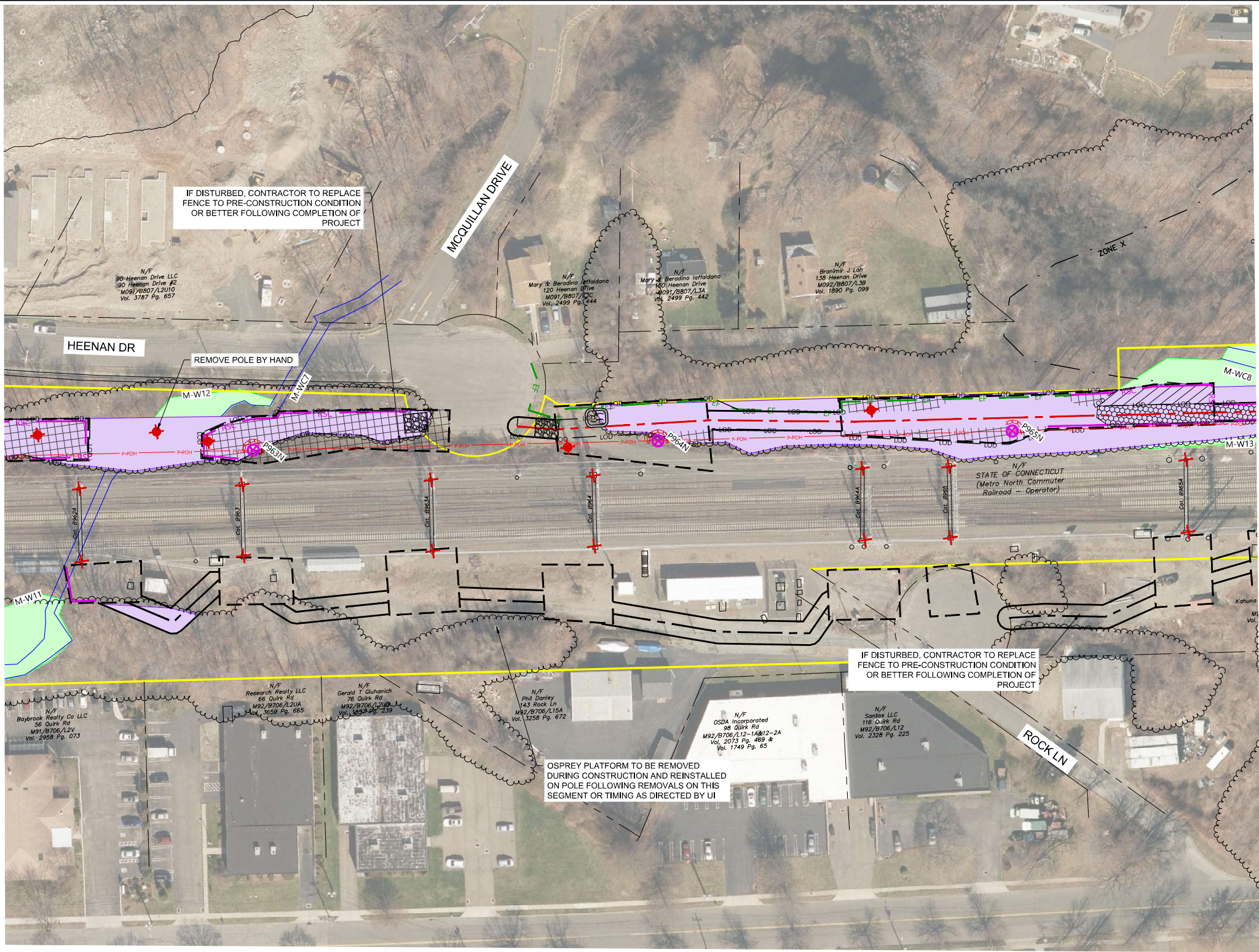
XS-7 (FACING EAST)

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P963N	TANGENT	XS-7	117	CONCRETE DRILLED PIER
P964N	TANGENT	XS-7	106	CONCRETE DRILLED PIER
P965N	TANGENT	XS-7	112	CONCRETE DRILLED PIER

STRUCTURE DETAIL TABLE



SEE SHEET 1



SEE SHEET 3

LEGEND

- PROPOSED TRANSMISSION POLE
- EXISTING TRANSMISSION POLE
- SUBSTATION TERMINATION STRUCTURE
- PROPOSED TRANSMISSION CENTER LINE
- NEW UI EASEMENT BOUNDARY
- PROPOSED TEMPORARY ACCESS ROAD
- PROPOSED WORK/PULLING PAD
- EXISTING CTDOT CORRIDOR BOUNDARY
- EXISTING PROPERTY LINE
- PROPOSED TEMPORARY MATTING / TEMPORARY GRAVEL WITH UI APPROVAL*
- TEMPORARY MATTED WETLAND IMPACT
- TEMPORARY MATTED STREAM IMPACT

- FIELD DELINEATED TIDAL WETLAND
- FIELD DELINEATED WETLAND
- FEMA 100-YEAR FLOODPLAIN LIMIT
- UI OWNED PROPERTY
- FIELD DELINEATED STREAM
- EXISTING TREE LINE
- PROPOSED TREE CLEARING
- FIELD DELINEATED TIDAL STREAM
- CT DEEP COASTAL ZONE
- PROPOSED PERMANENT ACCESS ROAD TO REMAIN
- PERMANENT WETLAND IMPACT
- OPEN ENDED WETLAND DELINEATION BOUNDARY
- OPEN ENDED WETLAND DELINEATION DIRECTION

- EXISTING HARDWARE ONLY TO BE REMOVED
- EXISTING STEEL POLE TO BE REMOVED
- EXISTING LATTICE TOWER TO BE REMOVED
- EXISTING BONNET TO BE REMOVED
- EXISTING WOOD POLE TO BE REMOVED
- EXISTING STEEL POLE TOP TO BE REMOVED AND CAPPED
- XX DRAINAGE CROSSING

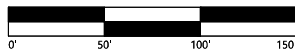
- CATCH BASIN INLET PROTECTION
- STRAW BALE SEDIMENT PROTECTION
- CONCRETE WASHOUT
- ROCK CONSTRUCTION ENTRANCE

- SEDIMENT BARRIER
- REDUNDANT SEDIMENT BARRIER - DOUBLE ROW
- EXCLUSIONARY FENCE
- LIMIT OF DISTURBANCE

**IN LOCATIONS WHERE MATTING HEIGHT IS A MINIMUM OF 20" ABOVE EXISTING GRADE WITHOUT ANY GAPS IN OUTER WALL FACE OF MATTING, THE MATTING IS AN ACCEPTABLE REPLACEMENT OF EXCLUSION FENCING SUBJECT TO APPROVAL OF UI AND/OR THEIR DESIGNATED REPRESENTATIVE.

* TEMPORARY MATTING IS THE PREFERRED METHOD. THE USE OF TEMPORARY GRAVEL WILL BE IMPLEMENTED ONLY ON AN AS-NEEDED BASIS WHERE SAFETY OR OTHER UNKNOWN FIELD CONDITIONS MAY EXIST, AND WILL REQUIRE PRIOR UI APPROVAL.

GRAVEL WILL NOT BE UTILIZED IN WETLAND, WATERCOURSE OR ANY OTHER UI IDENTIFIED SENSITIVE RESOURCE AREAS. ALL WATERCOURSE CROSSINGS MUST BE SPANNED AS TO NOT IMPEDE WATER FLOW OR FISH PASSAGE.



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UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN				UI AVANGRID		SCALE AS SHOWN FILE:		REV.
REV.	DATE	BY	DESCRIPTION	APP.	DATE:	NO.	MAPSHEET 2 OF 12	
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR.	SFB	CK.	JRD	0-0B
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW	APP.	MSP	DATE:	JUNE 2024	

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P966AN, P968N, P969N AND P970N WILL BE INSTALLED.
- BONNETS B966N/S, B966AN/S, B967N/S, B969N/S, B969AN/S, B970N/S AND B970AN/S WILL BE REMOVED.
- THREE EXISTING WOOD POLES NEAR P966AN, 3 WOOD POLES NEAR P969N TO BE REMOVED.
- EXISTING STEEL POLE P968AN TO BE REMOVED.
- EXISTING STEEL POLE TOP AT EXISTING STRUCTURE P968AS TO BE REMOVED AND CAPPED.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EXCLUSIONARY FENCING TO BE COORDINATED WITH UI IN ACCORDANCE WITH THE SPECIES PROTECTION PLAN.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.

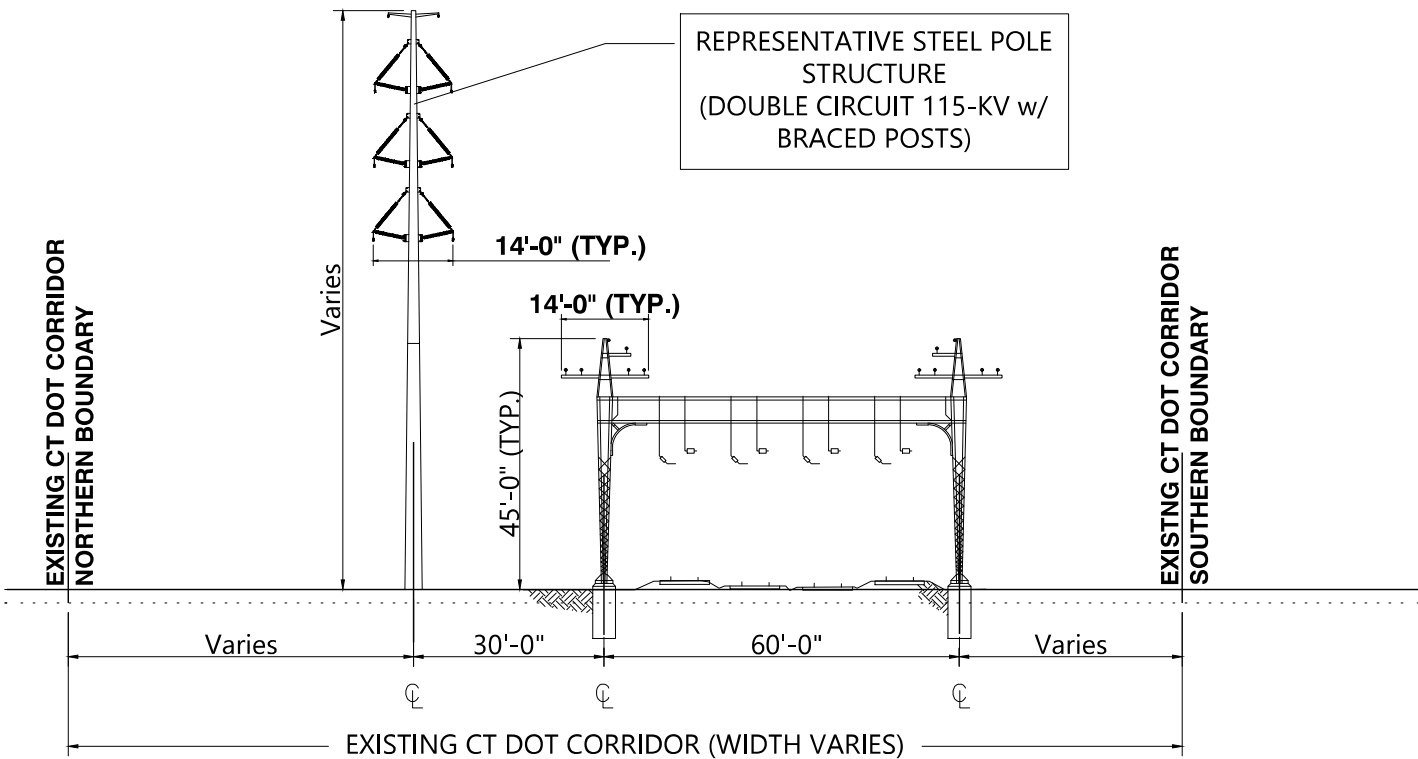
CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

- WETLAND M-W13
- WETLAND M-W14
- WETLAND M-W15
- WATERCOURSE M-WC8
- ENTIRETY OF SHEET WITHIN NDDB AREA. COORDINATE WITH ENVIRONMENTAL INSPECTOR/SPECIES PROTECTION SPECIALIST REGARDING WORK IN NDDB AREAS
- PORTION OF SHEET CONTAINS FEMA MAPPED 100-YEAR FLOODPLAIN

ROAD CROSSINGS

- NONE



XS-7 (FACING EAST)

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P966AN	DEADEND	XS-7	116.5	CONCRETE DRILLED PIER
P968N	TANGENT	XS-7	117	CONCRETE DRILLED PIER
P969N	TANGENT	XS-7	112	CONCRETE DRILLED PIER
P970N	TANGENT	XS-7	101	CONCRETE DRILLED PIER

STRUCTURE DETAIL TABLE

Drainage ID	Watercourse ID	Status	Crossing type/Culvert size	Closest Structure
10	M-WC8	Temporary	Culvert - size to channel dimensions	P968N
11	M-WC8	Temporary	Culvert - size to channel dimensions	P969N

DRAINAGE CROSSING TABLE

ANSI D CADD Drawing. DO NOT REVISE MANUALLY.

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Lansdale, PA 19446

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UI
AVANGRID

UI 115-kV RR PROJECT MILVON TO WEST RIVER
DEVELOPMENT & MANAGEMENT PLAN

MAPSHEET 3 STRUCTURES AND NOTES

0-0B

2024/05/15

WESTWOOD

SEGMENT 4 D&M PLAN FOR CSC SUBMISSION

0-0A

2024/04/19

WESTWOOD

SEGMENT 4 D&M PLAN FOR UI REVIEW

REV.

DATE

BY

DESCRIPTION

DR. SFB

CK. JRD

APP. MSP

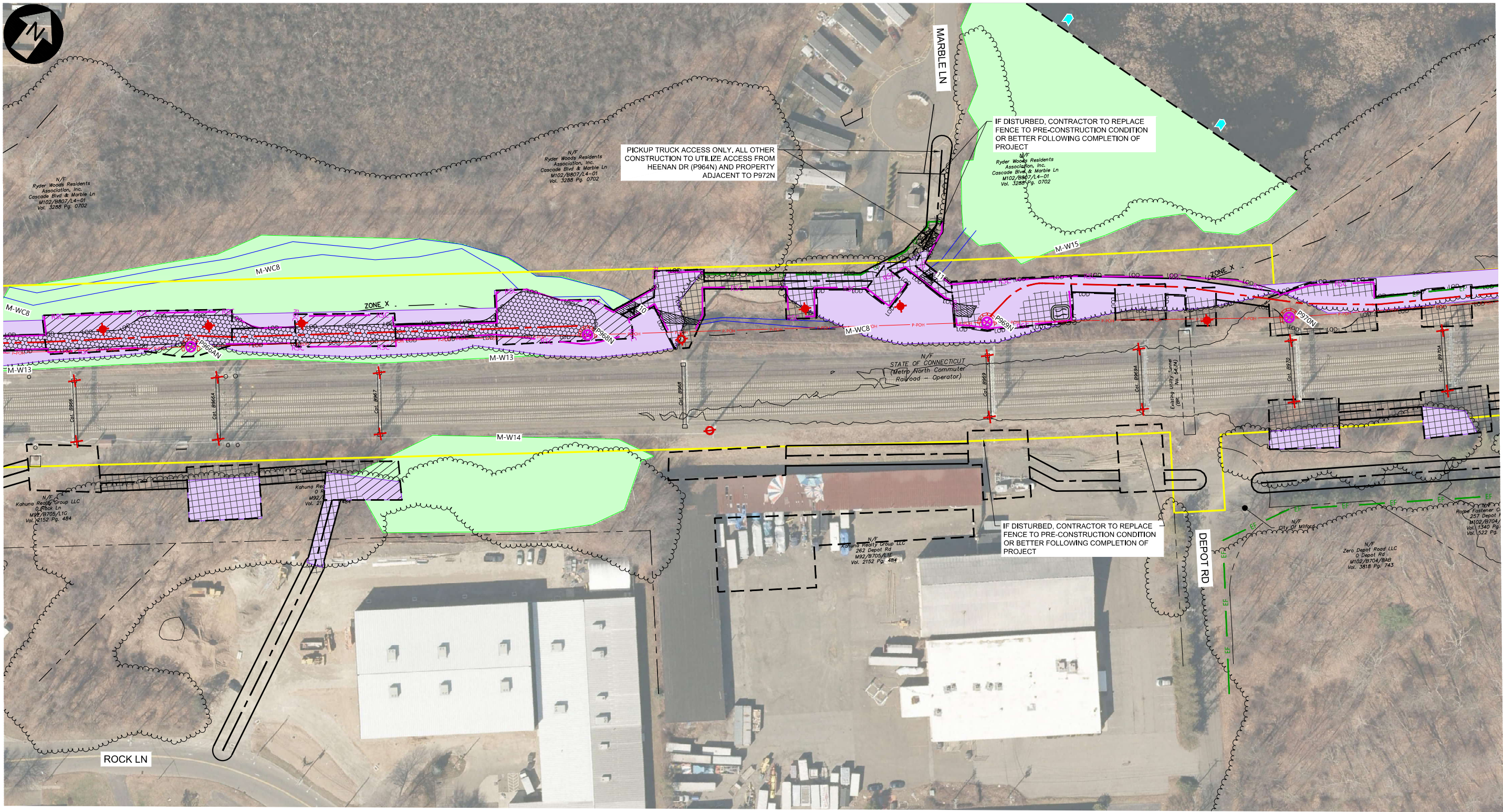
SCALE AS SHOWN

FILE:

NO.

DATE: JUNE 2024


REV. 0-0B




SEE SHEET 2

SEE SHEET 4


LEGEND

- 


PROPOSED TRANSMISSION POLE




EXISTING TRANSMISSION POLE




SUBSTATION TERMINATION STRUCTURE



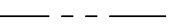
PROPOSED TRANSMISSION CENTER LINE




NEW UI EASEMENT BOUNDARY




PROPOSED TEMPORARY ACCESS ROAD




PROPOSED WORK/PULLING PAD



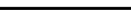
EXISTING CTDOT CORRIDOR BOUNDARY




EXISTING PROPERTY LINE




PROPOSED TEMPORARY MATTING / TEMPORARY GRAVEL WITH UI APPROVAL*



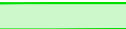
TEMPORARY MATTED WETLAND IMPACT




TEMPORARY MATTED STREAM IMPACT




FIELD DELINEATED TIDAL WETLAND



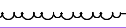
FIELD DELINEATED WETLAND




FEMA 100-YEAR FLOODPLAIN LIMIT




UI OWNED PROPERTY




FIELD DELINEATED STREAM



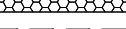
EXISTING TREE LINE




PROPOSED TREE CLEARING



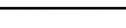
FIELD DELINEATED TIDAL STREAM




CT DEEP COASTAL ZONE



PROPOSED PERMANENT ACCESS ROAD TO REMAIN




PERMANENT WETLAND IMPACT




OPEN ENDED WETLAND DELINEATION BOUNDARY


OPEN ENDED WETLAND DELINEATION DIRECTION




EXISTING HARDWARE ONLY TO BE REMOVED




EXISTING STEEL POLE TO BE REMOVED




EXISTING LATTICE TOWER TO BE REMOVED




EXISTING BONNET TO BE REMOVED




EXISTING WOOD POLE TO BE REMOVED




EXISTING STEEL POLE TOP TO BE REMOVED AND CAPPED




XX DRAINAGE CROSSING




CATCH BASIN INLET PROTECTION




STRAW BALE SEDIMENT PROTECTION




CONCRETE WASHOUT




ROCK CONSTRUCTION ENTRANCE




SEDIMENT BARRIER



REDUNDANT SEDIMENT BARRIER - DOUBLE ROW



EXCLUSIONARY FENCE



LIMIT OF DISTURBANCE
- **IN LOCATIONS WHERE MATTING HEIGHT IS A MINIMUM OF 20" ABOVE EXISTING GRADE WITHOUT ANY GAPS IN OUTER WALL FACE OF MATTING, THE MATTING IS AN ACCEPTABLE REPLACEMENT OF EXCLUSION FENCING SUBJECT TO APPROVAL OF UI AND/OR THEIR DESIGNATED REPRESENTATIVE.
- * TEMPORARY MATTING IS THE PREFERRED METHOD. THE USE OF TEMPORARY GRAVEL WILL BE IMPLEMENTED ONLY ON AN AS-NEEDED BASIS WHERE SAFETY OR OTHER UNKNOWN FIELD CONDITIONS MAY EXIST, AND WILL REQUIRE PRIOR UI APPROVAL.
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- | UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN | | | | | UI AVANGRID | | SCALE AS SHOWN FILE: | | REV. |
|---|------------|----------|---------------------------------------|----------|-------------|-----------|----------------------|------------------|------|
| REV. | DATE | BY | DESCRIPTION | APP. | DATE: | JUNE 2024 | NO. | MAPSHEET 3 OF 12 | |
| 0-0B | 2024/05/15 | WESTWOOD | SEGMENT 4 D&M PLAN FOR CSC SUBMISSION | DR. SFB | CK. JRD | | | | |
| 0-0A | 2024/04/19 | WESTWOOD | SEGMENT 4 D&M PLAN FOR UI REVIEW | APP. MSP | | | | | |

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P971N, P972N, P973NN, AND P974N WILL BE INSTALLED.
- BONNETS B971N/S, B971AN/S, B972N/S, B972AS, B973S, B974N/S, AND B975N/S WILL BE REMOVED.
- EXISTING STEEL POLE P973AN WILL REMAIN; HARDWARE AND INSULATORS WILL BE REPLACED.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EXCLUSIONARY FENCING TO BE COORDINATED WITH UI IN ACCORDANCE WITH THE SPECIES PROTECTION PLAN.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.

CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

- WETLAND M-W16
- WETLAND M-W17
- WATERCOURSE M-WC9
- WATERCOURSE M-WC10
- MAJORITY OF SHEET WITHIN NDDB AREA. COORDINATE WITH ENVIRONMENTAL INSPECTOR/SPECIES PROTECTION SPECIALIST REGARDING WORK IN NDDB AREAS
- PORTION OF SHEET WITHIN CT DEEP COASTAL ZONE

ROAD CROSSINGS

- NONE

Drainage ID	Watercourse ID	Status	Crossing type/Culvert size	Closest Structure
12	M-WC9	Temporary	Mat bridge	P973NN/AN

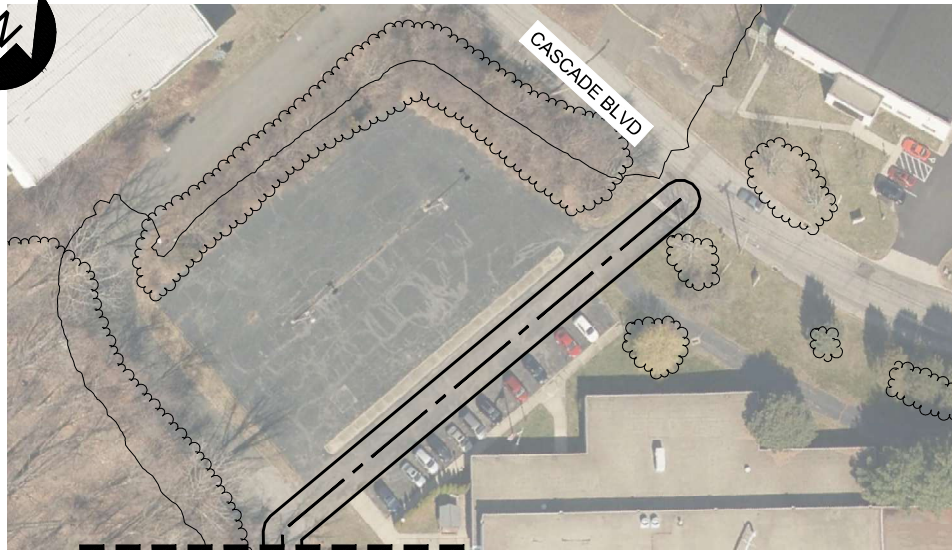
DRAINAGE CROSSING TABLE

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P971N	TANGENT	XS-7	106	CONCRETE DRILLED PIER
P972N	TANGENT	XS-8	101	CONCRETE DRILLED PIER
P973NN	TANGENT	XS-8	96	CONCRETE DRILLED PIER
P974N	TANGENT	XS-8	106	CONCRETE DRILLED PIER

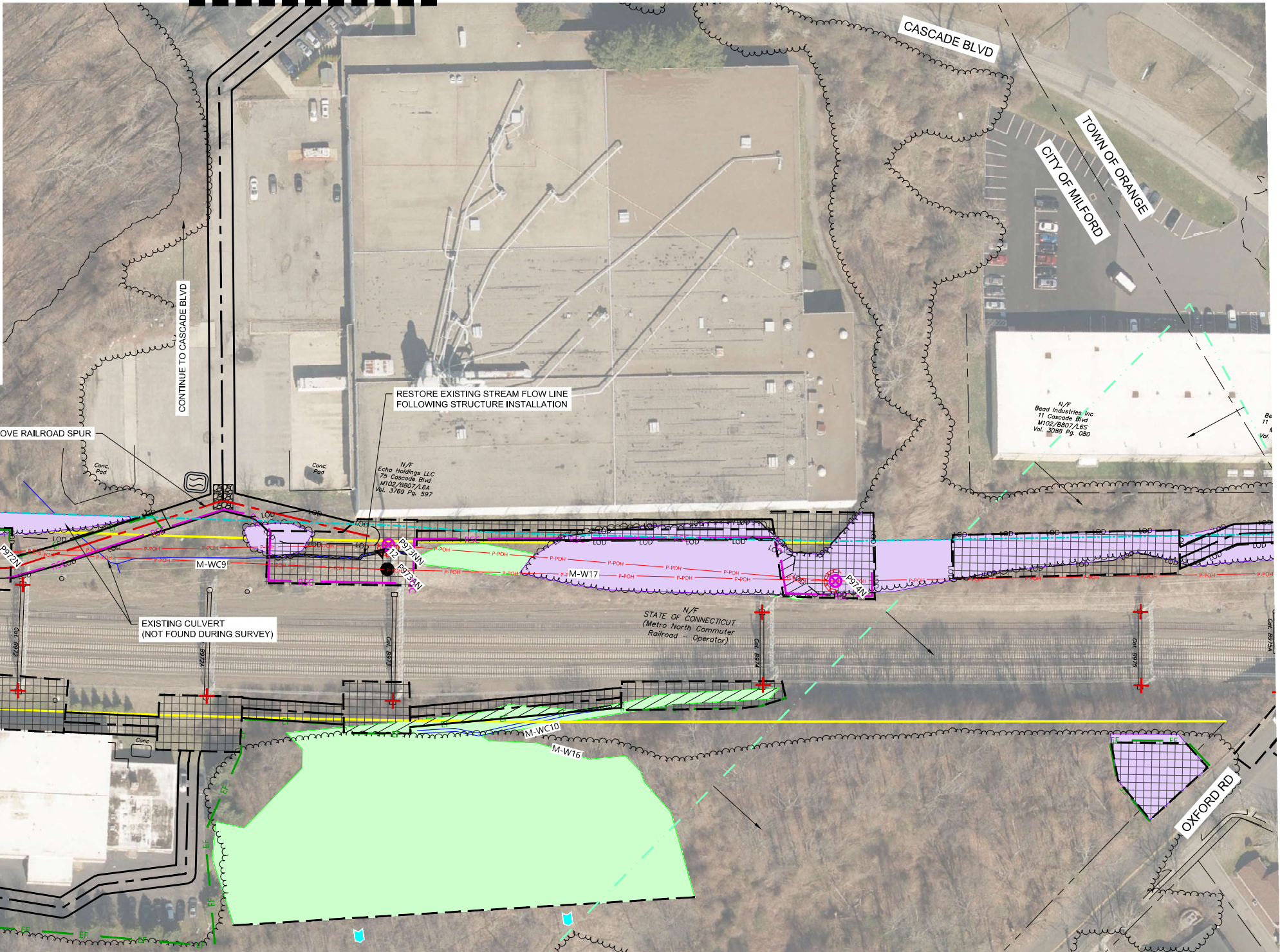
STRUCTURE DETAIL TABLE



MATCHLINE



MATCHLINE



SEE SHEET 5

SEE SHEET 3

LEGEND

- | | | | | | |
|--|---|--|--|--|--|
| | PROPOSED TRANSMISSION POLE | | FIELD DELINEATED TIDAL WETLAND | | EXISTING HARDWARE ONLY TO BE REMOVED |
| | EXISTING TRANSMISSION POLE | | FIELD DELINEATED WETLAND | | EXISTING STEEL POLE TO BE REMOVED |
| | SUBSTATION TERMINATION STRUCTURE | | FEMA 100-YEAR FLOODPLAIN LIMIT | | EXISTING LATTICE TOWER TO BE REMOVED |
| | PROPOSED TRANSMISSION CENTER LINE | | UI OWNED PROPERTY | | EXISTING BONNET TO BE REMOVED |
| | NEW UI EASEMENT BOUNDARY | | FIELD DELINEATED STREAM | | EXISTING WOOD POLE TO BE REMOVED |
| | PROPOSED TEMPORARY ACCESS ROAD | | EXISTING TREE LINE | | EXISTING STEEL POLE TOP TO BE REMOVED AND CAPPED |
| | PROPOSED WORK/PULLING PAD | | PROPOSED TREE CLEARING | | DRAINAGE CROSSING |
| | EXISTING CTDOT CORRIDOR BOUNDARY | | CT DEEP COASTAL ZONE | | |
| | EXISTING PROPERTY LINE | | PROPOSED PERMANENT ACCESS ROAD TO REMAIN | | |
| | PROPOSED TEMPORARY MATTING / TEMPORARY GRAVEL WITH UI APPROVAL* | | PERMANENT WETLAND IMPACT | | |
| | TEMPORARY MATTED WETLAND IMPACT | | OPEN ENDED WETLAND DELINEATION BOUNDARY | | |
| | TEMPORARY MATTED STREAM IMPACT | | OPEN ENDED WETLAND DELINEATION DIRECTION | | |

- | | |
|--|--------------------------------|
| | CATCH BASIN INLET PROTECTION |
| | STRAW BALE SEDIMENT PROTECTION |
| | CONCRETE WASHOUT |
| | ROCK CONSTRUCTION ENTRANCE |

- | | |
|--|---|
| | SEDIMENT BARRIER |
| | REDUNDANT SEDIMENT BARRIER - DOUBLE ROW |
| | EXCLUSIONARY FENCE |
| | LIMIT OF DISTURBANCE |

**IN LOCATIONS WHERE MATTING HEIGHT IS A MINIMUM OF 20" ABOVE EXISTING GRADE WITHOUT ANY GAPS IN OUTER WALL FACE OF MATTING, THE MATTING IS AN ACCEPTABLE REPLACEMENT OF EXCLUSION FENCING SUBJECT TO APPROVAL OF UI AND/OR THEIR DESIGNATED REPRESENTATIVE.

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GRAVEL WILL NOT BE UTILIZED IN WETLAND, WATERCOURSE OR ANY OTHER UI IDENTIFIED SENSITIVE RESOURCE AREAS. ALL WATERCOURSE CROSSINGS MUST BE SPANNED AS TO NOT IMPEDE WATER FLOW OR FISH PASSAGE.

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UI AVANGRID				UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN			
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR.	SFB	SCALE	AS SHOWN
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW	CK.	JRD	NO.	
REV.	DATE	BY	DESCRIPTION	APP.	MSP	DATE:	JUNE 2024
				MAPSHEET 4 OF 12			
				REV. 0-0B			

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P975EN, P977N, P978N, P979N AND P980N WILL BE INSTALLED.
- BONNETS B975EN/S, B976WN/S, B976N/S, B977N/S, B978N/S, B979N/S AND B980N/S WILL BE REMOVED.
- BONNETS B975EN/S AND B976WN/S NEAR MARSH HILL ROAD SHALL BE ACCESSED AND REMOVED UTILIZING THE ROAD.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EXCLUSIONARY FENCING TO BE COORDINATED WITH UI IN ACCORDANCE WITH THE SPECIES PROTECTION PLAN.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.
- GUARD STRUCTURE WORK SPACES TO BE UTILIZED ON AN AS-NEEDED BASIS FOR WIRE PULLING ACTIVITIES. SCHEDULE AND WORK AREA PROTECTION TO BE PROVIDED BY CONTRACTOR IN ACCORDANCE WITH MUTCD GUIDELINES AND APPROPRIATELY COORDINATED WITH MUNICIPALITY AND CT DOT.

CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

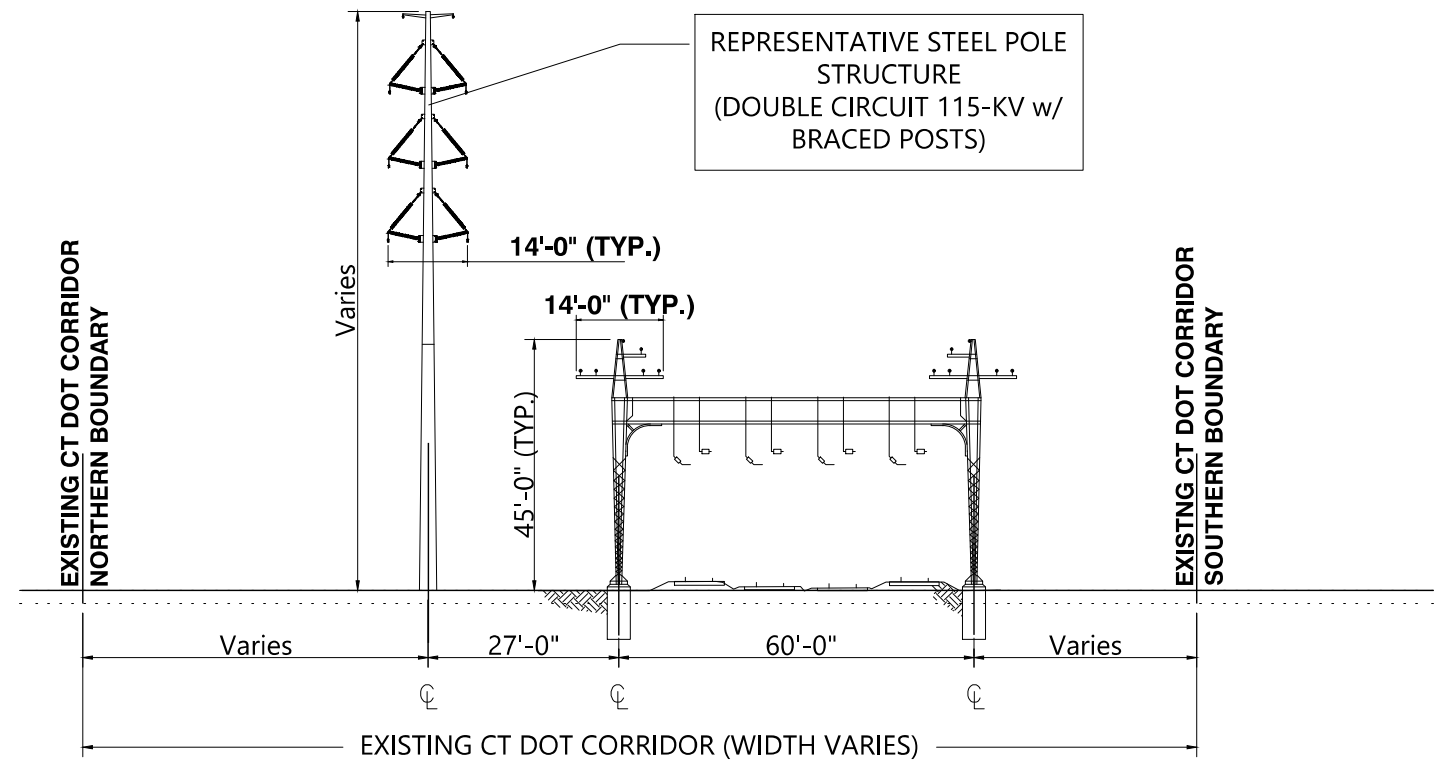
- WATERCOURSE O-WC1
- MAJORITY OF SHEET WITHIN NDDB AREA. COORDINATE WITH ENVIRONMENTAL INSPECTOR/SPECIES PROTECTION SPECIALIST REGARDING WORK IN NDDB AREAS
- PORTION OF SHEET WITHIN CT DEEP COASTAL ZONE

ROAD CROSSINGS

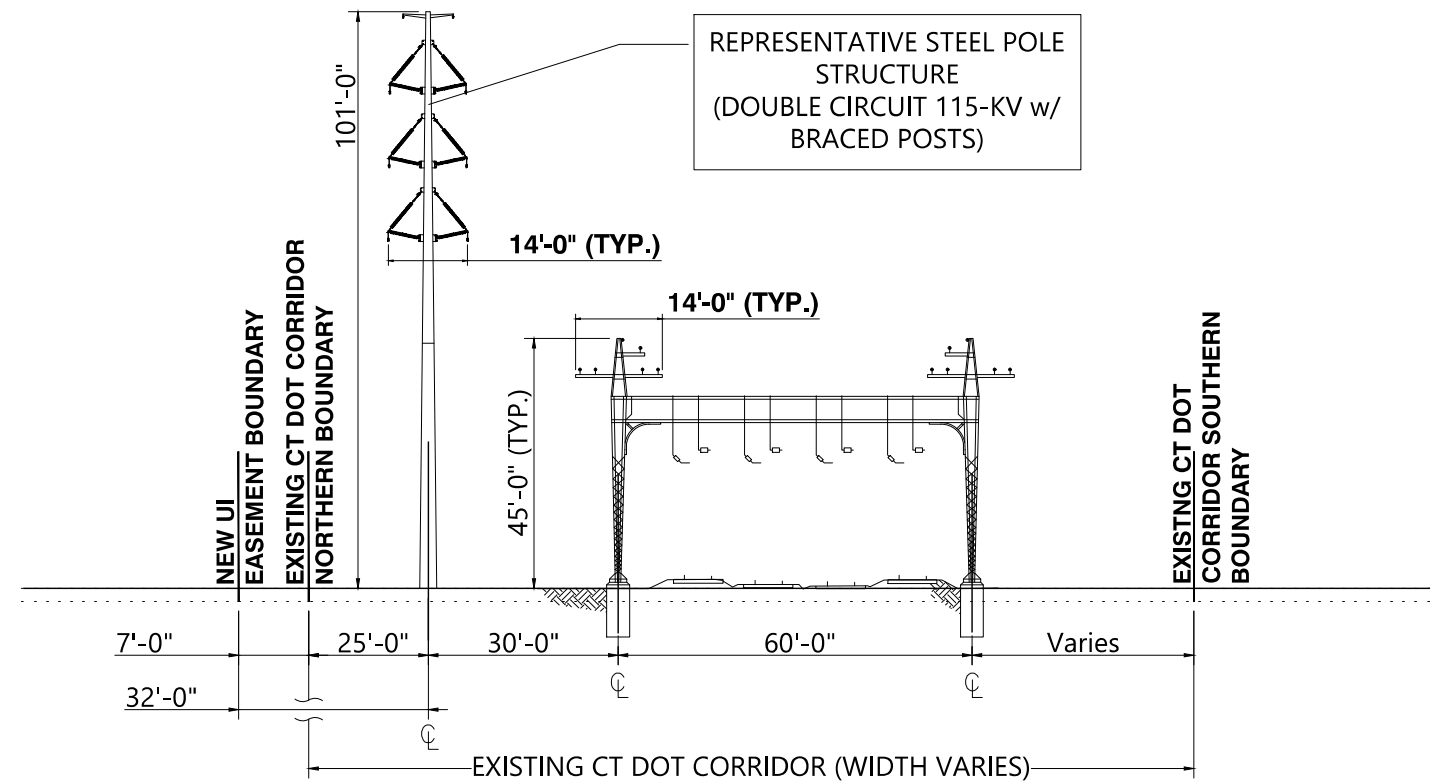
- MARSH HILL RD

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P975EN	TANGENT	XS-8	101	CONCRETE DRILLED PIER
P977N	TANGENT	XS-7	112	CONCRETE DRILLED PIER
P978N	TANGENT	XS-7	91	CONCRETE DRILLED PIER
P979N	TANGENT	XS-7	81	CONCRETE DRILLED PIER
P980N	TANGENT	XS-7	81	CONCRETE DRILLED PIER

STRUCTURE DETAIL TABLE



XS-7 (FACING EAST)



XS-8 (FACING EAST)

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UI
AVANGRID

0-0B2024/05/15WESTWOODSEGMENT 4 D&M PLAN FOR CSC SUBMISSION

0-0A2024/04/19WESTWOODSEGMENT 4 D&M PLAN FOR UI REVIEW

REV. DATE BY DESCRIPTION

DR. SFB
CK. JRD
APP. MSP

SCALE AS SHOWN
NO.
DATE: JUNE 2024

REV. 0-0B

UI 115-kV RR PROJECT MILVON TO WEST RIVER
DEVELOPMENT & MANAGEMENT PLAN

MAPSHEET 5 STRUCTURES AND NOTES

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P981N, P982N AND P983N WILL BE INSTALLED.
- BONNETS B981N/S, B982N/S, B983N/S AND B984N/S WILL BE REMOVED.
- THREE EXISTING WOOD POLES AROUND P983N TO BE REMOVED
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EXCLUSIONARY FENCING TO BE COORDINATED WITH UI IN ACCORDANCE WITH THE SPECIES PROTECTION PLAN
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- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.

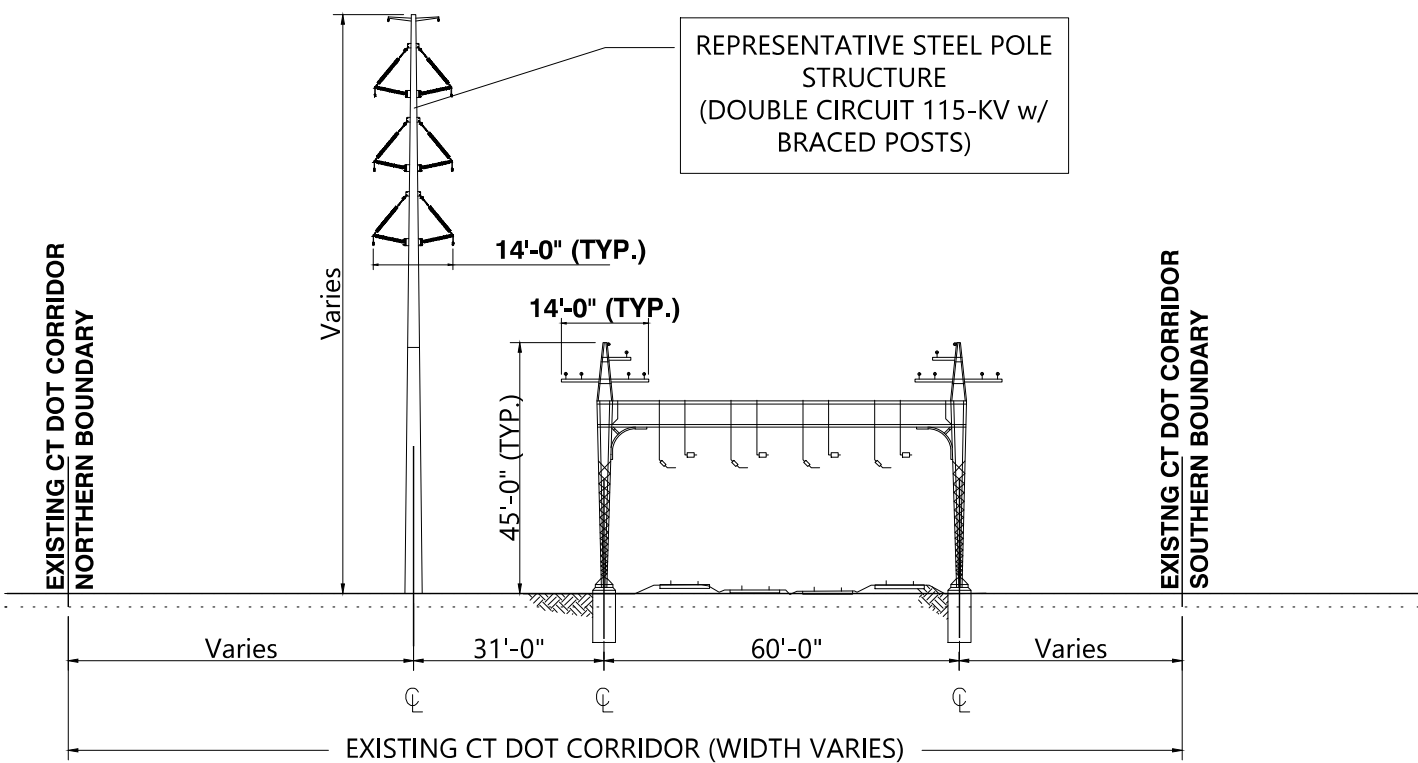
CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

- WETLAND O-W1
- WETLAND O-W2
- WATERCOURSE O-WC2
- WATERCOURSE O-WC3
- WATERCOURSE WH-WC1
- PORTION OF SHEET WITHIN NDDB AREA. COORDINATE WITH ENVIRONMENTAL INSPECTOR/SPECIES PROTECTION SPECIALIST REGARDING WORK IN NDDB AREAS
- PORTION OF SHEET CONTAINS FEMA MAPPED 100-YEAR FLOODPLAIN

ROAD CROSSINGS

- NONE



XS-7 (FACING EAST)

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P981N	TANGENT	XS-7	86	CONCRETE DRILLED PIER
P982N	TANGENT	XS-7	96	CONCRETE DRILLED PIER
P983N	TANGENT	XS-7	116.5	CONCRETE DRILLED PIER

STRUCTURE DETAIL TABLE

Drainage ID	Watercourse ID	Status	Crossing type/Culvert size	Closest Structure
13	O-WC2	Permanent	1-54"; 2-42"; 3-36	P982N
14	O-WC2	Temporary	Culvert - size to channel dimensions	P982N
15	O-WC2	Permanent	1-54"; 2-42"; 3-36"	B983N

DRAINAGE CROSSING TABLE

CADD Drawing. DO NOT REVISE MANUALLY.

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UI 115-kV RR PROJECT MILVON TO WEST RIVER
DEVELOPMENT & MANAGEMENT PLAN

MAPSHEET 6 STRUCTURES AND NOTES

DR.	SFB	SCALE	AS SHOWN	FILE:			
CK.	JRD	NO.					
APP.	MSP						
REV.	DATE	BY	DESCRIPTION	APP.	DATE	JUNE 2024	

REV.

0-0B

ANSI D CADD Drawing. DO NOT REVISE MANUALLY.

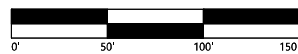
LEGEND

- PROPOSED TRANSMISSION POLE
- EXISTING TRANSMISSION POLE
- SUBSTATION TERMINATION STRUCTURE
- PROPOSED TRANSMISSION CENTER LINE
- NEW UI EASEMENT BOUNDARY
- PROPOSED TEMPORARY ACCESS ROAD
- PROPOSED WORK/PULLING PAD
- EXISTING CTDOT CORRIDOR BOUNDARY
- EXISTING PROPERTY LINE
- PROPOSED TEMPORARY MATTING / TEMPORARY GRAVEL WITH UI APPROVAL*
- TEMPORARY MATTED WETLAND IMPACT
- TEMPORARY MATTED STREAM IMPACT

- FIELD DELINEATED TIDAL WETLAND
- FIELD DELINEATED WETLAND
- FEMA 100-YEAR FLOODPLAIN LIMIT
- UI OWNED PROPERTY
- FIELD DELINEATED STREAM
- EXISTING TREE LINE
- PROPOSED TREE CLEARING
- FIELD DELINEATED TIDAL STREAM
- CT DEEP COASTAL ZONE
- PROPOSED PERMANENT ACCESS ROAD TO REMAIN
- PERMANENT WETLAND IMPACT
- OPEN ENDED WETLAND DELINEATION BOUNDARY
- OPEN ENDED WETLAND DELINEATION DIRECTION

- EXISTING HARDWARE ONLY TO BE REMOVED
- EXISTING STEEL POLE TO BE REMOVED
- EXISTING LATTICE TOWER TO BE REMOVED
- EXISTING BONNET TO BE REMOVED
- EXISTING WOOD POLE TO BE REMOVED
- EXISTING STEEL POLE TOP TO BE REMOVED AND CAPPED
- XX DRAINAGE CROSSING

- CATCH BASIN INLET PROTECTION
- STRAW BALE SEDIMENT PROTECTION
- CONCRETE WASHOUT
- ROCK CONSTRUCTION ENTRANCE



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- SF SEDIMENT BARRIER
- RSC REDUNDANT SEDIMENT BARRIER - DOUBLE ROW
- EF EXCLUSIONARY FENCE
- LOD LIMIT OF DISTURBANCE

**IN LOCATIONS WHERE MATTING HEIGHT IS A MINIMUM OF 20" ABOVE EXISTING GRADE WITHOUT ANY GAPS IN OUTER WALL FACE OF MATTING, THE MATTING IS AN ACCEPTABLE REPLACEMENT OF EXCLUSION FENCING SUBJECT TO APPROVAL OF UI AND/OR THEIR DESIGNATED REPRESENTATIVE.

* TEMPORARY MATTING IS THE PREFERRED METHOD. THE USE OF TEMPORARY GRAVEL WILL BE IMPLEMENTED ONLY ON AN AS-NEEDED BASIS WHERE SAFETY OR OTHER UNKNOWN FIELD CONDITIONS MAY EXIST, AND WILL REQUIRE PRIOR UI APPROVAL.

GRAVEL WILL NOT BE UTILIZED IN WETLAND, WATERCOURSE OR ANY OTHER UI IDENTIFIED SENSITIVE RESOURCE AREAS. ALL WATERCOURSE CROSSINGS MUST BE SPANNED AS TO NOT IMPEDE WATER FLOW OR FISH PASSAGE.

							UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN				
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION		DR.	SFB	SCALE AS SHOWN		FILE:		
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW		CK.	JRD	NO.				REV.
REV.	DATE	BY	DESCRIPTION		APP.	MSP	MAPSHEET 6 OF 12				0-0B
						DATE: JUNE 2024					

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P985N, P986N, P987NN, P988N AND P989N WILL BE INSTALLED.
- BONNETS B985N/S, B986N/S, B987S, B988N/S AND B989N/S WILL BE REMOVED.
- EXISTING STEEL POLE P987AN WILL REMAIN; HARDWARE AND INSULATORS WILL BE REPLACED.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EXCLUSIONARY FENCING TO BE COORDINATED WITH UI IN ACCORDANCE WITH THE SPECIES PROTECTION PLAN.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.

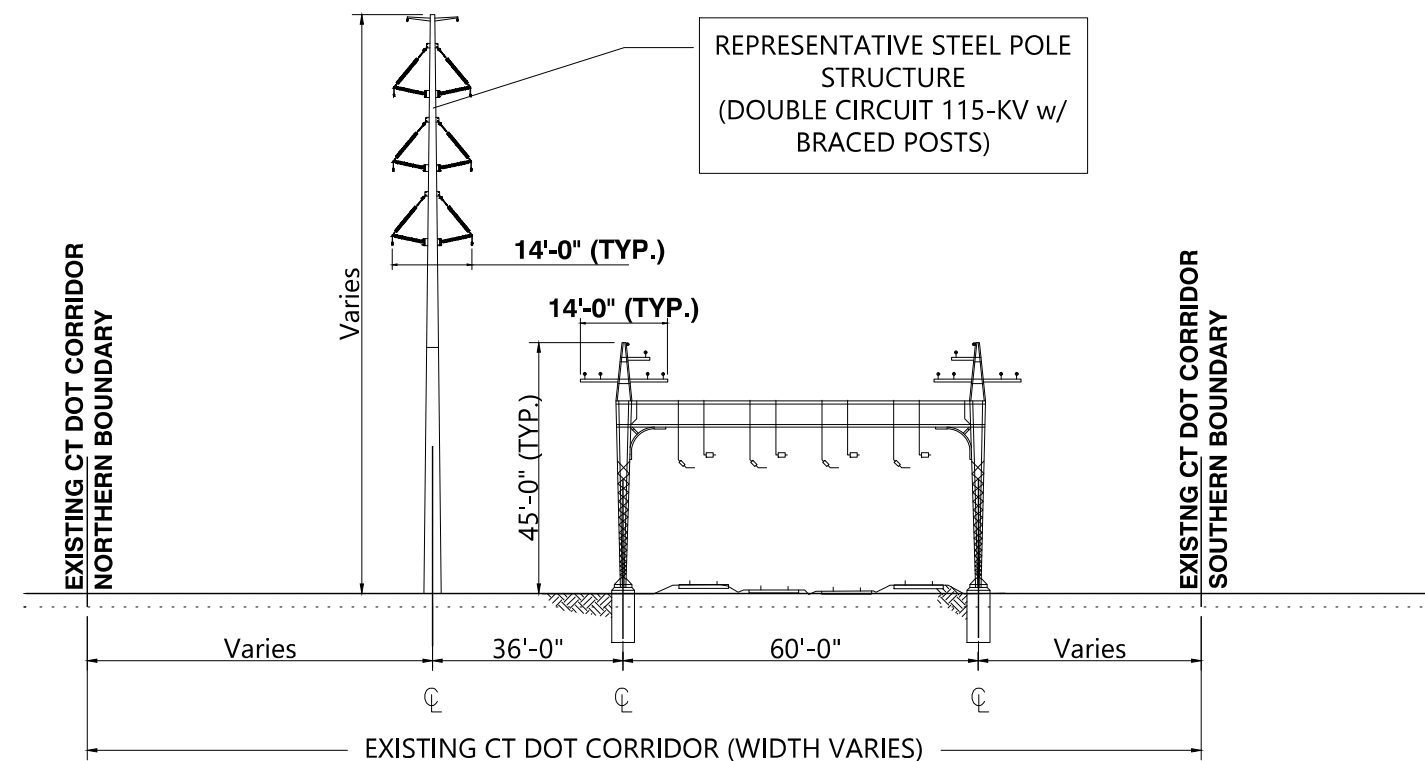
CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

- WETLAND WH-W1
- WATERCOURSE WH-WC2
- WATERCOURSE WH-WC3
- WATERCOURSE WH-WC4
- NO NDDDB AREA WITHIN SHEET

ROAD CROSSINGS

- NONE



XS-7 (FACING EAST)

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P985N	TANGENT	XS-7	106	CONCRETE DRILLED PIER
P986N	TANGENT	XS-7	91	CONCRETE DRILLED PIER
P987NN	TANGENT	XS-7	95	CONCRETE DRILLED PIER
P988N	TANGENT	XS-7	96	CONCRETE DRILLED PIER
P989N	TANGENT	XS-7	91	CONCRETE DRILLED PIER

STRUCTURE DETAIL TABLE

Drainage ID	Watercourse ID	Status	Crossing type/Culvert size	Closest Structure
16	WH-WC3	Temporary	Mat bridge	P987NN/AN

DRAINAGE CROSSING TABLE

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P990N, P991N, P992N AND P993N WILL BE INSTALLED.
- BONNETS B990N/S, B991N/S, B992N/S AND B993N/S WILL BE REMOVED.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.

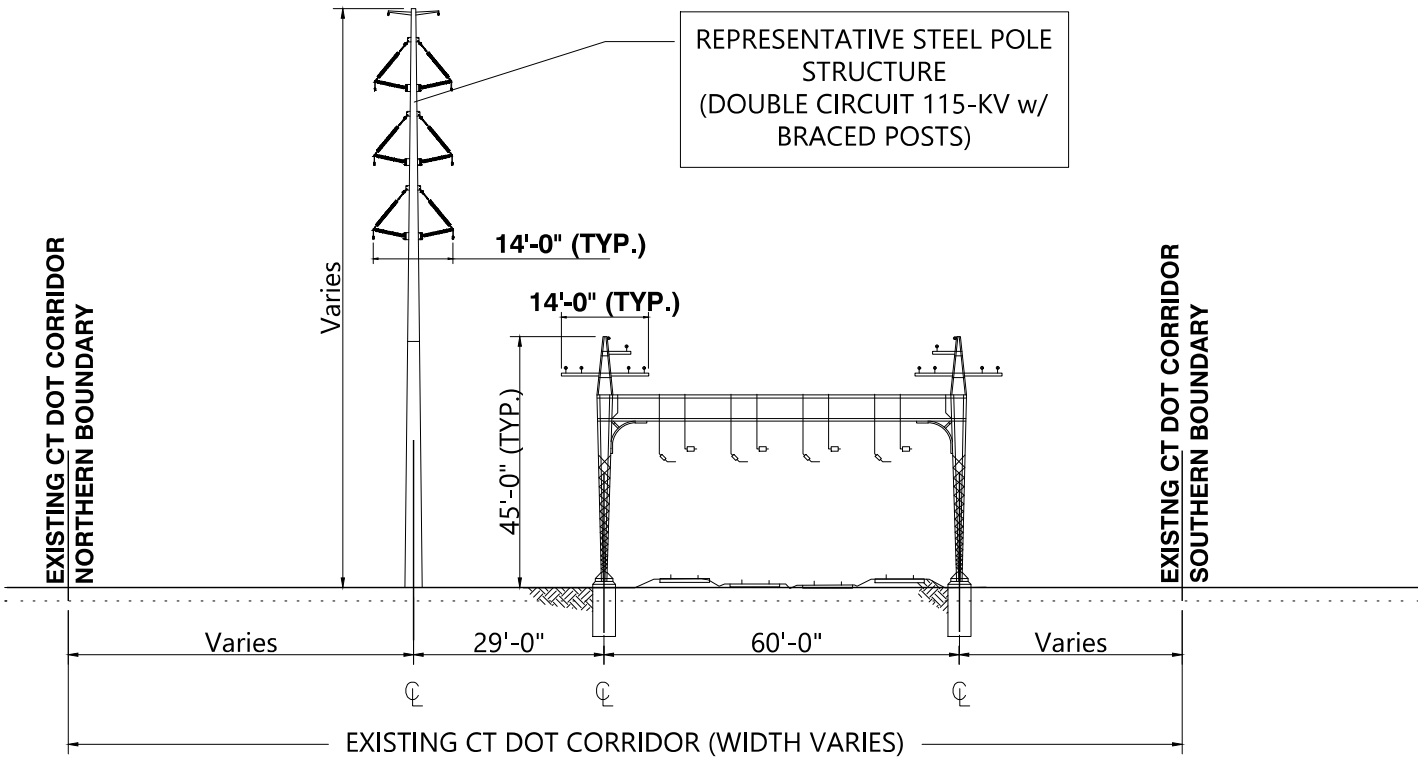
CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

- WETLAND WH-W1
- NO NDDB AREA WITHIN SHEET
- PORTION OF SHEET CONTAINS FEMA MAPPED 100-YEAR FLOODPLAIN

ROAD CROSSINGS

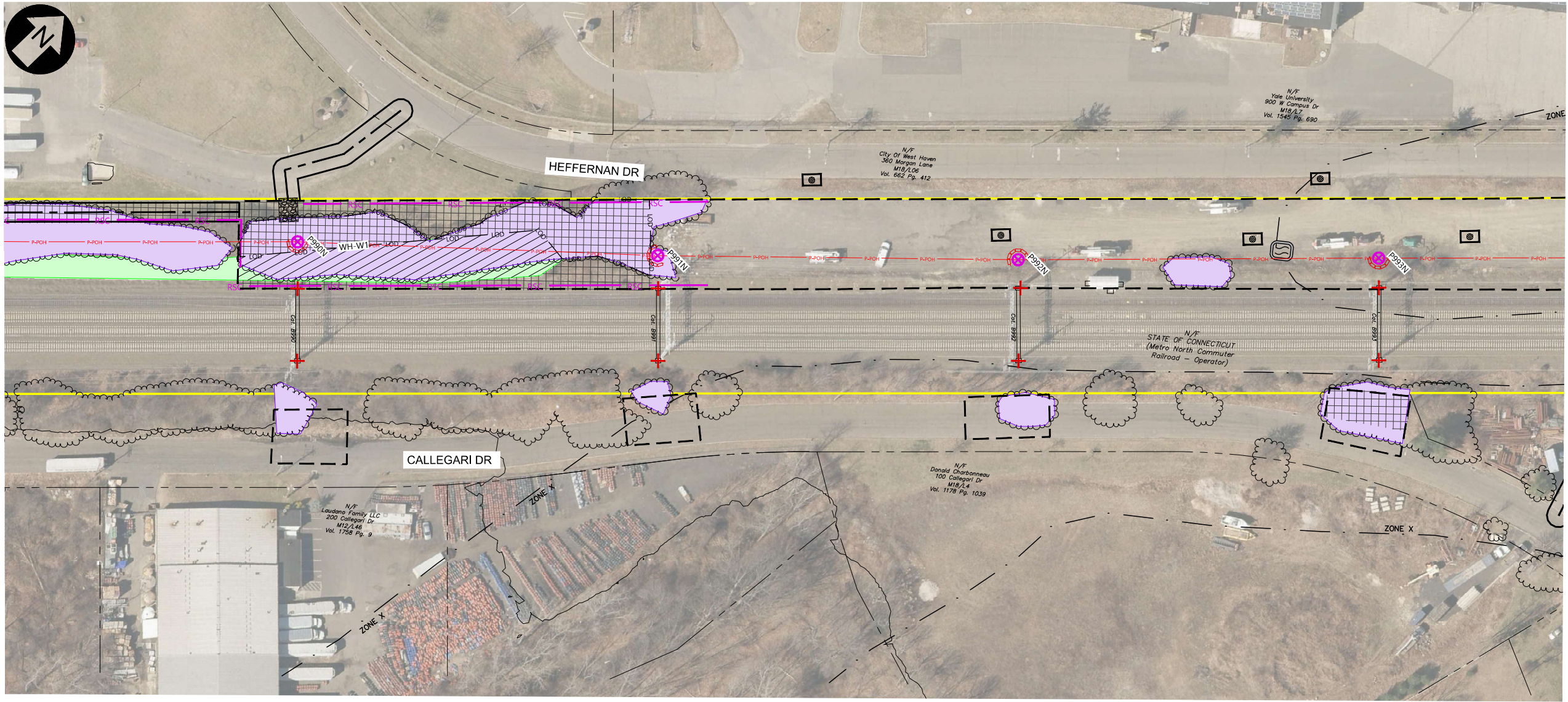
- NONE



XS-7 (FACING EAST)

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P990N	TANGENT	XS-7	86	CONCRETE DRILLED PIER
P991N	DEADEND	XS-7	91	CONCRETE DRILLED PIER
P992N	TANGENT	XS-7	96.5	CONCRETE DRILLED PIER
P993N	TANGENT	XS-7	106	CONCRETE DRILLED PIER

STRUCTURE DETAIL TABLE



SEE SHEET 7

SEE SHEET 9

LEGEND

PROPOSED TRANSMISSION POLE

EXISTING TRANSMISSION POLE

SUBSTATION TERMINATION STRUCTURE

PROPOSED TRANSMISSION CENTER LINE

NEW UI EASEMENT BOUNDARY

PROPOSED TEMPORARY ACCESS ROAD

PROPOSED WORK/PULLING PAD

EXISTING CTDOT CORRIDOR BOUNDARY

EXISTING PROPERTY LINE

PROPOSED TEMPORARY MATTING / TEMPORARY GRAVEL WITH UI APPROVAL*

TEMPORARY MATTED WETLAND IMPACT

TEMPORARY MATTED STREAM IMPACT

FIELD DELINEATED TIDAL WETLAND

FIELD DELINEATED WETLAND

FEMA 100-YEAR FLOODPLAIN LIMIT

UI OWNED PROPERTY

FIELD DELINEATED STREAM

EXISTING TREE LINE

PROPOSED TREE CLEARING

FIELD DELINEATED TIDAL STREAM

CT DEEP COASTAL ZONE

PROPOSED PERMANENT ACCESS ROAD TO REMAIN

PERMANENT WETLAND IMPACT

OPEN ENDED WETLAND DELINEATION BOUNDARY

OPEN ENDED WETLAND DELINEATION DIRECTION

EXISTING HARDWARE ONLY TO BE REMOVED

EXISTING STEEL POLE TO BE REMOVED

EXISTING LATTICE TOWER TO BE REMOVED

EXISTING BONNET TO BE REMOVED

EXISTING WOOD POLE TO BE REMOVED

EXISTING STEEL POLE TOP TO BE REMOVED AND CAPPED

DRAINAGE CROSSING

CATCH BASIN INLET PROTECTION

STRAW BALE SEDIMENT PROTECTION

CONCRETE WASHOUT

ROCK CONSTRUCTION ENTRANCE

SEDIMENT BARRIER

REDUNDANT SEDIMENT BARRIER - DOUBLE ROW

EXCLUSIONARY FENCE

LIMIT OF DISTURBANCE

**IN LOCATIONS WHERE MATTING HEIGHT IS A MINIMUM OF 20" ABOVE EXISTING GRADE WITHOUT ANY GAPS IN OUTER WALL FACE OF MATTING, THE MATTING IS AN ACCEPTABLE REPLACEMENT OF EXCLUSION FENCING SUBJECT TO APPROVAL OF UI AND/OR THEIR DESIGNATED REPRESENTATIVE.

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GRAVEL WILL NOT BE UTILIZED IN WETLAND, WATERCOURSE OR ANY OTHER UI IDENTIFIED SENSITIVE RESOURCE AREAS. ALL WATERCOURSE CROSSINGS MUST BE SPANNED AS TO NOT IMPEDE WATER FLOW OR FISH PASSAGE.

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0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR.	SFB	SCALE AS SHOWN	FILE:
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW	CK.	JRD	NO.	
REV.	DATE	BY	DESCRIPTION	APP.	MSP	DATE:	JUNE 2024

UI
AVANGRID

UI 115-kV RR PROJECT MILVON TO WEST RIVER
DEVELOPMENT & MANAGEMENT PLAN

MAPSHEET 8 OF 12

REV. 0-0B

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P994N, P996N, P997N AND P998N WILL BE INSTALLED.
- BONNETS B994N/S, B995N/S, B996N/S, B997N/S AND B998N/S WILL BE REMOVED.
- EXISTING WOOD POLES FROM P997N TO P998N WILL BE REMOVED.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.
- GUARD STRUCTURE WORK SPACES TO BE UTILIZED ON AN AS-NEEDED BASIS FOR WIRE PULLING ACTIVITIES. SCHEDULE AND WORK AREA PROTECTION TO BE PROVIDED BY CONTRACTOR IN ACCORDANCE WITH MUTCD GUIDELINES AND APPROPRIATELY COORDINATED WITH MUNICIPALITY AND CT DOT.

CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

- WETLAND WH-W2
- WATERCOURSE WH-WC5
- WATERCOURSE WH-WC6
- WATERCOURSE WH-WC7
- NO NDDB AREA WITHIN SHEET
- PORTION OF SHEET CONTAINS FEMA MAPPED 100-YEAR FLOODPLAIN

ROAD CROSSINGS

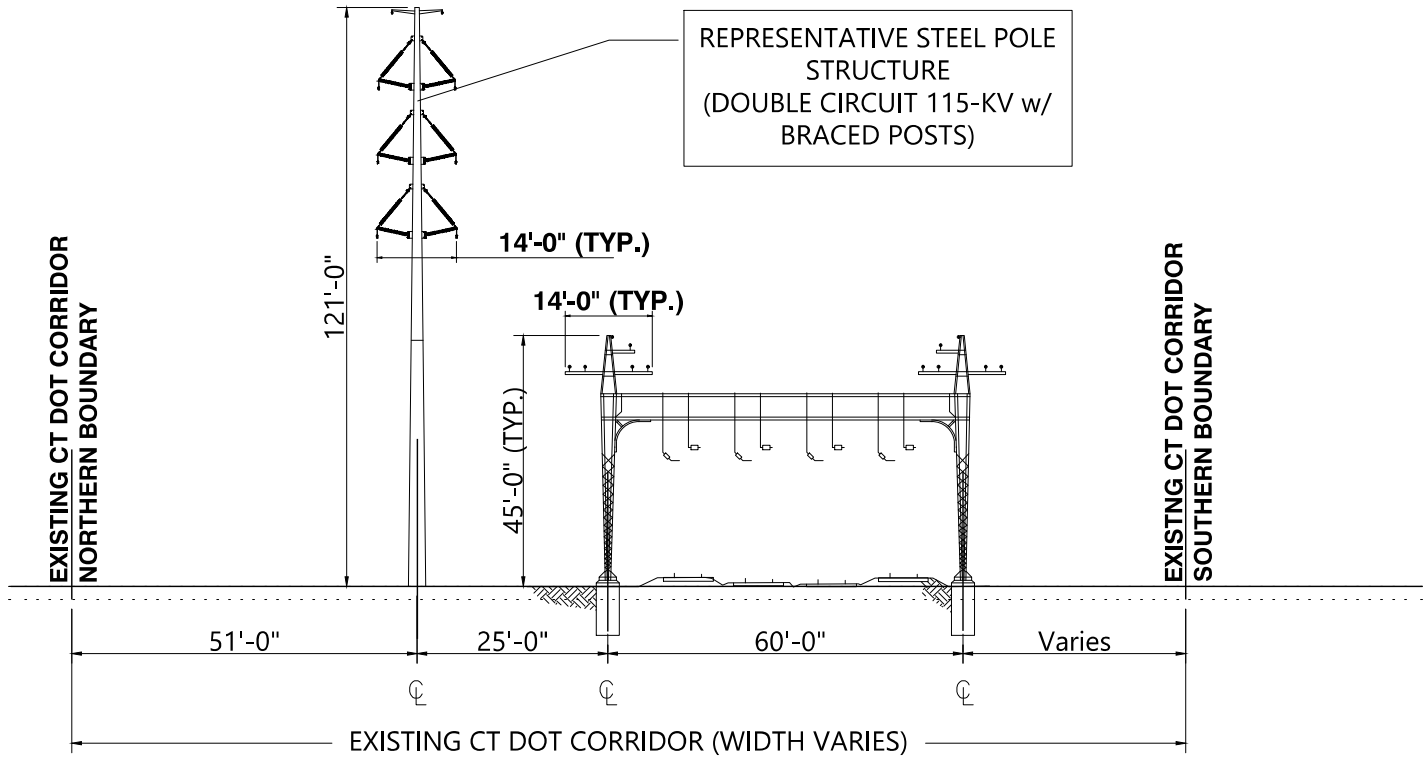
- MORGAN LN

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P994N	TANGENT	XS-7	121	CONCRETE DRILLED PIER
P996N	TANGENT	XS-8	111	CONCRETE DRILLED PIER
P997N	TANGENT	XS-8	101	CONCRETE DRILLED PIER
P998N	TANGENT	XS-8	91	CONCRETE DRILLED PIER

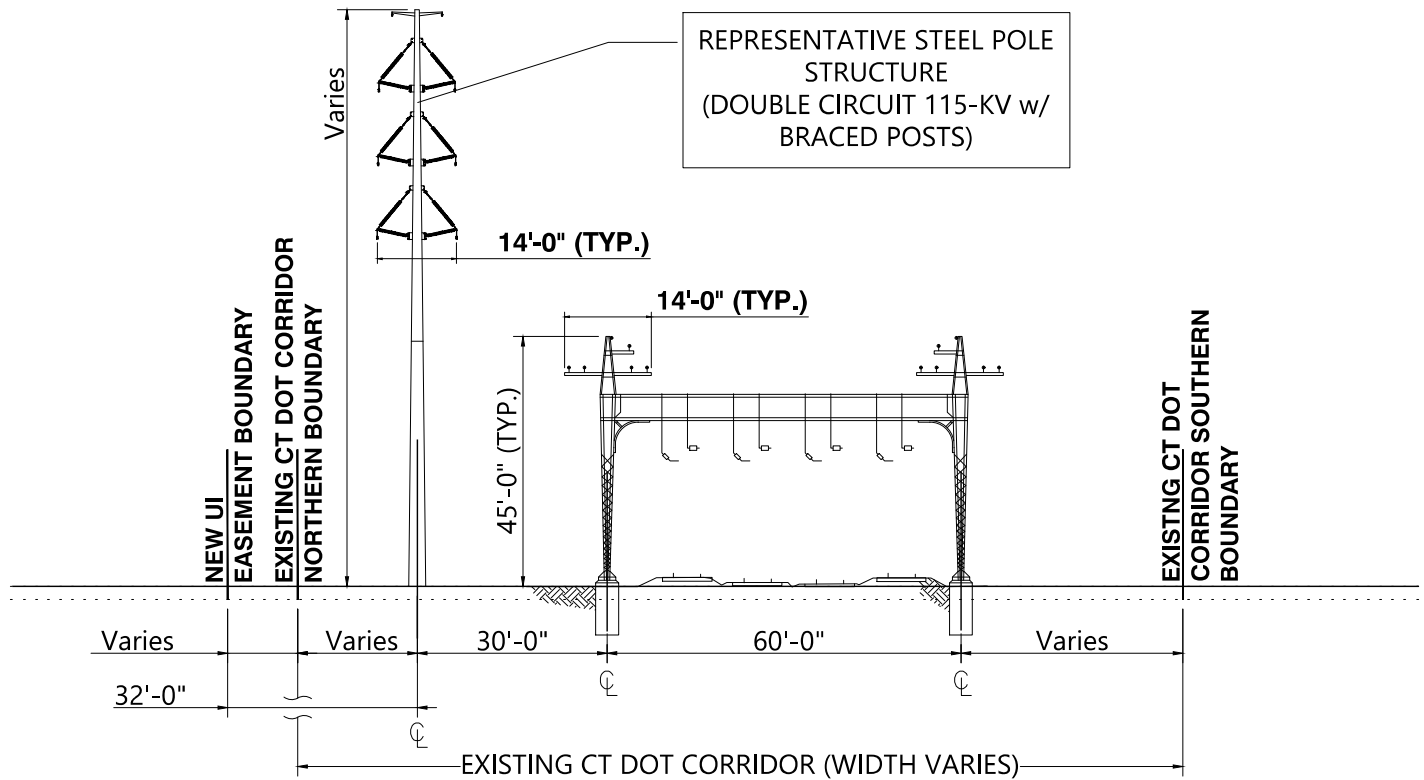
STRUCTURE DETAIL TABLE

Drainage ID	Watercourse ID	Status	Crossing type/Culvert size	Closest Structure
17	WH-WC6	Temporary	Culvert - size to channel dimensions	P997N

DRAINAGE CROSSING TABLE

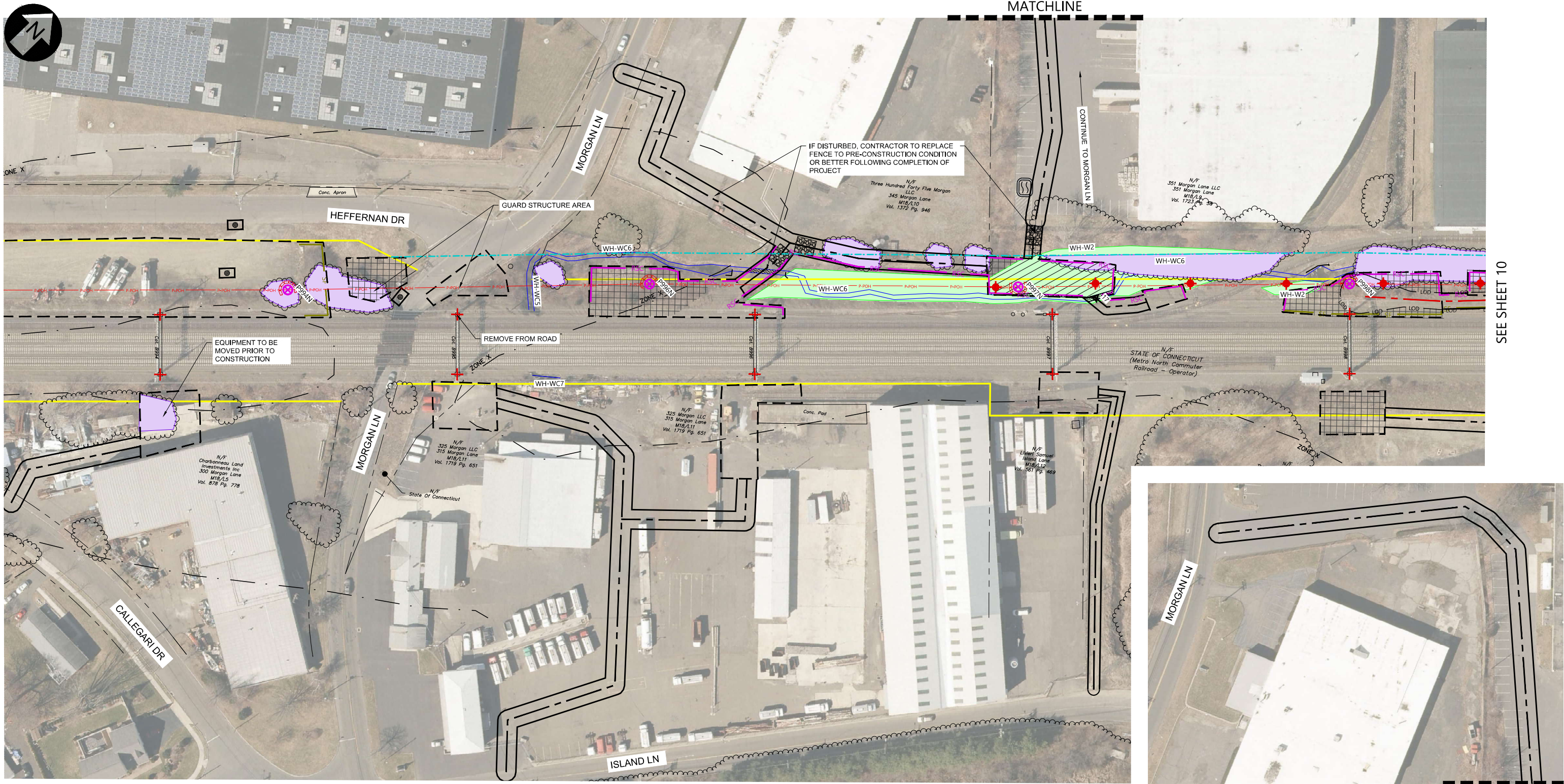


XS-7 (FACING EAST)



XS-8 (FACING EAST)

CADD Drawing. DO NOT REVISE MANUALLY.



LEGEND

PROPOSED TRANSMISSION POLE

EXISTING TRANSMISSION POLE

SUBSTATION TERMINATION STRUCTURE

PROPOSED TRANSMISSION CENTER LINE

NEW UI EASEMENT BOUNDARY

PROPOSED TEMPORARY ACCESS ROAD

PROPOSED WORK/PULLING PAD

EXISTING CTDOT CORRIDOR BOUNDARY

EXISTING PROPERTY LINE

PROPOSED TEMPORARY MATTING / TEMPORARY GRAVEL WITH UI APPROVAL*

TEMPORARY MATTED WETLAND IMPACT

TEMPORARY MATTED STREAM IMPACT

FIELD DELINEATED TIDAL WETLAND

FIELD DELINEATED WETLAND

FEMA 100-YEAR FLOODPLAIN LIMIT

UI OWNED PROPERTY

FIELD DELINEATED STREAM

EXISTING TREE LINE

PROPOSED TREE CLEARING

FIELD DELINEATED TIDAL STREAM

CT DEEP COASTAL ZONE

PROPOSED PERMANENT ACCESS ROAD TO REMAIN

PERMANENT WETLAND IMPACT

OPEN ENDED WETLAND DELINEATION BOUNDARY

OPEN ENDED WETLAND DELINEATION DIRECTION

EXISTING HARDWARE ONLY TO BE REMOVED

EXISTING STEEL POLE TO BE REMOVED

EXISTING LATTICE TOWER TO BE REMOVED

EXISTING BONNET TO BE REMOVED

EXISTING WOOD POLE TO BE REMOVED

EXISTING STEEL POLE TOP TO BE REMOVED AND CAPPED

XX

DRAINAGE CROSSING

CATCH BASIN INLET PROTECTION

STRAW BALE SEDIMENT PROTECTION

CONCRETE WASHOUT

ROCK CONSTRUCTION ENTRANCE

SF

RSC

EF

LOD

LOD

SEDIMENT BARRIER

REDUNDANT SEDIMENT BARRIER - DOUBLE ROW

EXCLUSIONARY FENCE

LIMIT OF DISTURBANCE

* IN LOCATIONS WHERE MATTING HEIGHT IS A MINIMUM OF 20" ABOVE EXISTING GRADE WITHOUT ANY GAPS IN OUTER WALL FACE OF MATTING, THE MATTING IS AN ACCEPTABLE REPLACEMENT OF EXCLUSION FENCING SUBJECT TO APPROVAL OF UI AND/OR THEIR DESIGNATED REPRESENTATIVE.

0' 50' 100' 150'

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REV.	DATE	BY	DESCRIPTION	APP.	DATE
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR. SFB	
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW	CK. JRD	

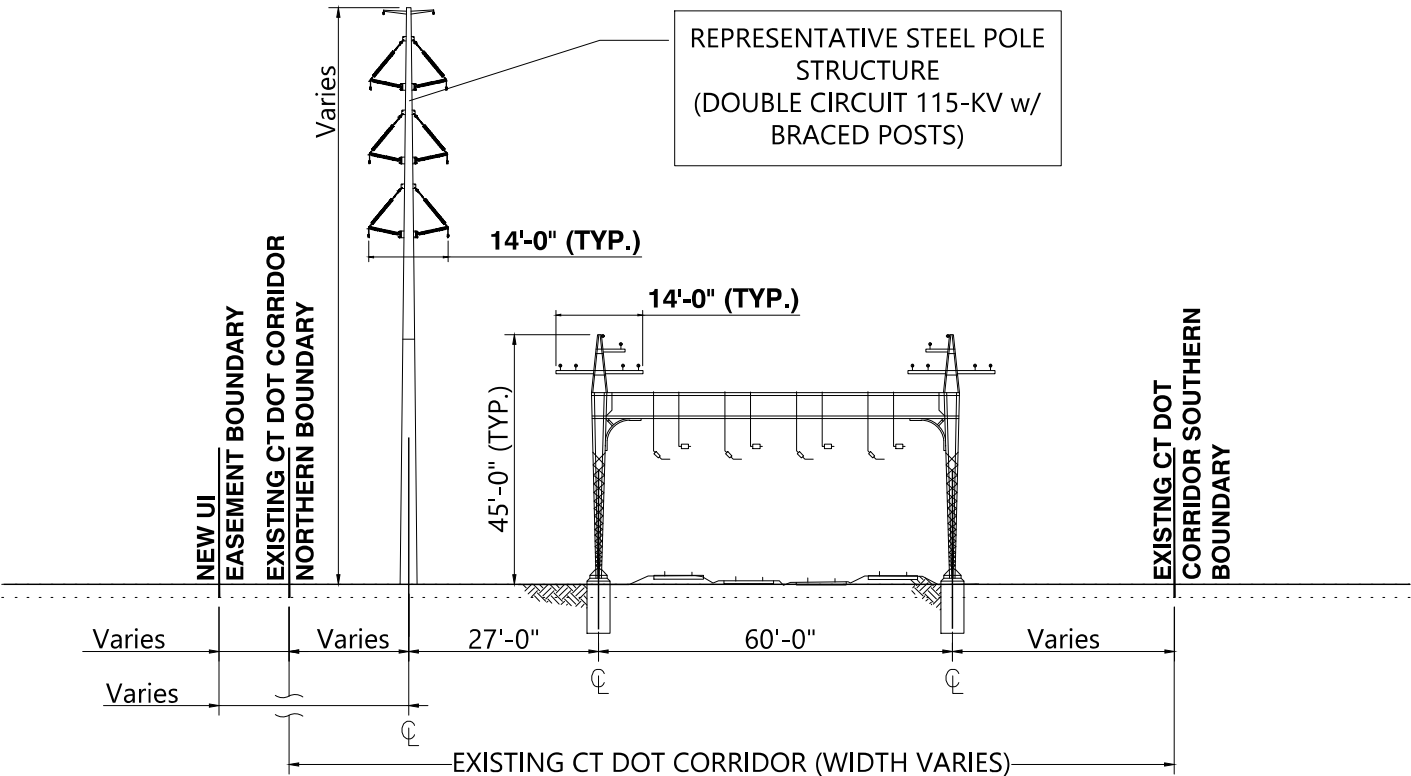
APP.	DATE	SCALE	AS SHOWN	FILE:
MSP	JUNE 2024	NO.		

UI
AVANGRID

UI 115-kV RR PROJECT MILVON TO WEST RIVER
DEVELOPMENT & MANAGEMENT PLAN

MAPSHEET 9 OF 12

REV. 0-0B



XS-8 (FACING EAST)

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P999N	TANGENT	XS-8	91	CONCRETE DRILLED PIER
P1000N	TANGENT	XS-8	91	CONCRETE DRILLED PIER
P1001N	TANGENT	XS-8	91	CONCRETE DRILLED PIER
P1002N	TANGENT	XS-8	91	CONCRETE DRILLED PIER

STRUCTURE DETAIL TABLE

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P999N, P1000N, P1001N AND P1002N WILL BE INSTALLED.
- BONNETS B999N/S, B1000N/S, B1001N/S AND B1002N/S WILL BE REMOVED.
- EXISTING WOOD POLES ON THE NORTH SIDE OF THE RAILROAD TRACKS TO BE REMOVED.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.

CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

- WETLAND WH-W3
- WATERCOURSE WH-WC6
- NO NDDDB AREA WITHIN SHEET
- PORTION OF SHEET CONTAINS FEMA MAPPED 100-YEAR FLOODPLAIN

ROAD CROSSINGS

- NONE


ANSI D CADD Drawing. DO NOT REVISE MANUALLY.

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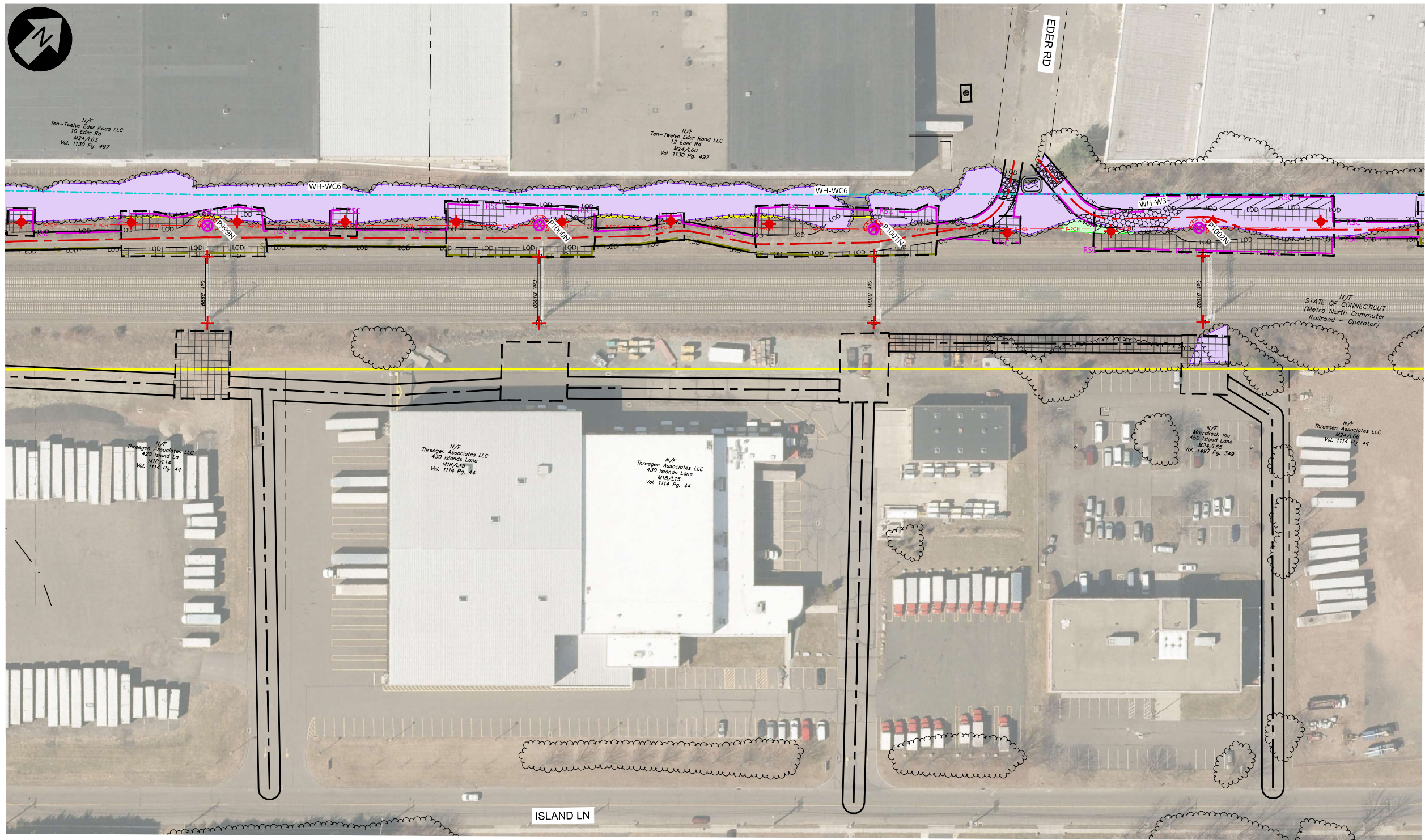


UI 115-kV RR PROJECT MILVON TO WEST RIVER
DEVELOPMENT & MANAGEMENT PLAN

MAPSHEET 10 STRUCTURES AND NOTES

0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR.	SFB	SCALE AS SHOWN	FILE:
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW	CK	JRD	NO.	
REV.	DATE	BY	DESCRIPTION	APP.	MSP		


REV. 0-0B




SEE SHEET 9

SEE SHEET 11


LEGEND

- 


PROPOSED TRANSMISSION POLE




EXISTING TRANSMISSION POLE




SUBSTATION TERMINATION STRUCTURE



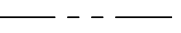
PROPOSED TRANSMISSION CENTER LINE




NEW UI EASEMENT BOUNDARY



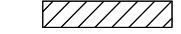
PROPOSED TEMPORARY ACCESS ROAD




PROPOSED WORK/PULLING PAD



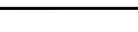
EXISTING CTDOT CORRIDOR BOUNDARY




EXISTING PROPERTY LINE




PROPOSED TEMPORARY MATTING / TEMPORARY GRAVEL WITH UI APPROVAL*




TEMPORARY MATTED WETLAND IMPACT




TEMPORARY MATTED STREAM IMPACT
- 


FIELD DELINEATED TIDAL WETLAND



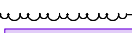
FIELD DELINEATED WETLAND



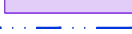
FEMA 100-YEAR FLOODPLAIN LIMIT




UI OWNED PROPERTY




FIELD DELINEATED STREAM




EXISTING TREE LINE




PROPOSED TREE CLEARING



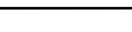
FIELD DELINEATED TIDAL STREAM




CT DEEP COASTAL ZONE




PROPOSED PERMANENT ACCESS ROAD TO REMAIN




PERMANENT WETLAND IMPACT




OPEN ENDED WETLAND DELINEATION BOUNDARY

OPEN ENDED WETLAND DELINEATION DIRECTION
- 


EXISTING HARDWARE ONLY TO BE REMOVED




EXISTING STEEL POLE TO BE REMOVED




EXISTING LATTICE TOWER TO BE REMOVED




EXISTING BONNET TO BE REMOVED




EXISTING WOOD POLE TO BE REMOVED




EXISTING STEEL POLE TOP TO BE REMOVED AND CAPPED




XX
DRAINAGE CROSSING
- 


CATCH BASIN INLET PROTECTION




STRAW BALE SEDIMENT PROTECTION




CONCRETE WASHOUT

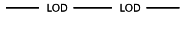


ROCK CONSTRUCTION ENTRANCE
- 

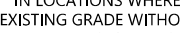
SEDIMENT BARRIER



REDUNDANT SEDIMENT BARRIER - DOUBLE ROW



EXCLUSIONARY FENCE



LOD
LIMIT OF DISTURBANCE

**IN LOCATIONS WHERE MATTING HEIGHT IS A MINIMUM OF 20" ABOVE EXISTING GRADE WITHOUT ANY GAPS IN OUTER WALL FACE OF MATTING, THE MATTING IS AN ACCEPTABLE REPLACEMENT OF EXCLUSION FENCING SUBJECT TO APPROVAL OF UI AND/OR THEIR DESIGNATED REPRESENTATIVE.

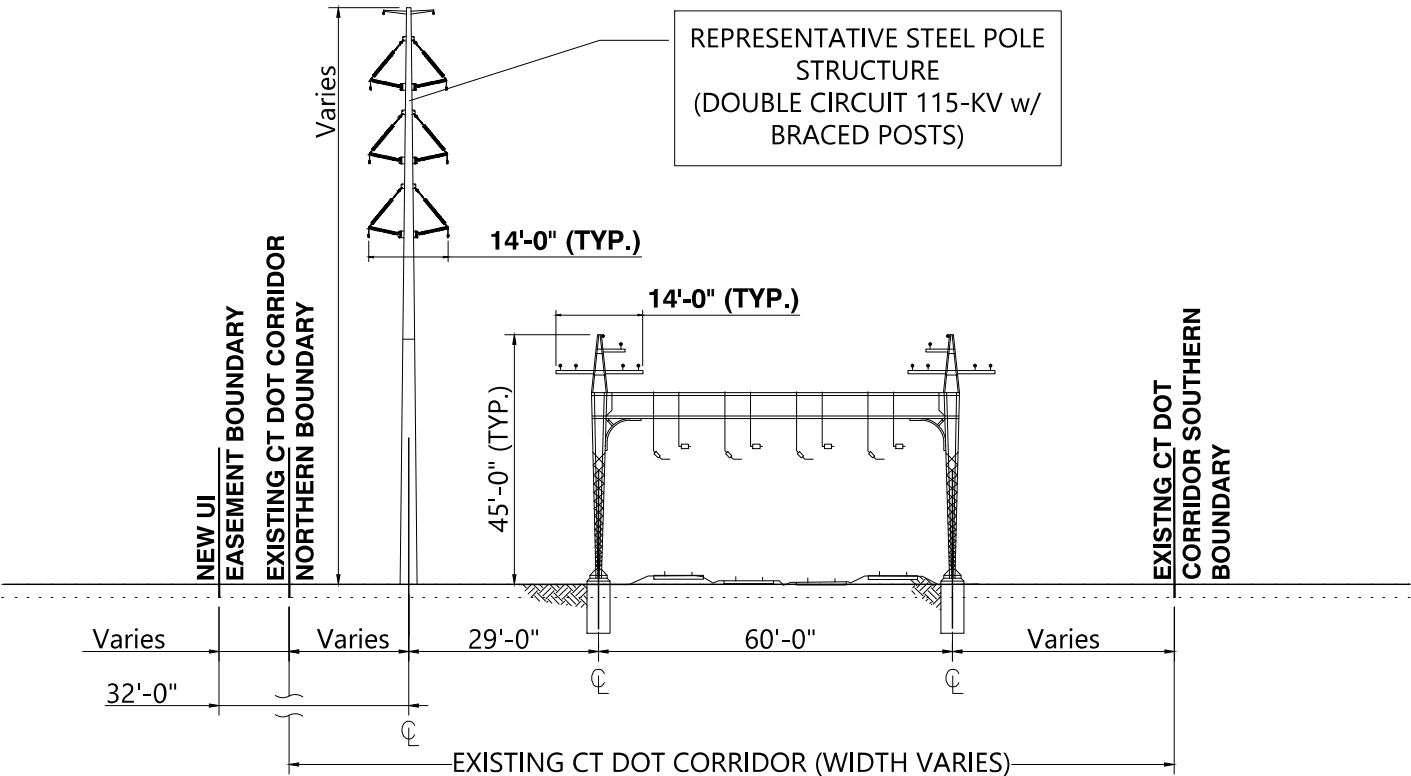
* TEMPORARY MATTING IS THE PREFERRED METHOD. THE USE OF TEMPORARY GRAVEL WILL BE IMPLEMENTED ONLY ON AN AS-NEEDED BASIS WHERE SAFETY OR OTHER UNKNOWN FIELD CONDITIONS MAY EXIST, AND WILL REQUIRE PRIOR UI APPROVAL.

GRAVEL WILL NOT BE UTILIZED IN WETLAND, WATERCOURSE OR ANY OTHER UI IDENTIFIED SENSITIVE RESOURCE AREAS. ALL WATERCOURSE CROSSINGS MUST BE SPANNED AS TO NOT IMPEDE WATER FLOW OR FISH PASSAGE.

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UI AVANGRID				UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN			
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR.	SFB	SCALE AS SHOWN	FILE:
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW	CK	JRD	NO.	
REV.	DATE	BY	DESCRIPTION	APP.	DATE:	JUNE 2024	
						MAPSHEET 10 OF 12	REV. 0-0B



XS-8 (FACING EAST)

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P1003N	TANGENT	XS-8	91	CONCRETE DRILLED PIER
P1004N	TANGENT	XS-8	81	CONCRETE DRILLED PIER
P1005N	TANGENT	XS-8	76	CONCRETE DRILLED PIER
P1006N	DEADEND	XS-8	86.5	CONCRETE DRILLED PIER

STRUCTURE DETAIL TABLE

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P1003N, P1004N, P1005N AND P1006N WILL BE INSTALLED.
- BONNETS B1003N/S, B1004N/S, B1005N/S AND B1006N/S WILL BE REMOVED.
- EXISTING WOOD POLES ON THE NORTH SIDE OF THE RAILROAD TRACKS TO BE REMOVED.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.

CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

- WATERCOURSE WH-WC8
- NO NDDDB AREA WITHIN SHEET

ROAD CROSSINGS

- NONE

ANSI D CADD Drawing. DO NOT REVISE MANUALLY.

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UI

AVANGRID

UI 115-kV RR PROJECT MILVON TO WEST RIVER

DEVELOPMENT & MANAGEMENT PLAN

MAPSHEET 11 STRUCTURES AND NOTES

0-0B

2024/05/15

WESTWOOD

SEGMENT 4 D&M PLAN FOR CSC SUBMISSION

0-0A

2024/04/19

WESTWOOD

SEGMENT 4 D&M PLAN FOR UI REVIEW

REV.

DATE

BY

DESCRIPTION

DR. SFB

CK. JRD

APP. MSP

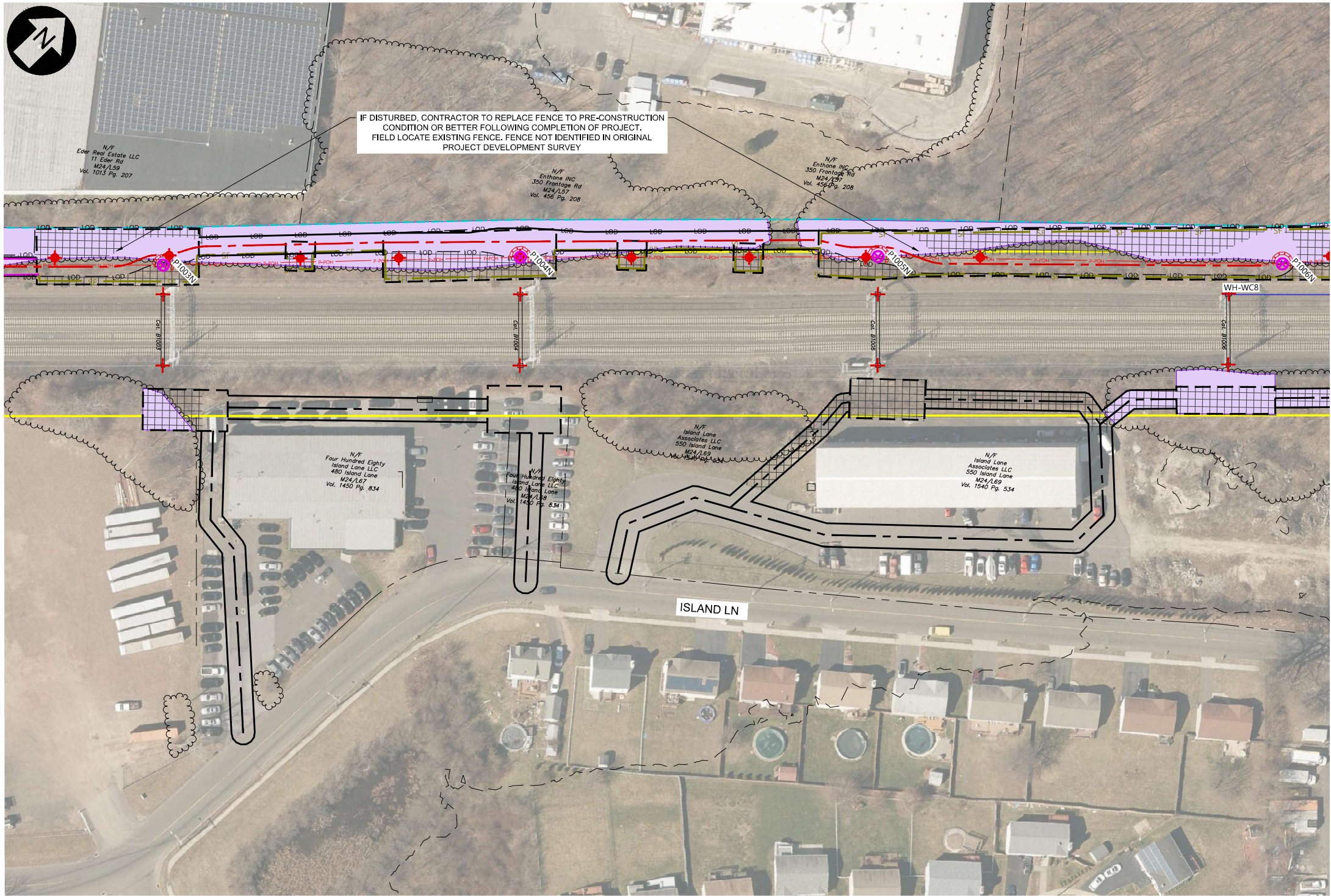
SCALE AS SHOWN

FILE:

NO.

REV.

0-0B



LEGEND

- PROPOSED TRANSMISSION POLE
EXISTING TRANSMISSION POLE
SUBSTATION TERMINATION STRUCTURE
PROPOSED TRANSMISSION CENTER LINE
NEW UI EASEMENT BOUNDARY
PROPOSED TEMPORARY ACCESS ROAD
PROPOSED WORK/PULLING PAD
EXISTING CTDOT CORRIDOR BOUNDARY
EXISTING PROPERTY LINE
PROPOSED TEMPORARY MATTING / TEMPORARY GRAVEL WITH UI APPROVAL*
TEMPORARY MATTED WETLAND IMPACT
TEMPORARY MATTED STREAM IMPACT
- FIELD DELINEATED TIDAL WETLAND
FIELD DELINEATED WETLAND
FEMA 100-YEAR FLOODPLAIN LIMIT
UI OWNED PROPERTY
FIELD DELINEATED STREAM
EXISTING TREE LINE
PROPOSED TREE CLEARING
FIELD DELINEATED TIDAL STREAM
CT DEEP COASTAL ZONE
PROPOSED PERMANENT ACCESS ROAD TO REMAIN
PERMANENT WETLAND IMPACT
OPEN ENDED WETLAND DELINEATION BOUNDARY
OPEN ENDED WETLAND DELINEATION DIRECTION
- EXISTING HARDWARE ONLY TO BE REMOVED
EXISTING STEEL POLE TO BE REMOVED
EXISTING LATTICE TOWER TO BE REMOVED
EXISTING BONNET TO BE REMOVED
EXISTING WOOD POLE TO BE REMOVED
EXISTING STEEL POLE TOP TO BE REMOVED AND CAPPED
DRAINAGE CROSSING
- CATCH BASIN INLET PROTECTION
STRAW BALE SEDIMENT PROTECTION
CONCRETE WASHOUT
ROCK CONSTRUCTION ENTRANCE
- SF
RSC
EF
LOD
LOD
LOD
- SEDIMENT BARRIER
REDUNDANT SEDIMENT BARRIER - DOUBLE ROW
EXCLUSIONARY FENCE
LIMIT OF DISTURBANCE

**IN LOCATIONS WHERE MATTING HEIGHT IS A MINIMUM OF 20" ABOVE EXISTING GRADE WITHOUT ANY GAPS IN OUTER WALL FACE OF MATTING, THE MATTING IS AN ACCEPTABLE REPLACEMENT OF EXCLUSION FENCING SUBJECT TO APPROVAL OF UI AND/OR THEIR DESIGNATED REPRESENTATIVE.

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GRAVEL WILL NOT BE UTILIZED IN WETLAND, WATERCOURSE OR ANY OTHER UI IDENTIFIED SENSITIVE RESOURCE AREAS. ALL WATERCOURSE CROSSINGS MUST BE SPANNED AS TO NOT IMPEDE WATER FLOW OR FISH PASSAGE.

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UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN				SCALE AS SHOWN FILE:		REV.
REV.	DATE	BY	DESCRIPTION	APP.	DATE	
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR. SFB	NO. MAPSHEET 11 OF 12	0-0B
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW	CK. JRD		
				APP. MSP		
				DATE: JUNE 2024		

GENERAL NOTES

GENERAL WORK DESCRIPTION

- NEW STEEL MONOPOLES P1007N, P1008NN AND P1008WN WILL BE INSTALLED.
- BONNETS B1007N/S AND B1008S WILL BE REMOVED.
- EXISTING STEEL STRUCTURES OUTSIDE AND ADJACENT TO ALLINGS CROSSING SUBSTATION WILL REMAIN; HARDWARE AND INSULATORS WILL BE REPLACED.
- THREE EXISTING STEEL POLES ON THE OPPOSITE SIDE OF THE TRACKS FROM ALLINGS CROSSING SUBSTATION TO BE REMOVED.
- EXISTING WOOD POLES ON THE NORTH SIDE OF THE RAILROAD TRACKS TO BE REMOVED.
- THE TOP PORTIONS OF FOUR EXISTING LATTICE TOWERS (TP1007EN, TP1007ES, TP1008WN, AND TP1008WS), LOCATED ADJACENT TO ALLINGS CROSSING ROAD, WILL BE REMOVED.
- PROJECT ACTIVITIES INCLUDE: VEGETATION CLEARING, REMOVAL OF EXISTING 115-kV EQUIPMENT, AND INSTALLATION OF ACCESS ROADS, WORK PADS, FOUNDATIONS, STEEL MONOPOLES, AND CONDUCTOR.
- EXCLUSIONARY FENCING TO BE COORDINATED WITH UI IN ACCORDANCE WITH THE SPECIES PROTECTION PLAN.
- EROSION AND SEDIMENT CONTROLS TO BE INSTALLED ON SITE UNDER THE SUPERVISION OF AN ENVIRONMENTAL INSPECTOR IN ACCORDANCE WITH CT DEEP GUIDELINES AND THE PROJECT SWPCP.
- EROSION AND SEDIMENT CONTROLS ARE SHOWN IN GENERAL LOCATIONS ON THE PLANS BUT MAY BE MODIFIED BASED ON SITE FIELD CONDITIONS.
- CONCRETE WASHOUTS ARE SHOWN IN GENERAL LOCATIONS AND ARE TO BE UTILIZED IN ACCORDANCE WITH CT DEEP EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- TEMPORARY ACCESS AND PADS WITH NO MATTING ARE ON EXISTING PAVEMENT OR GRAVEL. ACCESS ROUTES THROUGH PAVEMENT AND GRAVEL AREAS ARE SUBJECT TO CHANGE BASED ON ACCESS AGREEMENTS WITH LANDOWNER.
- NO STUMP REMOVAL, GRADING, MECHANIZED PUSHING, DRAGGING, TEMPORARY SIDE-CASTING OF EXCAVATED MATERIAL OR OTHER SIMILAR ACTIVITIES TO BE CONDUCTED IN WETLANDS UNLESS OTHERWISE DEPICTED.
- GUARD STRUCTURE WORK SPACES TO BE UTILIZED ON AN AS-NEEDED BASIS FOR WIRE PULLING ACTIVITIES. SCHEDULE AND WORK AREA PROTECTION TO BE PROVIDED BY CONTRACTOR IN ACCORDANCE WITH MUTCD GUIDELINES AND APPROPRIATELY COORDINATED WITH MUNICIPALITY

CT DOT CORRIDOR/PROJECT WORK AREA DESCRIPTION

WETLANDS, WATERCOURSES, WATERWAYS AND SENSITIVE AREAS

- WATERCOURSES WH-WC8, 9, 10, 11, 12, 13, 14, 15, & 16
- WETLAND WH-W4
- WETLAND WH-W5
- NO NDDB AREA WITHIN SHEET

ROAD CROSSINGS

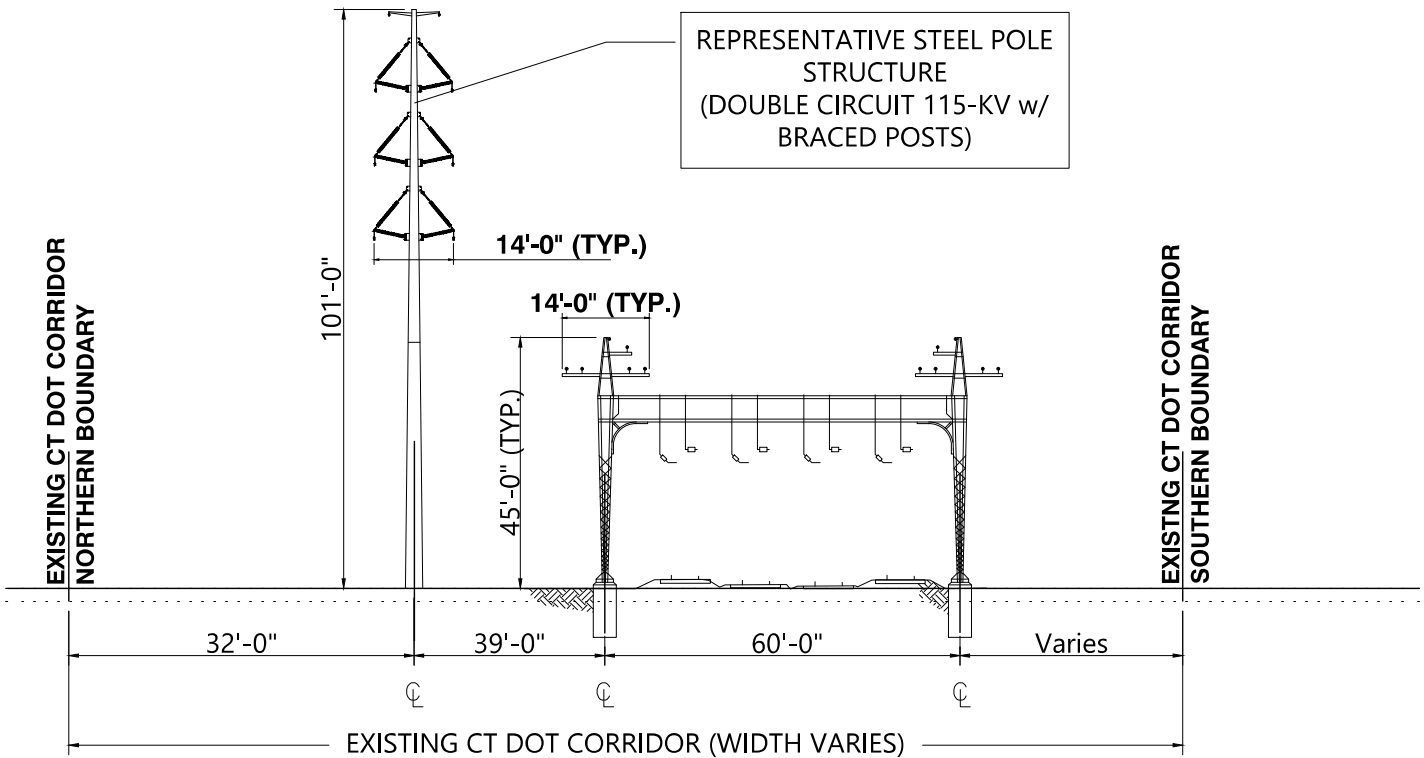
- ALLINGS CROSSING RD

Drainage ID	Watercourse ID	Status	Crossing type/Culvert size	Closest Structure
18	WH-WC9	Temporary	Culvert - size to channel dimensions	P1007N
19	WH-WC10	Temporary	Culvert - size to channel dimensions	B1008S
20	WH-WC14	Temporary	Culvert - size to channel dimensions	TP1008

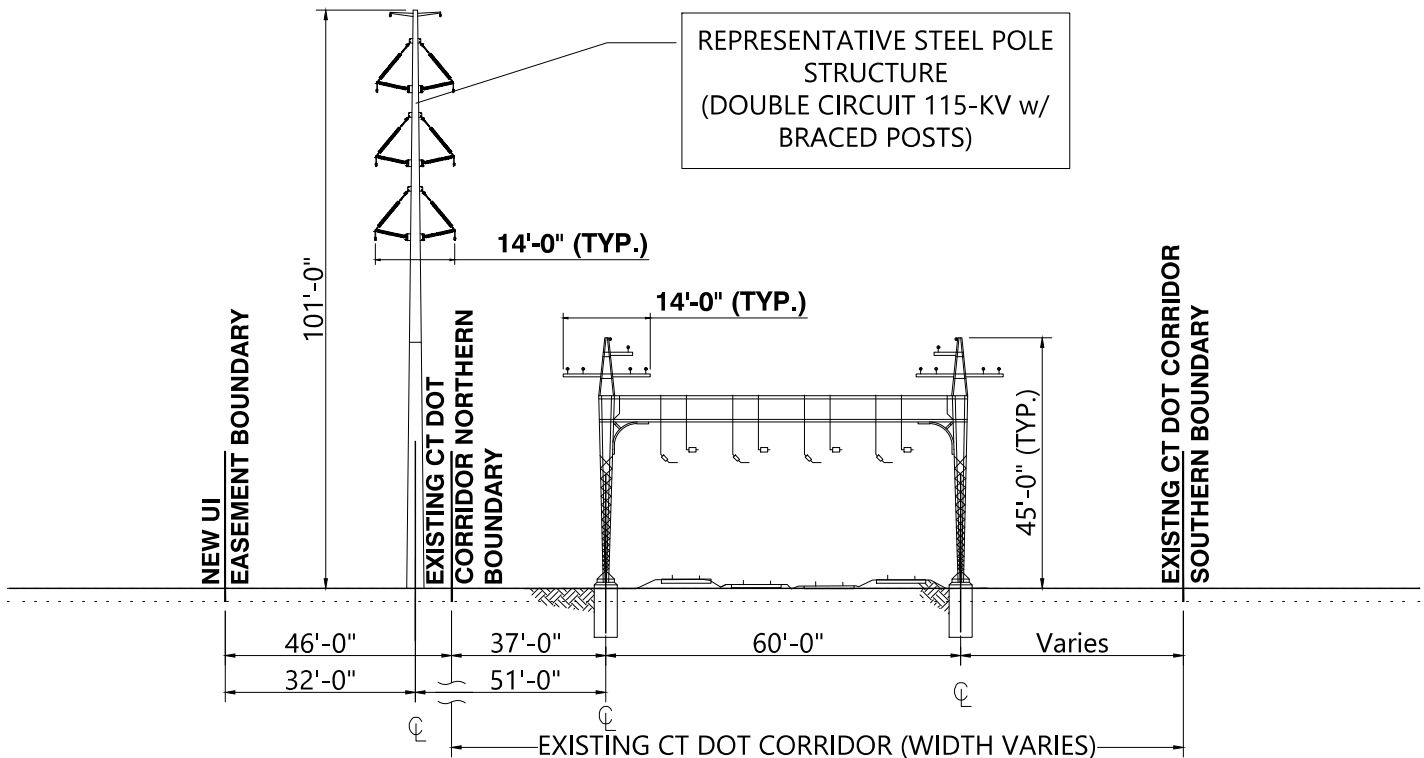
DRAINAGE CROSSING TABLE

STRUCTURE NO.	STRUCTURE TYPE	CROSS SECTION	HEIGHT (FT)	FOUNDATION
P1007N	TANGENT	XS-8a	101	CONCRETE DRILLED PIER
P1008WN	TANGENT	XS-7	101	CONCRETE DRILLED PIER

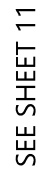
STRUCTURE DETAIL TABLE


























XS-7 (FACING EAST)










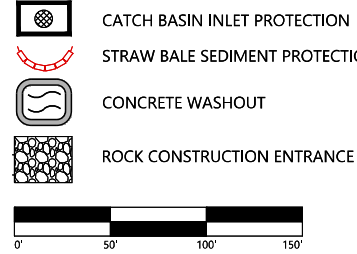
XS-8a (FACING EAST)







PROPOSED TRANSMISSION POLE
 EXISTING TRANSMISSION POLE
 SUBSTATION TERMINATION STRUCTURE
 PROPOSED TRANSMISSION CENTER LINE
 NEW UI EASEMENT BOUNDARY
 PROPOSED TEMPORARY ACCESS ROAD
 PROPOSED WORK/PULLING PAD
 EXISTING CTDOT CORRIDOR BOUNDARY
 EXISTING PROPERTY LINE
 PROPOSED TEMPORARY MATTING / TEMPORARY GRAVEL WITH UI APPROVAL*
 TEMPORARY MATTED WETLAND IMPACT
 TEMPORARY MATTED STREAM IMPACT

- | | | | |
|---|--|---|----|
|  | FIELD DELINEATED TIDAL WETLAND |  | E |
|  | FIELD DELINEATED WETLAND |  | E |
|  | FEMA 100-YEAR FLOODPLAIN LIMIT |  | E |
|  | UI OWNED PROPERTY |  | E |
|  | FIELD DELINEATED STREAM |  | E |
|  | EXISTING TREE LINE |  | E |
|  | PROPOSED TREE CLEARING |  | A |
|  | FIELD DELINEATED TIDAL STREAM |  | XX |
|  | CT DEEP COASTAL ZONE |  | D |
|  | PROPOSED PERMANENT ACCESS ROAD TO REMAIN |  | |
|  | PERMANENT WETLAND IMPACT | | |
|  | OPEN ENDED WETLAND DELINEATION BOUNDARY | | |
|  | OPEN ENDED WETLAND DELINEATION DIRECTION | | |

- | | |
|---|---|
|  | EXISTING HARDWARE ONLY TO BE REMOVED |
|  | EXISTING STEEL POLE TO BE REMOVED |
|  | EXISTING LATTICE TOWER TO BE REMOVED |
|  | EXISTING BONNET TO BE REMOVED |
|  | EXISTING WOOD POLE TO BE REMOVED |
|  | EXISTING STEEL POLE TOP TO BE REMOVED
AND CAPPED |
| xx
 | DRAINAGE CROSSING |



-  SF SEDIMENT BARRIER
 RSC REDUNDANT SEDIMENT BARRIER - DOUBLE ROW
 EF EXCLUSIONARY FENCE
 LOD LOD LIMIT OF DISTURBANCE

****IN LOCATIONS WHERE MATTING HEIGHT IS A MINIMUM OF 20" ABOVE EXISTING GRADE WITHOUT ANY GAPS IN OUTER WALL FACE OF MATTING, THE MATTING IS AN ACCEPTABLE REPLACEMENT OF EXCLUSION FENCING SUBJECT TO APPROVAL OF UI AND/OR THEIR DESIGNATED REPRESENTATIVE.**


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GRAVEL WILL NOT BE UTILIZED IN WETLAND, WATERCOURSE OR ANY OTHER UI IDENTIFIED SENSITIVE RESOURCE AREAS. ALL WATERCOURSE CROSSINGS MUST BE SPANNED AS TO NOT IMPEDE WATER FLOW OR FISH PASSAGE.

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 AVANGRID					UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN				
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION		DR. CK.	SFB JRD	SCALE AS SHOWN FILE: NO.		
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW		APP. DATE:	MSP JUNE 2024	MAPSHEET 12 OF 12		
REV.	DATE	BY	DESCRIPTION		APP.		REV.		
							0-0B		


SUBSTATION WORK PLANS

SUBSTATION 1: WOODMONT SUBSTATION
SUBSTATION 2: ALLINGS CROSSING SUBSTATION

ANSI D CADD Drawing. DO NOT REVISE MANUALLY.

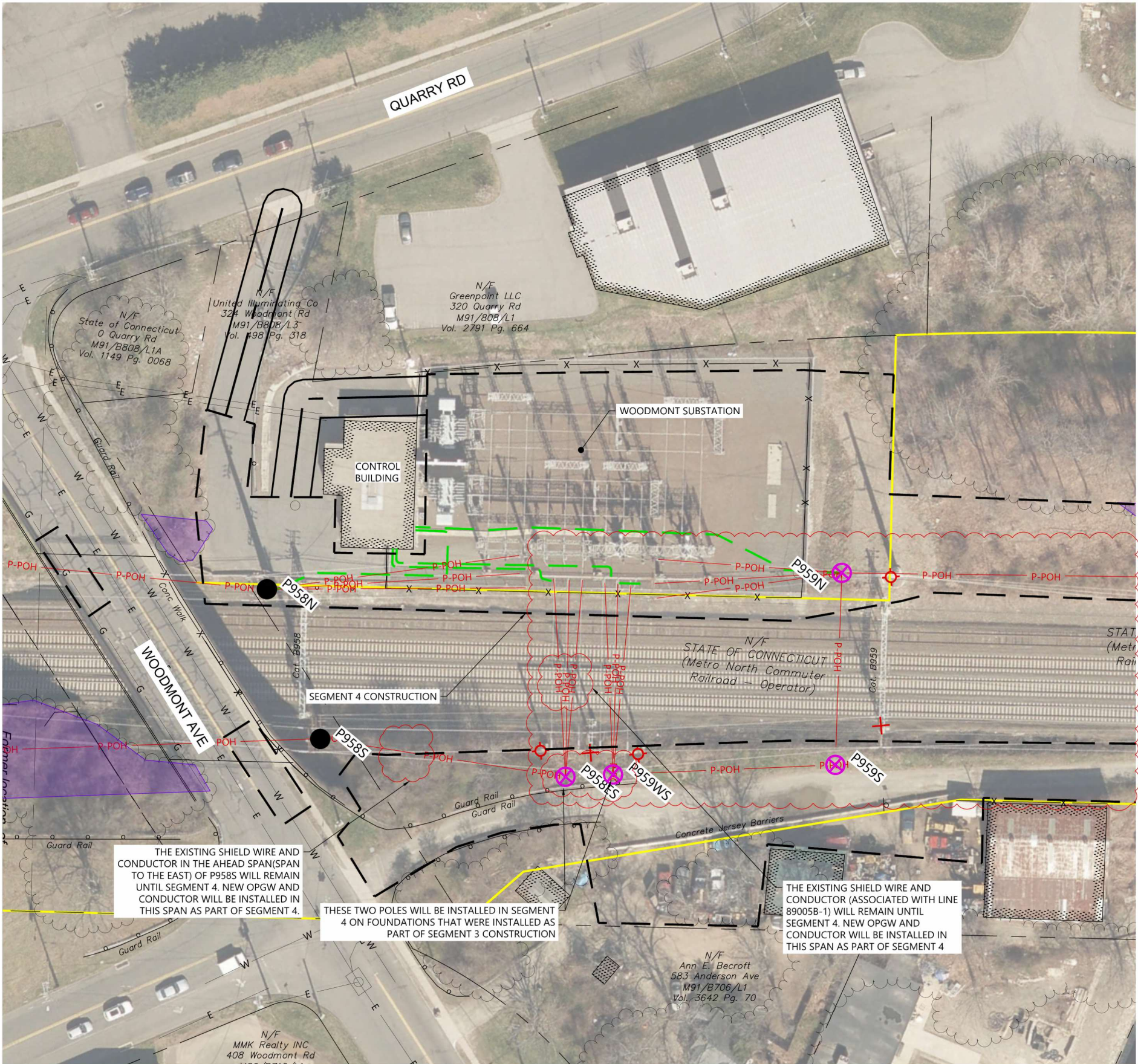
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						UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN			
						SUBSTATION WORK PLAN INDEX			
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR.	SFB	SCALE	AS SHOWN	FILE:	REV.
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW	CK.	JRD	NO.			
REV.	DATE	BY	DESCRIPTION	APP.	DATE:				
					JUNE 2024			0-0B	


GENERAL WOODMONT SUBSTATION NOTES:


1. UNDERGROUND ADSS FROM EXISTING P958N TO SUBSTATION CONTROL BUILDING IS TO BE INSTALLED AS PART OF SEGMENT 3. ALL OTHER UNDERGROUND ADSS MAY BE INSTALLED AS PART OF SEGMENT 3 DEPENDING ON CONTRACTOR'S SEQUENCING APPROACH. IF NOT INSTALLED AS PART OF SEGMENT 3 CONSTRUCTION, THEY WILL BE INSTALLED AS PART OF SEGMENT 4 CONSTRUCTION.





LEGEND


- 

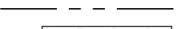
PROPOSED TRANSMISSION POLE
- 


EXISTING TRANSMISSION POLE
- 


SUBSTATION TERMINATION STRUCTURE
- 


PROPOSED TRANSMISSION CENTER LINE
- 

NEW UI EASEMENT BOUNDARY
- 

PROPOSED TEMPORARY ACCESS ROAD
- 


PROPOSED WORK/PULLING CONSTRUCTION AREA
- 

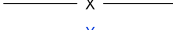
EXISTING CTDOT CORRIDOR BOUNDARY
- 


EXISTING PROPERTY LINE
- 


PROPOSED TEMPORARY MATTING
-


EXISTING TREE LINE
-


PROPOSED TREE CLEARING
- 


PROPOSED UNDERGROUND ADSS
- 


EXISTING FENCE
- 


PROPOSED TEMPORARY SUBSTATION FENCE
- 

EXISTING HARDWARE ONLY TO BE REMOVED
- 

EXISTING STEEL POLE TO BE REMOVED
- 

EXISTING LATTICE TOWER TO BE REMOVED
- 

EXISTING BONNET TO BE REMOVED
- 

EXISTING WOOD POLE TO BE REMOVED
- 

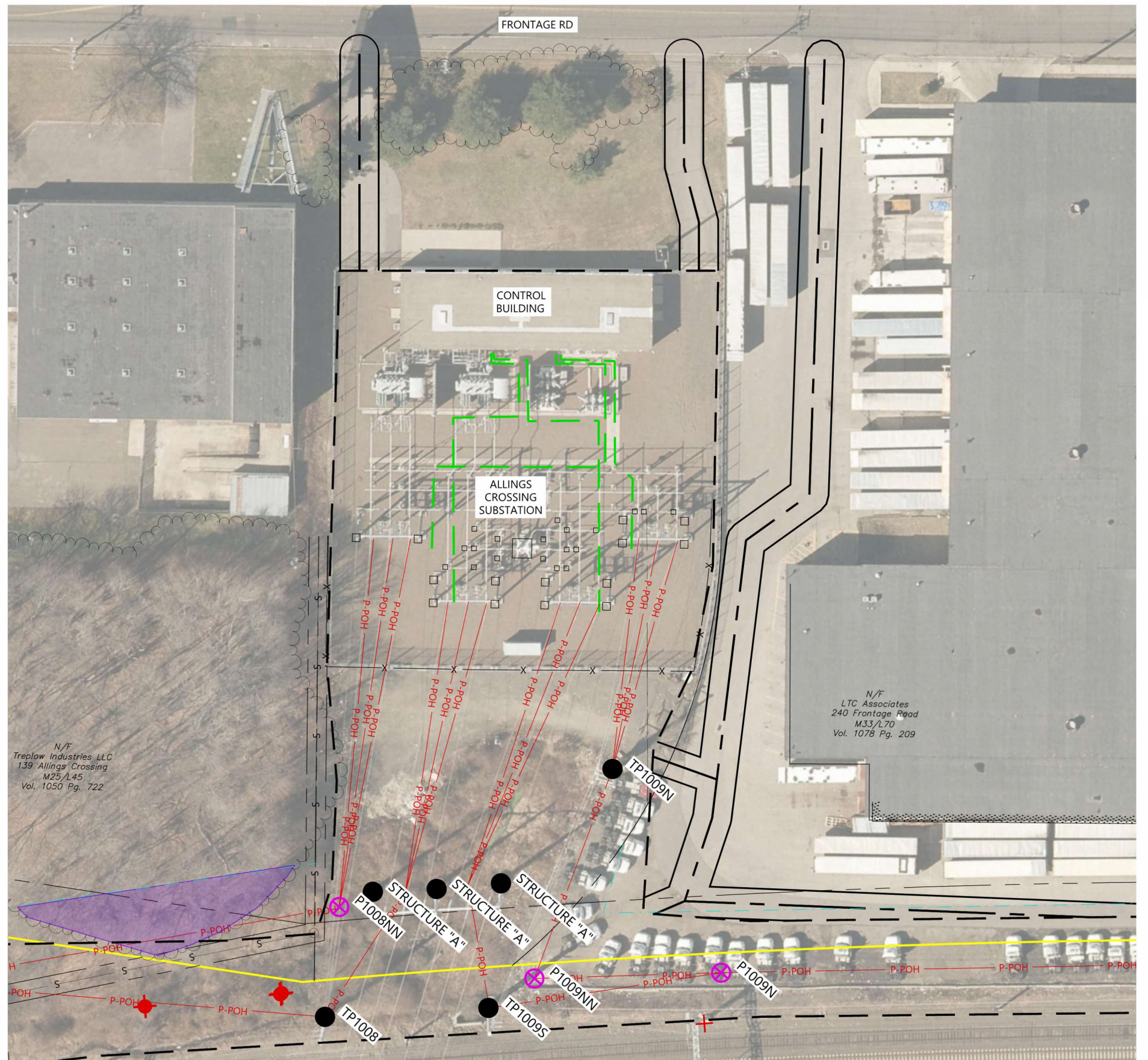
EXISTING STEEL POLE TOP TO BE REMOVED AND CAPPED

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UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN WOODMONT SUBSTATION					UI AVANGRID		SUB 1 OF 2		REV.
REV.	DATE	BY	DESCRIPTION	APP.	DATE:	FILE:	NO.	FILE:	0-08
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION		DR.	SFB			
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW		CK.	JRD			
					APP.	MSP			
					DATE:	JUNE 2024			

1. UNDERGROUND ADSS FROM THE EASTERN MOST TERMINATION STRUCTURE TO THE SUBSTATION CONTROL BUILDING IS TO BE INSTALLED AS PART OF SEGMENT 2. ALL OTHER UNDERGROUND ADSS MAY BE INSTALLED AS PART OF SEGMENT 2 DEPENDING ON CONTRACTORS SEQUENCING APPROACH. IF NOT INSTALLED AS PART OF SEGMENT 2, IT WILL BE INSTALLED AS PART OF SEGMENT 4 CONSTRUCTION.




	PROPOSED TRANSMISSION POLE		PROPOSED UNDERGROUND ADSS
	EXISTING TRANSMISSION POLE		EXISTING FENCE
	SUBSTATION TERMINATION STRUCTURE		PROPOSED TEMPORARY SUBSTATION FENCE
	PROPOSED TRANSMISSION CENTER LINE		EXISTING HARDWARE ONLY TO BE REMOVED
	NEW UI EASEMENT BOUNDARY		EXISTING STEEL POLE TO BE REMOVED
	PROPOSED TEMPORARY ACCESS ROAD		EXISTING LATTICE TOWER TO BE REMOVED
	PROPOSED WORK/PULLING CONSTRUCTION AREA		EXISTING BONNET TO BE REMOVED
	EXISTING CTDOT CORRIDOR BOUNDARY		EXISTING WOOD POLE TO BE REMOVED
	EXISTING PROPERTY LINE		EXISTING STEEL POLE TOP TO BE REMOVED AND CAPPED
	PROPOSED TEMPORARY MATTING		
	EXISTING TREE LINE		
	PROPOSED TREE CLEARING		

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<div></div>						UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN					
						ALLINGS CROSSING SUBSTATION					
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION				DR.	SFB	SCALE AS SHOWN		FILE:
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW				CK.	JRD	NO.		REV.
							APP.	MSP	SUB 2 OF 2		0-0B
REV.	DATE	BY	DESCRIPTION		APP.	DATE	JUNE 2024				

TYPICAL DETAILS

DET 1: EROSION AND SEDIMENT CONTROL
DET 2: CONSTRUCTION DETAILS

ANSI D CADD Drawing. DO NOT REVISE MANUALLY.

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						UI 115-kV RR PROJECT MILVON TO WEST RIVER DEVELOPMENT & MANAGEMENT PLAN			
						TYPICAL DETAIL INDEX			
0-0B	2024/05/15	WESTWOOD	SEGMENT 4 D&M PLAN FOR CSC SUBMISSION	DR.	SFB	SCALE	AS SHOWN	FILE:	
0-0A	2024/04/19	WESTWOOD	SEGMENT 4 D&M PLAN FOR UI REVIEW	CK	JRD	NO.			
REV.	DATE	BY	DESCRIPTION	APP.	DATE:	JUNE 2024			REV.
								0-0B	

NOTE:
1. FIBER ROLLS SHALL BE PREFABRICATED AND MADE FROM WEED FREE RICE STRAW, FLAX, OR A SIMILAR AGRICULTURAL MATERIAL BOUND INTO A TIGHT TUBULAR ROLL BY NETTING.
2. STAKE FIBER ROLLS INTO THE TRENCH. DRIVE STAKES AT THE END OF EACH FIBER ROLL AND SPACED 4 FEET MAXIMUM ON CENTER. USE WOOD STAKES WITH NOMINAL CLASSIFICATION OF 0.75 IN BY 0.75 IN. AND A MINIMUM LENGTH OF 24 IN.
3. PREPARE THE SLOPE BEFORE BEGINNING THE INSTALLATION.
4. DIG SMALL TRENCHES ACROSS THE SLOPE ON THE CONTOUR. THE TRENCH DEPTH SHOULD BE 1/4 TO 1/3 OF THE THICKNESS OF THE ROLL AND THE WIDTH SHOULD EQUAL THE ROLL DIAMETER, IN ORDER TO PROVIDE AREA TO BACKFILL THE TRENCH.
5. ROLLS SHALL BE INSTALLED PERPENDICULAR TO WATER MOVEMENT, AND PARALLEL TO THE SLOPE CONTOUR.
6. START BUILDING TRENCHES AND INSTALLING ROLLS FROM THE BOTTOM OF THE SLOPE AND WORK UP.
7. TURN THE ENDS OF THE FIBER ROLLS UP SLOPE TO PREVENT RUNOFF FROM GOING AROUND THE ROLL.
8. IF MORE THAN ONE FIBER ROLL IS PLACED IN A ROW, THE ROLLS SHOULD BE OVERLAPPED, NOT ABUTTED.
9. FIBER ROLLS ENCASED WITH PLASTIC NETTING ARE USED FOR A TEMPORARY APPLICATION ONLY AND SHOULD BE REMOVED FOLLOWING STABILIZATION. FIBER ROLLS USED IN A PERMANENT APPLICATION SHALL BE ENCASED WITH A BIODEGRADABLE MATERIAL AND MAY BE LEFT IN.
10. TEMPORARY INSTALLATIONS SHOULD ONLY BE REMOVED WHEN UP GRADIENT AREAS ARE STABILIZED PER GENERAL PERMIT REQUIREMENTS, AND/OR POLLUTANT SOURCES NO LONGER PRESENT A HAZARD. BUT, THEY SHOULD ALSO BE REMOVED BEFORE VEGETATION BECOMES TOO MATURE SO THAT THE REMOVAL PROCESS DOES NOT DISTURB MORE SOIL AND VEGETATION THAN IS NECESSARY.
11. FIBER ROLLS MUST BE INSPECTED IN ACCORDANCE WITH GENERAL PERMIT REQUIREMENTS FOR THE ASSOCIATED PROJECT TYPE AND RISK LEVEL. IT IS RECOMMENDED THAT A MINIMUM, THE BMPs BE INSPECTED WEEKLY, PRIOR TO FORECASTED RAIN EVENTS, DAILY DURING EXTENDED RAIN EVENTS, AND AFTER THE CONCLUSION OF RAIN EVENTS.

SLOPE (H:V)	MAX. SPACING (FT)
< 4:1	20
4:1 - 2:1	15
> 2:1	10

* A CLOSER SPACING IS MORE EFFECTIVE

12. REPAIR OR REPLACE SPLIT, TORN, UNRAVELING, OR SLUMPING FIBER ROLLS.
13. IF THE FIBER ROLL IS USED AS A SEDIMENT CAPTURE DEVICE, OR AS AN EROSION CONTROL DEVICE TO MAINTAIN SHEET FLOWS, SEDIMENT THAT ACCUMULATES IN THE BMP SHOULD BE PERIODICALLY REMOVED IN ORDER TO MAINTAIN BMP EFFECTIVENESS. SEDIMENT SHOULD BE REMOVED WHEN SEDIMENT ACCUMULATION REACHES ONE-THIRD THE DESIGNATED SEDIMENT STORAGE DEPTH.
14. IF FIBER ROLLS ARE USED FOR EROSION CONTROL, SEDIMENT REMOVAL SHOULD NOT BE REQUIRED AS LONG AS THE SYSTEM CONTINUES TO CONTROL THE GRADE. SEDIMENT CONTROL BMPs WILL LIKELY BE REQUIRE IN CONJUNCTION WITH THIS TYPE OF APPLICATION.
15. REPAIR ANY RILLS OR GULLIES PROMPTLY.

GD45

NOTE:
ROCK CONSTRUCTION ENTRANCE SHOULD CONTAIN MAXIMUM SIDE SLOPES OF 4:1. ROCK ENTRANCE SHOULD BE INSPECTED AND MAINTAINED REGULARLY. ROCK ENTRANCE LENGTH MAY NEED TO BE EXTENDED IN CLAY SOILS.

GD07

NOTE:
1. REFER TO THE PROJECT SWPCP FOR IMPLEMENTATION REQUIREMENTS.
2. MATS/BLANKETS SHOULD BE INSTALLED VERTICALLY DOWNSLOPE.
3. SLOPE SURFACE SHALL BE FREE OF ROCKS, CLODS, STICK AND GRASS.
4. MATS/BLANKETS SHALL HAVE GOOD SOIL CONTACT.
5. LAY BLANKETS LOOSELY AND STAKE OR STAPLE TO MAINTAIN DIRECT CONTACT WITH THE SOIL.
6. DO NOT STRETCH.
7. BLANKET TYPE AND WEIGHT MUST BE CHOSEN BASED ON SITE CONDITIONS AND MANUFACTURERS RECOMMENDATIONS.
8. STAPLE LENGTHS SHALL CONFORM TO MANUFACTURERS RECOMMENDATIONS.

GD21

NOTE:
1. ALL GEOTEXTILE USED FOR INLET PROTECTION SHALL BE MONOFILAMENT IN BOTH DIRECTIONS.
2. FINISHED SIZE (INCLUDING POCKETS) SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
3. DO NOT INSTALL PROTECTION IN INLETS SHALLOWER THAN 30". THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CLINCH THE BAG (USING PLASTIC ZIP TIES) TO ACHIEVE THE 3" CLEARANCE.

GD10

NOTES:
1. MULCH ROLLS SHALL BE INSTALLED PRIOR TO UPSLOPE DISTURBANCE ACTIVITIES COMMENCE.
2. ROLLS SHALL BE INSTALLED PERPENDICULAR TO WATER MOVEMENT, AND PARALLEL TO THE SLOPE CONTOUR.
3. TURN THE ENDS OF THE MULCH ROLLS UP SLOPE TO PREVENT RUNOFF FROM GOING AROUND THE ROLL. THE UPSLOPE POINT SHOULD BE A MINIMUM 6" HIGHER IN ELEVATION THAN THE LOW POINT.
4. IF MORE THAN ONE MULCH ROLL IS PLACED IN A ROW, THE ROLLS SHOULD BE OVERLAPPED A MINIMUM OF 12 INCHES, NOT ABUTTED.
5. TEMPORARY INSTALLATIONS SHOULD ONLY BE REMOVED WHEN UP GRADIENT AREAS ARE STABILIZED PER GENERAL PERMIT REQUIREMENTS, AND/OR POLLUTANT SOURCES NO LONGER PRESENT A HAZARD. BUT, THEY SHOULD ALSO BE REMOVED BEFORE VEGETATION BECOMES TOO MATURE SO THAT THE REMOVAL PROCESS DOES NOT DISTURB MORE SOIL AND VEGETATION THAN IS NECESSARY.
6. MULCH ROLLS MUST BE INSPECTED IN ACCORDANCE WITH GENERAL PERMIT REQUIREMENTS. IT IS RECOMMENDED THAT AT A MINIMUM, THE BMPs BE INSPECTED WEEKLY, PRIOR TO FORECASTED RAIN EVENTS, DAILY DURING EXTENDED RAIN EVENTS, AND AFTER THE CONCLUSION OF RAIN EVENTS.
7. REPAIR OR REPLACE SPLIT, TORN, OR FLATTENED MULCH ROLLS.
8. SEDIMENT THAT ACCUMULATES UPSLOPE OF THE BMP SHOULD BE PERIODICALLY REMOVED IN ORDER TO MAINTAIN BMP EFFECTIVENESS. SEDIMENT SHOULD BE REMOVED WHEN SEDIMENT ACCUMULATION REACHES ONE-HALF THE DESIGNATED SEDIMENT STORAGE DEPTH.
9. RILLS OR GULLIES MAY BEGIN TO FORM FOLLOWING MAJOR STORM EVENTS WHERE RUNOFF HAS OVERTOPPED THE MULCH ROLLS. THESE RILLS OR GULLIES SHOULD BE PROMPTLY REPAIRED.

GD43

NOTE:
1. BALES SHOULD BE SEED-FREE STRAW BALES
2. BALES SHOULD BE WIRE BOUND OR STRING TIED
3. ENSURE BALES ARE TIGHTLY FIT TOGETHER. FILL ANY GAPS OR HOLES WITH LOOSE STRAW TO PREVENT WASHOUT OF SEDIMENT

GD61

NOTE:
1. SILT FENCE MATERIAL SHOULD BE POLYPROPYLENE, POLYETHYLENE OR POLYAMIDE WOVEN OR NONWOVEN FABRIC. THE FABRIC WIDTH SHOULD BE 36 INCHES, WITH A MINIMUM UNIT WEIGHT OF 4.5 OZ/YD, MULLEN BURST STRENGTH EXCEEDING 190 LB/IN 2, ULTRAVIOLET STABILITY EXCEEDING 70%, AND MINIMUM APPARENT OPENING SIZE OF U.S. SIEVE NO. 30.
2. SILT FENCE SHOULD BE SECURELY FASTENED TO EACH SUPPORT POST. THERE SHOULD BE A 3FT OVERLAP, SECURELY FASTENED WHERE ENDS OF FABRIC MEET.
3. SILT FENCE SHALL BE PLACED ON SLOPE CONTOURS TO MAXIMIZE PONDING EFFICIENCY.
4. ALL ENDS OF THE SILT FENCE SHALL BE WRAPPED UPSLOPE SO THE ELEVATION OF THE BOTTOM OF FABRIC IS HIGHER THAN "PONDING HEIGHT".
5. INSPECT AND REPAIR FENCE AFTER EACH STORM EVENT AND REMOVE SEDIMENT WHEN ACCUMULATED TO 1/3 HEIGHT OF THE FABRIC OR MORE.
6. REMOVED SEDIMENT SHALL BE DEPOSITED TO AN AREA THAT WILL NOT CONTRIBUTE SEDIMENT OFF-SITE AND CAN BE PERMANENTLY STABILIZED.
7. SILT FENCE SHOULD REMAIN IN PLACE AND MAINTAINED UNTIL THE DISTURBED AREA IS PERMANENTLY STABILIZED.
8. INSTALL A SECOND ROW OF SILT FENCE APPROXIMATELY 5' FROM INITIAL ROW WHEN REDUNDANT PROTECTION IS REQUIRED WITHIN 50' OF WETLANDS AND STREAMS.

ESC01

NOTE:
1. EXCLUSIONARY FENCING SHALL BE INSTALLED IN ACCORDANCE WITH THIS DETAIL OR DETAIL ESC01 - SILT FENCE. IN AREAS WHERE THE FENCING CANNOT BE BURIED, THE FENCING SHALL BE INSTALLED WITH FABRIC FOLDED ONTO EXISTING GRADE TOWARDS THE WORK AREA AND COVERED WITH 6" OF CRUSHED STONE, PER ABOVE.
2. EXCLUSIONARY FENCE MATERIAL SHOULD BE POLYPROPYLENE, POLYETHYLENE OR POLYAMIDE WOVEN OR NONWOVEN FABRIC. THE FABRIC WIDTH SHOULD BE 36 INCHES, WITH A MINIMUM UNIT WEIGHT OF 4.5 OZ/YD, MULLEN BURST STRENGTH EXCEEDING 190 LB/IN 2, ULTRAVIOLET STABILITY EXCEEDING 70%, AND MINIMUM APPARENT OPENING SIZE OF U.S. SIEVE NO. 30.
3. EXCLUSIONARY FENCE SHOULD BE SECURELY FASTENED TO EACH SUPPORT POST. THERE SHOULD BE A 3FT OVERLAP, SECURELY FASTENED WHERE ENDS OF FABRIC MEET.
4. INSPECT EXCLUSIONARY FENCING DAILY IN ACTIVE WORK AREAS AND AFTER EACH STORM EVENT OF 0.25 INCHES OR GREATER; CONDUCT REPAIRS AS NECESSARY.
5. EXCLUSIONARY FENCE SHOULD REMAIN IN PLACE AND MAINTAINED UNTIL CONSTRUCTION ACTIVITIES HAVE CEASED.
6. ALL OPENINGS IN THE ISOLATION BARRIER USED FOR WORK AREA ACCESS ARE TO BE MAINTAINED AND MONITORED IN ACCORDANCE WITH THE SPECIES PROTECTION PLAN. ALL OPENINGS, USED DURING THE WORKDAY FOR ACCESSIBILITY, MUST BE CLOSED WITH TEMPORARY SILT FENCING BACKED WITH STRAWBALES AT THE COMPLETION OF EACH DAY AND WHEN THE WORK AREA IS NOT IN ACTIVE USE. ALTERNATIVELY, A TEMPORARY, SELF-SUPPORTED PLYWOOD BARRIER WHICH MAINTAINS COMPLETE CONTACT WITH THE GROUND CAN BE USED.
7. STRAW BALES MAY NOT EXCLUSIVELY BE USED AS EXCLUSIONARY FENCE.

ESC01.1

NOTE:
1. CULVERT INSTALLATION AND COMPACTION SHALL ADHERE TO MANUFACTURER'S RECOMMENDATIONS.
2. REFER TO DRAINAGE CROSSING SCHEDULE FOR SIZING.
3. FIT CULVERT TO FIELD CONDITIONS.
4. MINIMIZE DISTURBANCE TO EXISTING STREAMS/DITCHES DURING CULVERT INSTALLATION.
5. PERIMETER SEDIMENT CONTROL MAY CONSIST OF SILT FENCE, FIBER LOGS, WOOD MULCH BERMS OR TOPSOIL BERMS. REFER TO ASSOCIATED DETAIL AND/OR NPDES PERMIT FOR DETAILS.

DN-03

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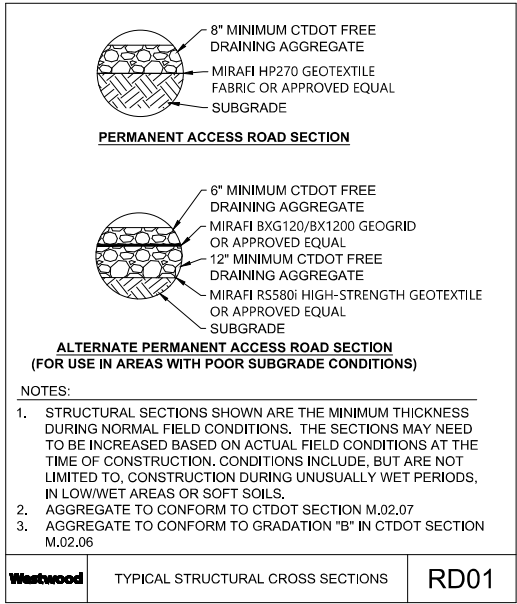
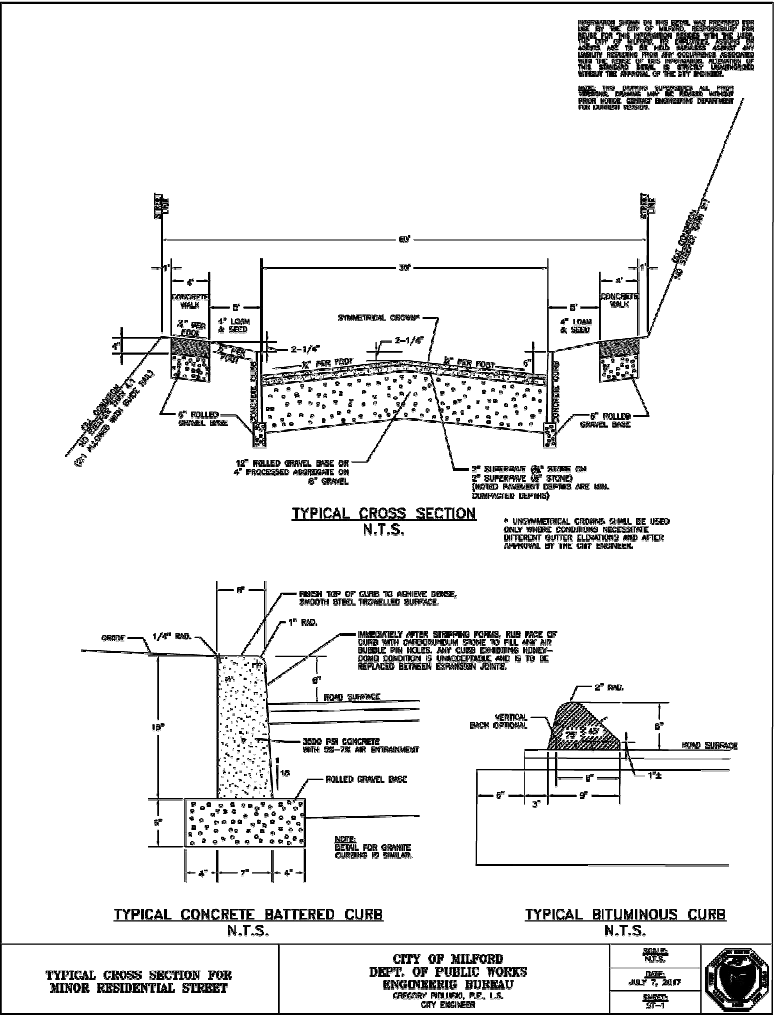
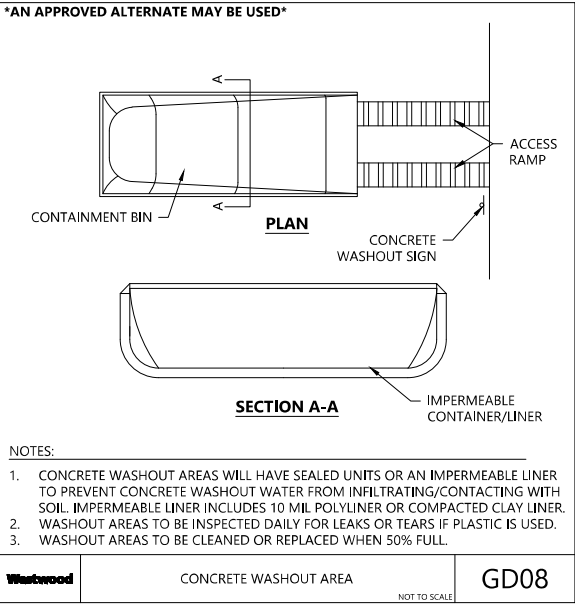
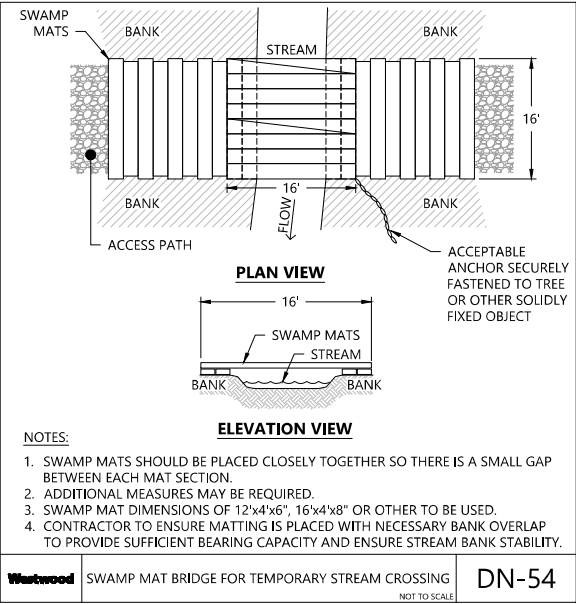
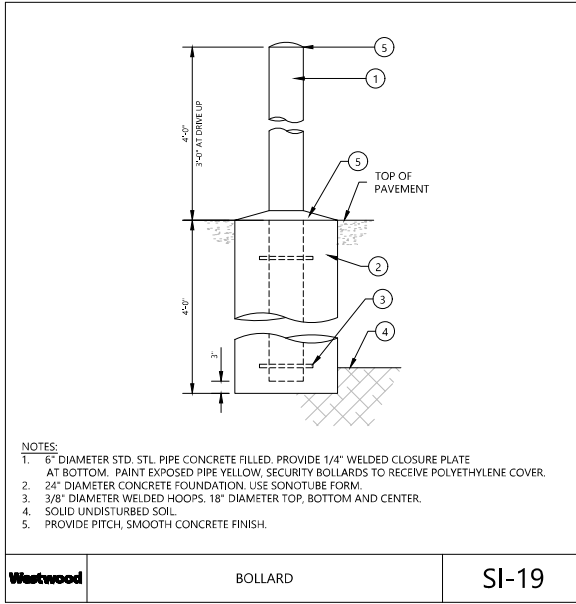
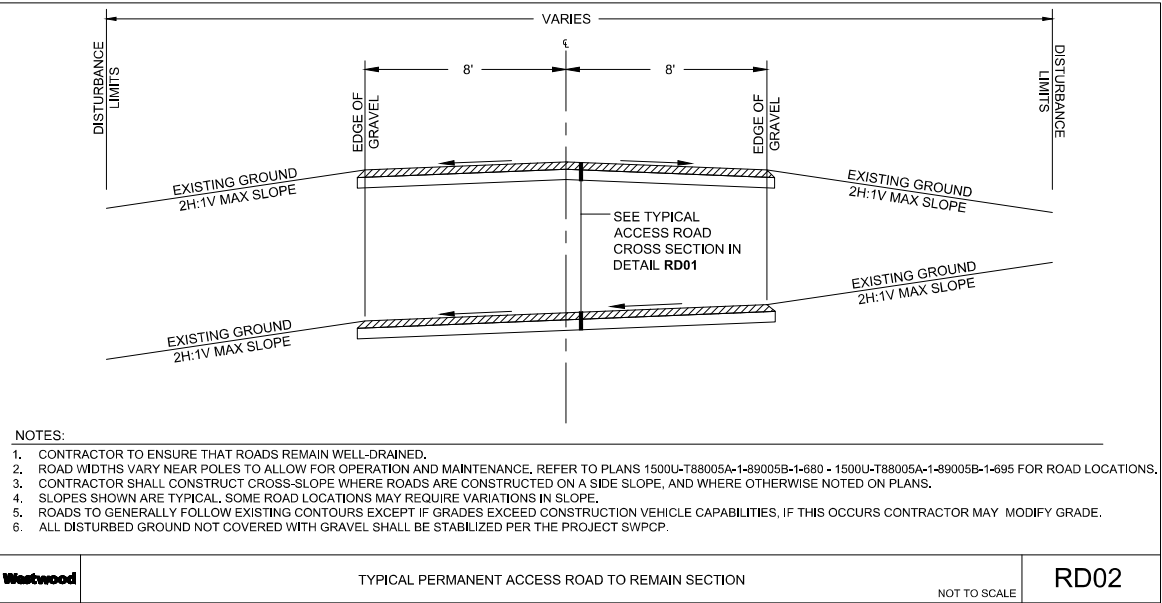
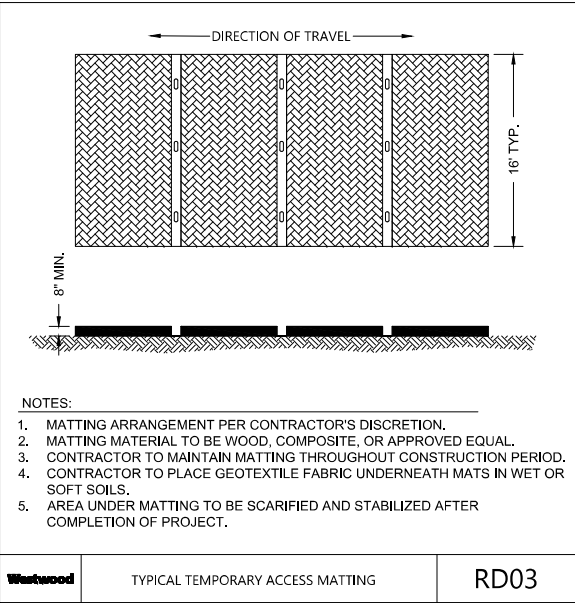
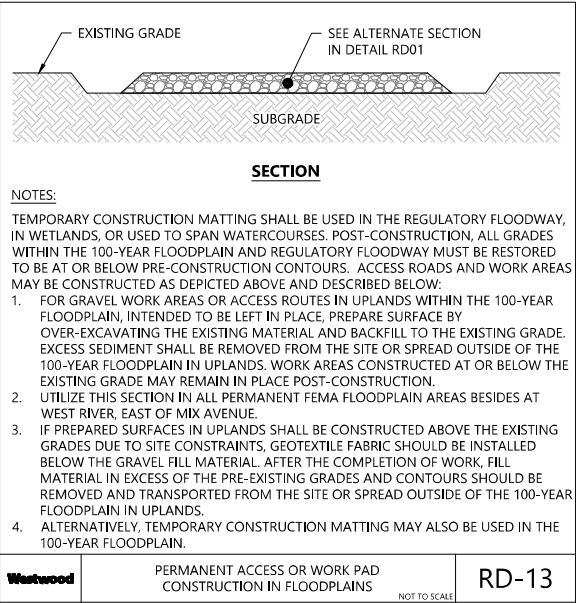
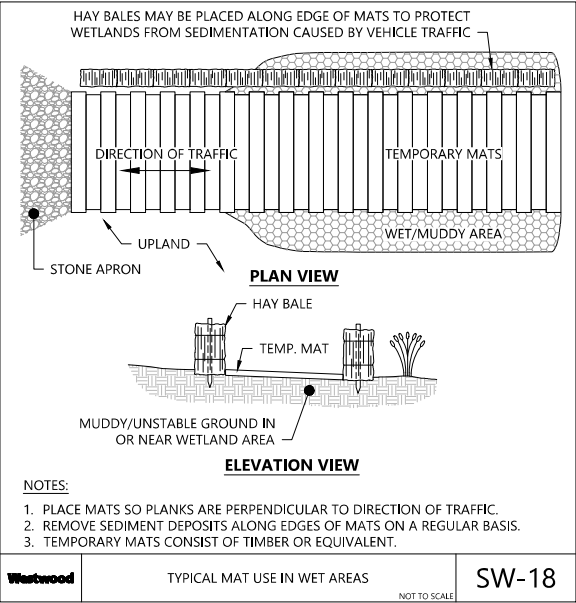
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REV.	DATE	BY	DESCRIPTION	APP.	MSP	DATE:	JUNE 2024	DET 1 OF 2	

UI
AVANGRID

UI 115-kV RR PROJECT MILVON TO WEST RIVER
DEVELOPMENT & MANAGEMENT PLAN
EROSION AND SEDIMENT CONTROL DETAILS




STRUCTURE DETAILS

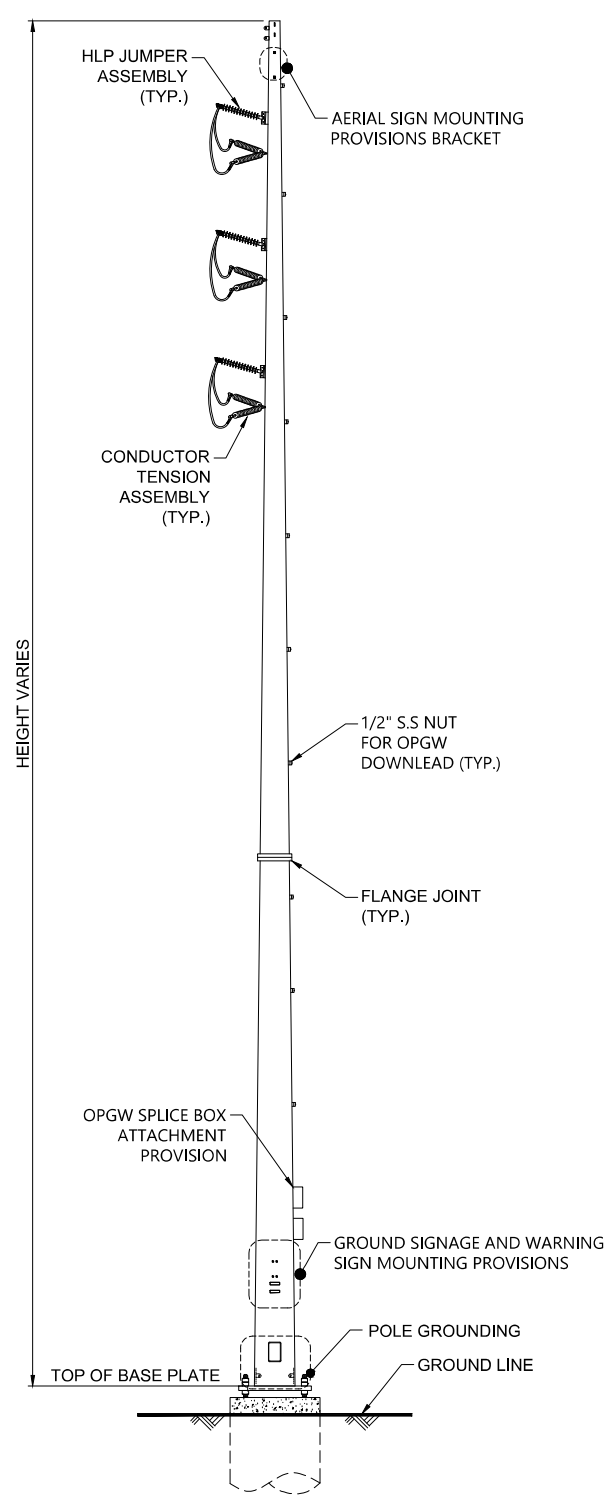
STR 1: TYPICAL STRUCTURE CONFIGURATIONS
STR 2: TYPICAL FOUNDATION & GROUNDING DETAILS

ANSI D CADD Drawing. DO NOT REVISE MANUALLY.

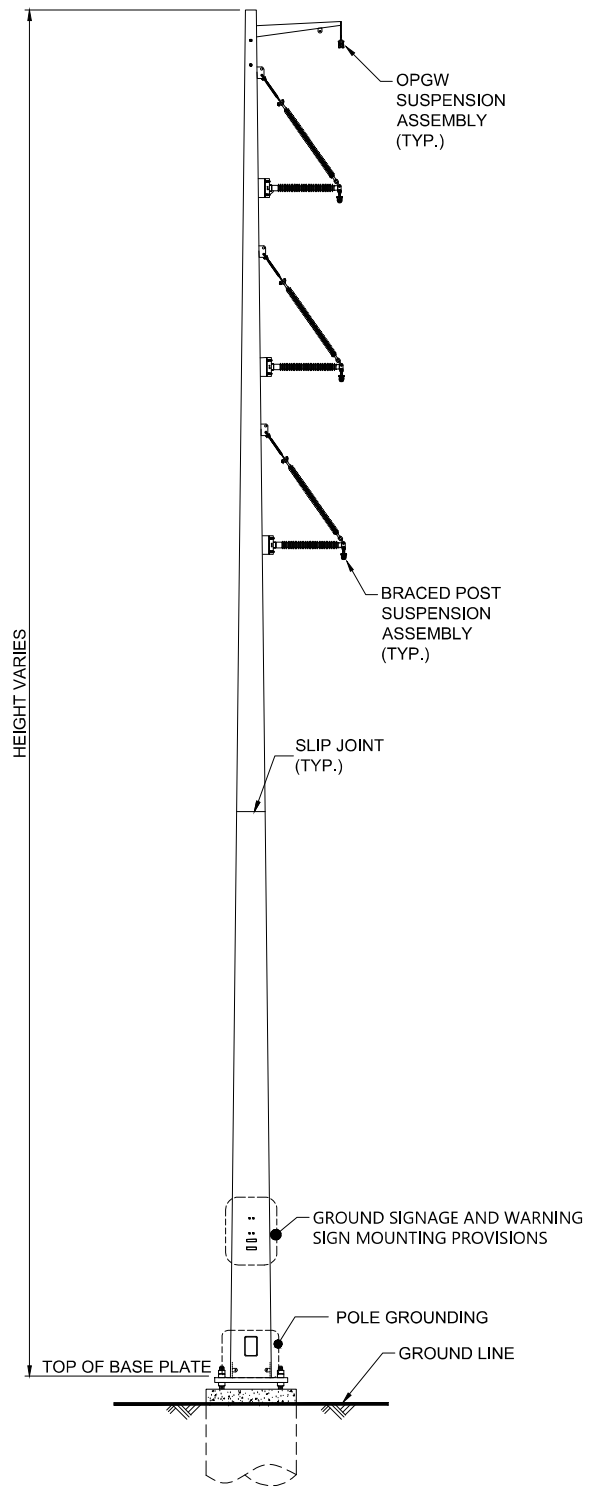
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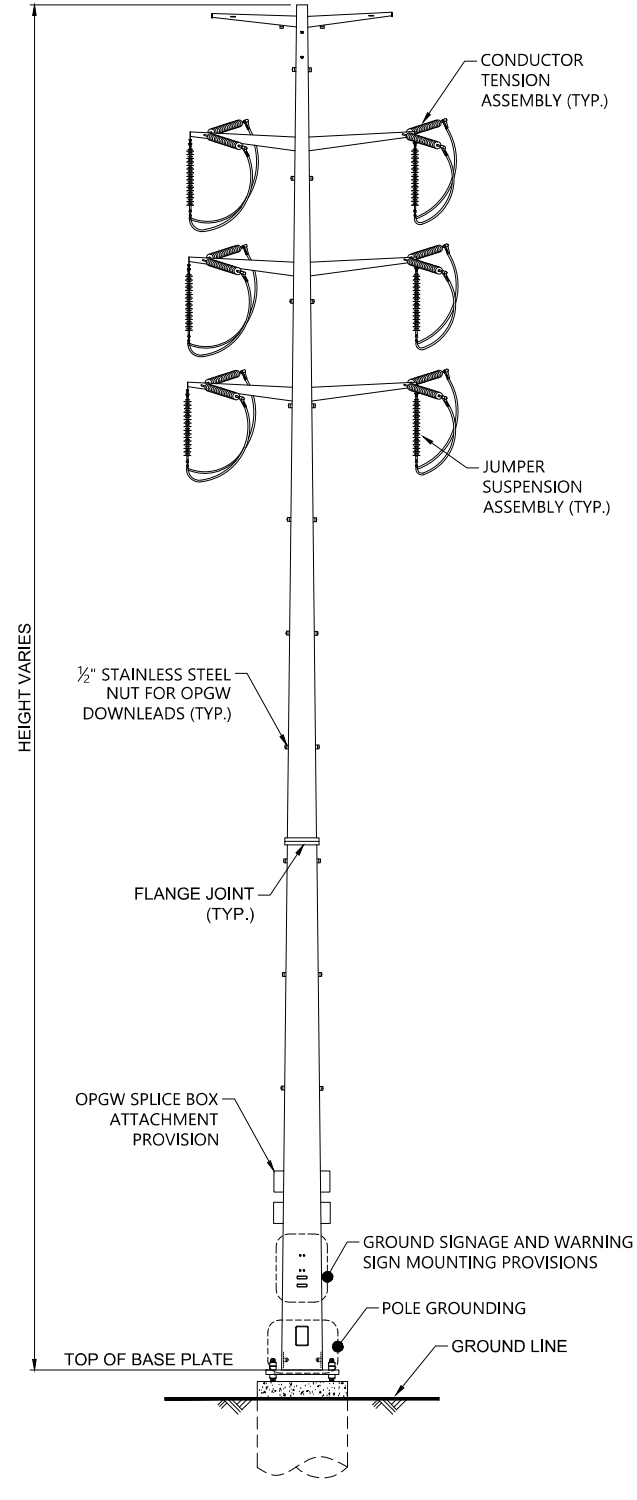
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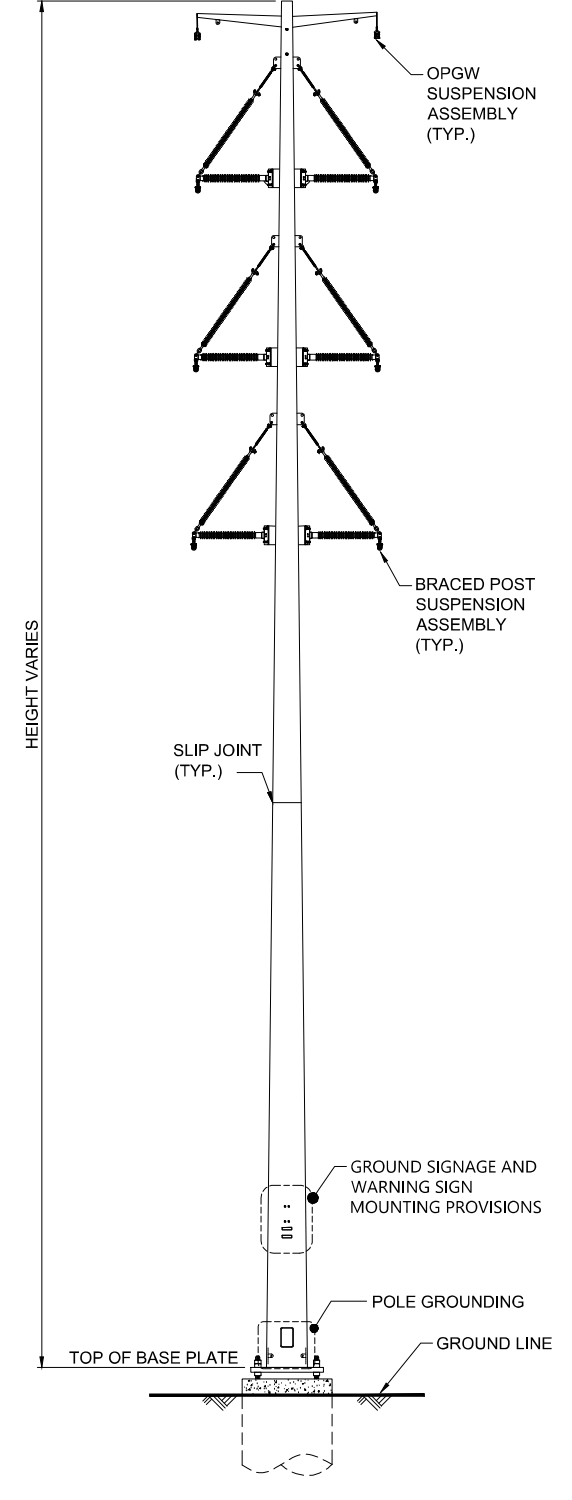
SINGLE CIRCUIT DEADEND STRUCTURE



SINGLE CIRCUIT BRACED POST SUSPENSION STRUCTURE




DOUBLE CIRCUIT DEADEND STRUCTURE



DOUBLE CIRCUIT BRACED POST SUSPENSION STRUCTURE

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						STR 1 OF 2	REV. 0-0B




1. FOR POLE LOCATION COORDINATES, SEE REFERENCE DOCUMENT NO. 1.
2. FOR WORK AREA ACCESS PLANS, SEE THE CIVIL ACCESS PLANS: DRAWING NUMBERS 1500U-T88005A-1-89005B-1-630 THROUGH 1500U-T88005A-1-89005B-1-642.
3. COMPRESSIVE STRENGTH OF CONCRETE SHALL BE MINIMUM 4,500 PSI IN 28 DAYS.
4. REINFORCING STEEL SHALL BE DEFORMED BARS CONFORMING TO ASTM A615, GR. 60.
5. ALL STRAIGHT VERTICAL REINFORCING BARS SHALL BE EVENLY SPACED AROUND THE REINFORCING TIES.
6. ALL REINFORCING TIES SHALL BE EVENLY SPACED.
7. PRE-ASSEMBLED ANCHOR BOLT SETTING TEMPLATES SHALL BE FURNISHED BY NELLO. ANCHOR BOLTS FURNISHED BY NELLO SHALL CONFORM TO ASTM A615 GRADE 75 AND PLATES SHALL CONFORM TO ASTM A572 GRADE 50.
8. ANCHOR BOLT ASSEMBLIES SHALL BE SET CENTERED IN THE AS-DRILLED CAISSON. FOR ADDITIONAL ANCHOR BOLT INSTALLATION INFORMATION, SEE NELLO'S INSTALLATION DRAWINGS FOR THE ANCHOR BOLT ASSEMBLIES.
9. CONTRACTOR INSTALLING THE ANCHOR BOLT ASSEMBLIES SHALL BE FAMILIAR WITH ALL OF NELLO'S INSTALLATION PROCEDURES PRIOR TO INSTALLING ANCHOR BOLT ASSEMBLIES. CONTRACTOR IS RESPONSIBLE FOR SELECTING THE CORRECT ANCHOR BOLT ASSEMBLY FOR THE PIER LOCATION AND VERIFICATION OF ASSEMBLY DIAMETER AND LENGTH REQUIRED PRIOR TO INSTALLATION.
10. ANCHOR BOLT CAGE SHALL BE INSTALLED SUCH THAT ONCE THE CONCRETE HAS CURED THE ANCHOR BOLTS ARE EACH WITHIN $\frac{1}{16}$ INCH OF THE DESIGN LOCATION AND PLUMB WITHIN $\frac{1}{8}$ INCH OVER THE ANCHOR ROD PROJECTION.
11. BEFORE CONCRETE IS PLACED, CARE SHALL BE TAKEN TO ASSURE THAT ALL EMBEDDED ITEMS ARE FIRMLY AND SECURELY FASTENED IN PLACE TO PREVENT DISPLACEMENT. ANCHOR BOLTS SHALL BE SUPPORTED AND SET BY USING TEMPLATES AND BOTTOM PLATES WITH TOP AND BOTTOM ENDS SECURELY TIED TO ENSURE ACCURACY OF LOCATION AND PLUMBNESS. TEMPLATES SHALL BE LEFT IN PLACE DURING AND AFTER CONCRETE PLACEMENT UNTIL INSTALLATION OF STEEL STRUCTURE.
12. BACKFILL AROUND THE FOUNDATIONS WITH CLEAN NATIVE SOIL FREE OF ORGANIC MATERIALS, COMPACTED TO A MINIMUM OF 90% OF MODIFIED PROCTOR DENSITY PER ASTM D1557.
13. FOR ADDITIONAL SUBSURFACE INFORMATION, SEE THE "GEOTECHNICAL REPORT". (ITEM #5 OF REFERENCE DOCUMENT LIST)
14. FORMED CONCRETE SURFACES WHICH WILL BE EXPOSED ABOVE GRADE SHALL BE CLEANED AND RUBBED TO PRODUCE A SMOOTH, UNIFORM SURFACE FREE OF MARKS, VOIDS, SURFACE GLAZE AND DISCOLORATIONS.
15. ALL CONCRETE SHALL BE PLACED IN A SINGLE CONTINUOUS OPERATION TO PRODUCE A MONOLITHIC FOUNDATION AS PER ITEM #4 OF REFERENCE DOCUMENT LIST.
16. ALL REINFORCING CLEAR DISTANCES SHALL BE THE DISTANCE FROM THE FACE OF EACH BAR.
17. NO WELDING TO CAISSON REINFORCING (VERTICAL REBAR OR TIES), OR TO ANCHOR BOLT CAGES SHALL BE PERMITTED. ALL LAP SPLICES USED FOR REINFORCING, IF USED, SHALL BE CLASS-B SPLICES IN ACCORDANCE WITH THE LATEST EDITION OF ACI 318.
18. NEAT-LINE CONCRETE VOLUMES DOES NOT INCLUDE EXTRA CONCRETE THAT MAY BE REQUIRED TO FILL VOIDS OR MAKE FIELD ADJUSTMENTS.
19. CASINGS USED TO FORM FOUNDATIONS SHALL BE REMOVED UNLESS OTHERWISE SPECIFIED BY THE ENGINEER. ANY USE OF A PERMANENT CASING SHALL BE APPROVED BY WESTWOOD.
20. USE OF TELESCOPING CASING IS ALLOWED BUT SHALL BE APPROVED BY UNITED ILLUMINATING AND WESTWOOD.
21. FOUNDATIONS ARE DESIGNED IN ACCORDANCE WITH THE RESPECTIVE SOIL PROPERTIES AND BORING LOGS IN THE "GEOTECHNICAL REPORT" (ITEM #5 OF REFERENCE DOCUMENT LIST).
22. DRILLED PIERS SHALL BE INSTALLED IN ACCORDANCE WITH THE LATEST REVISION OF ACI 336.3R.
23. IF ROCK IS ENCOUNTERED AT A DIFFERENT DEPTH THAN SHOWN ON THE BORING LOGS, NOTIFY WESTWOOD OF POSSIBLE RE-DESIGN OF THAT SPECIFIC FOUNDATION.
24. CORRUGATED FLEXIBLE CONDUIT SHALL BE INSTALLED SUCH THAT WATER DOES NOT COLLECT IN THE CONDUIT AND BELOW GRADE PORTION IS PARALLEL TO RAILROAD TRACKS. SEE ITEMS #6 & #7 OF REFERENCE DOCUMENT LIST FOR MORE INFORMATION ON GROUNDING AND CONDUIT INSTALLATION.
25. CONTRACTORS INVOLVED IN THE INSTALLATION OF THE POLE STRUCTURE FOUNDATIONS SHALL BE FAMILIAR WITH REQUIREMENTS AND INFORMATION CONTAINED IN THE REFERENCE DOCUMENTS. SEE LISTING BELOW.

REFERENCE DRAWINGS AND SPECIFICATIONS		
ITEM #	REFERENCE DRAWING OR SPECIFICATION NUMBER	DRAWING OR SPECIFICATION TITLE
1	1500U-18804A-89048-0000	WOODMONT TO ALLINGS CROSSING STAKING TABLE
2	1500U-18804A-89048-9000	WOODMONT TO ALLINGS CROSSING STRUCTURE LIST
3	TECHNICAL SPECIFICATION SECTION 31 63 29	DRILLED PIER FOUNDATIONS
4	TECHNICAL SPECIFICATION SECTION 03 31 00	CAST-IN-PLACE CONCRETE
5	1500U-188005A-1-89005B-1-9200	GEOTECHNICAL REPORT
6	1500U-18804A-89048-5020	STEEL POLE GROUNDING
7	1500U-18804A-89048-5022	STEEL POLE GROUNDING W/ CROSS BOND CONNECTION

(SEE REFERENCE DRAWINGS FOR ORIENTATION & INFORMATION)

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