



October 31, 2024

Ms. Melanie Bachman, Executive Director
Connecticut Siting Council
10 Franklin Square
New Britain, CT 06051

RE: Docket No. 508: Milvon to West River Railroad Transmission Line 115kV Rebuild Project
Quarterly Construction Status Report – Q3 2024

Dear Ms. Bachman,

Pursuant to the Regulations of Connecticut State Agencies section 16-50j-62(b)(3) and as described in the Milvon to West River Railroad Transmission Line 115kV Rebuild Project Decision & Order (D&O) dated August 28, 2022, The United Illuminating Company hereby provides to the Connecticut Siting Council (“Council”) this Construction Progress Report for the third quarter of 2024.

Should you or the Council members have any questions regarding this submission, please do not hesitate to contact me via e-mail at pkenline@uinet.com.

Sincerely,

A handwritten signature in blue ink that reads 'Paul Kenline'.

Paul Kenline
Project Manager

Enclosure



**CSC DOCKET #508 MILVON TO WEST RIVER RAILROAD TRANSMISSION LINE 115 KV
REBUILD**

Quarter: Q3 2024

Report #5

**CSC DOCKET #508 MILVON TO WEST RIVER RAILROAD
TRANSMISSION LINE 115 KV REBUILD PROJECT SUMMARY**

Segment 1: Elmwest Substation to West River Substation

Pre-Construction Activities

Pre-construction activities completed in Segment 1 included survey/staking of construction work areas, vegetation clearing limits demarcation, confirmation/reflagging of environmental resources on an as-needed basis, the installation of E&S controls, the installation and/or maintenance of species protection controls, and utility location/marketing.

Construction Activities

Construction activities were not conducted.

Post-Construction Activities

Restoration activities in Segment 1 have been significantly completed. The final stormwater stabilization inspection for Segment 1 will be completed following one (1) full growing season or two (2) subsequent seeding seasons in the year following construction. Segment 1 will be considered "active" on a stormwater stabilization perspective. Inspections will be continued as specified in the CTDEEP General Permit for the Discharge of Stormwater and Dewatering Wastewaters from Construction Activities (SWCGP) and the Stormwater Pollution Control Plan (SWPCP) until final stabilization is achieved.



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Segment 2: Allings Crossings Substation to Elmwest Substation

Pre-Construction Activities

Pre-construction activities completed in Segment 2 included survey/staking of construction work areas, vegetation clearing limits demarcation, confirmation/reflagging of environmental resources on an as-needed basis, the installation of E&S controls, and utility location/marketing.

Construction Activities

Construction activities conducted in Segment 2 included vegetation clearing, the establishment and/or maintenance of work pads, the establishment and maintenance of temporary or permanent access roads, maintenance of temporary or permanent access roads, soft-digging, foundation/grounding installation, pole setting, and overhead electrical work.

Post-Construction Activities

No activities completed.



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D & M Deviations

Significant D&M Deviations approved and completed in the third quarter of 2024 include the following:

- UI requested relocation of structures P1008NN, P1008WN, P1009NN, P1022N, P1023N, and P1023AN, as well as the elimination of the temporary access road associated with structure P102AN. The deviation was requested on August 14th and approved by the CSC on August 20th, 2024.
- UI requested a change in construction sequence for Segment 3 (Milvon Substation to Woodmont Substation) and Segment 4 (Allings Crossing Substation to Woodmont Substation) on August 14th, 2024. The CSC granted approval on August 20th, 2024.
- UI requested a Segment referencing change on September 26th, 2024. Specifically, UI requested Segment 3 (Milvon Substation to Woodmont Substation), be referred to as Segment 4 (Milvon Substation to Woodmont Substation), and Segment 4 (Allings Crossing Substation to Woodmont Substation) be referred to as Segment 3 (Allings Crossing to Woodmont Substation). The CSC granted approval on October 4th, 2024.

Minor D&M Deviations completed in the third quarter of 2024 are as follows;

- Minor additional tree removal in the West Haven Railroad Station was necessary to install matting and establish work pads. This was approved by the property owner. However, the additional clearing was requested during pup season for the Northern Long-Eared Bat (NLEB). United Illuminating conducted two (2) NLEB Habitat Assessments to determine if suitable roost habitat for NLEBs was present. Suitable roost habitat for NLEB was *not* observed at the portion of the Project area specified. Therefore, UI conducted clearing in this area.
- The NLEB Habitat Assessment conducted on July 11th, 2024, identified one (1) mockingbird nest with nestlings within the work pad area. United Illuminating worked with the US Fish and Wildlife Service (USFWS) to obtain a Depredation Permit to maintain compliance under the Migratory Bird Treaty Act (MBTA). United Illuminating requested that any nestlings captured be rehabilitated. During the USFWS's field visit, it was found that the nestlings had fledged and left the nest. The USFWS removed the nest and clearing commenced.



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Bonnet Donations

The United Illuminating Company, LLC contacted local railroad museums, including the Shoreline Trolley Museum and the New England Railroad Museum (RMNE) for donation of a bonnet. United Illuminating corresponded with Howard Pincus, Chairman of the RMNE on July 25th, 2024, and contacted the Shoreline Trolley Museum on July 24th, 2024. A response from the Shoreline Trolley Museum was not received. According to Mr. Pincus, the subject components (bonnets) were not something the New England Railroad Museum was interested in.

Osprey Deterrents

Pursuant to condition “a” on the Partial Development and Management (D&M) Plan III-Segment 3 approval, dated May 13th, 2024, United Illuminating contacted the CTDEEP Wildlife Division on May 29th, 2024 regarding the use of osprey deterrents on the new structures and replacement of any osprey platforms for any osprey nests that are removed during construction. According to CTDEEP, there are no requirements regarding the use of osprey deterrents on new structures or the replacement of osprey platforms for osprey nests removed during construction.

However, United Illuminating has met with representatives of the CT Audubon Society and is working with the CTDEEP to determine additional areas to potentially construct Osprey nesting platforms. If United Illuminating constructs additional Osprey nesting platforms, the locations will be disclosed to the Council.

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Photo 1: Typical bonnet removal.

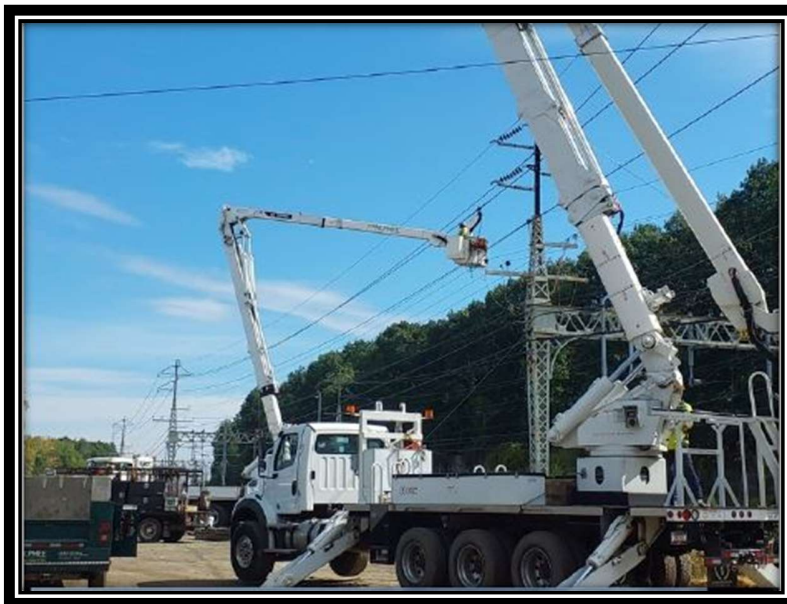


Photo 2: Typical bonnet removal.

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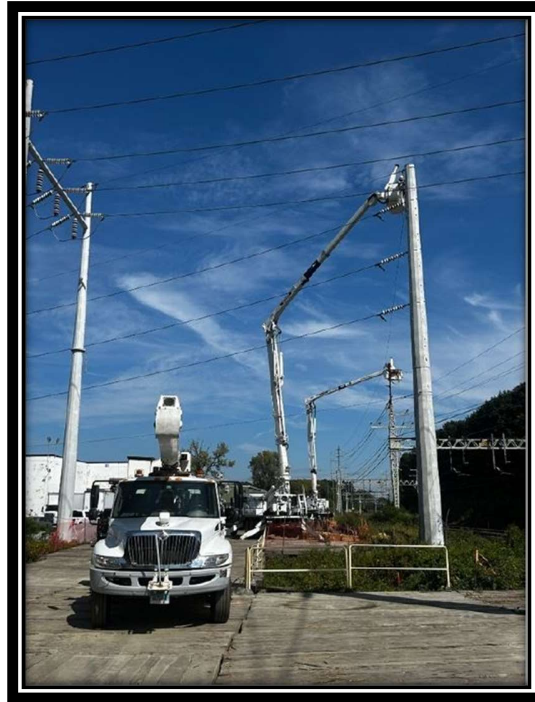


Photo 3: Typical overhead utility work.



Photo 4 : Foundation drilling, typical.



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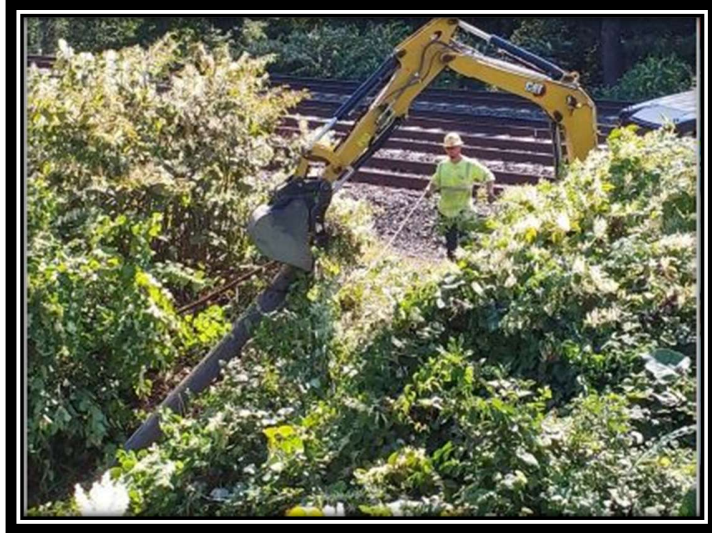


Photo 5: Wood pole removal, typical.



Photo 6: Conductor pull, typical.



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Photo 6: Foundation drilling, typical.



Photo 7: Constructed monopole base, typical.



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Photo 8: Matting placement activities, typical.



Photo 9: Soft dig activities, typical.

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Photo 10: Spinoff bin for drilling activities, lined with impermeable polyethylene sheeting, typical.